

Planning and Zoning Board Agenda April 25, 2023 Room 102 – 7:00 P.M.

Call to Order and Roll Call

Chair Announcement: The public hearing regarding an appeal at 1378 Margret Street has been withdrawn by the appellant and will not be heard this evening.

Approval of Minutes, April 11, 2023

Public Comment: For matters that are not on the agenda.

Pending Applications:

1. Address: 2777 Mannheim Road
(continued from April 11, 2023)

Case Number: 23-014-CU-TSUB

The petitioner is requesting the following items: (i) a conditional use permit to allow three drive-through uses on the subject property; (ii) a tentative plat of subdivision to resubdivide the five existing lots into three lots of record; and (iii) and any other variations, waivers, and zoning relief as may be necessary.

PINs: 09331080120000, 09-33-108-013-0000, 09-33-108-014-0000, 09-33-108-022-0000, and 09-33-108-023-0000

Petitioner: GW Properties (Mitch Goltz), 2211 N. Elston Avenue, Suite 400, Chicago, IL, 60614

Owner: Gus Sutter, 2777 Mannheim Road, Des Plaines, IL, 60018

2. Address: 820-848 Lee Street
(continued from April 11, 2023)

Case Number: 23-013-CU

The petitioner is requesting a conditional use permit for (i) operating a Commercially Zoned Assembly Use in the C-5 Zoning District; (ii) operating a private elementary and high school in the C-5 Zoning District; and (iii) any other variations, waivers, and zoning relief as may be necessary.

PINs: 09-17-425-029-0000, 09-17-425-030-0000, 09-17-425-031-0000, 09-17-425-032-0000, and 09-17-425-033-0000

Petitioner: Little Bulgarian School in Chicago, 832 Lee Street, Des Plaines, IL, 60016

Owner: Little Bulgarian School in Chicago, 832 Lee Street, Des Plaines, IL, 60016

3. Address: 1773 Webster Lane

Case Number: 23-019-V

The petitioner is requesting variations to allow a trellis in the interior side yard that would exceed the height and width requirement for trellises, and any other variations, waivers, and zoning relief as may be necessary.

PIN: 09-29-101-022-0000

Petitioner: Demetrios and Isabelle Giokaris, 1773 Webster Lane, Des Plaines, IL, 60018

Owner: Demetrios and Isabelle Giokaris, 1773 Webster Lane, Des Plaines, IL, 60018

City of Des Plaines, in compliance with the Americans With Disabilities Act, requests that persons with disabilities, who require certain accommodations to allow them to observe and/or participate in the meeting(s) or have questions about the accessibility of the meeting(s) or facilities, contact the ADA Coordinator at 847-391-5486 to allow the City to make reasonable accommodations for these persons. The public hearing may be continued to a further date, time and place without publication of a further published notice such as this notice.

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| Case 23-013 -CU | 820-848 Lee | Conditional Use |
| Case 22-054-CU | 827 Elmhurst Rd | Conditional Use |
| Case 22-055- Appeal | 1378 Margret | Appeal |
| Case 23-012-TSUB | 930,946, & 970 North | Tentative Subdivision |



DES PLAINES PLANNING AND ZONING BOARD MEETING
April 11, 2023
DRAFT MINUTES

The Des Plaines Planning and Zoning Board held its regularly scheduled meeting on Tuesday, April 11, 2023, at 7:00 p.m. in Room 102 of the Des Plaines Civic Center.

Chair Szabo called the meeting to order at 7:00 p.m. and roll call was established.

PRESENT: Catalano, Hofherr, Fowler, Saletnik, Veremis Weaver, Szabo

ABSENT: None

ALSO PRESENT: John Carlisle, AICP, CED Director
Jonathan Stytz, AICP, Senior Planner
Samantha Redman, Associate Planner
Margie Mosele, Executive Assistant

A quorum was present.

Call to Order and Roll Call

Approval of Minutes: March 28, 2023

APPROVAL OF MINUTES

A motion was made by Board Member Hofherr, seconded by Board Member Catalano to approve the meeting minutes of March 28, 2023.

AYES: Hofherr, Catalano, Weaver, Fowler, Saletnik, Szabo

NAYES: None

ABSTAIN: Veremis

*****MOTION CARRIES****

PUBLIC COMMENT ON NON-AGENDA ITEM

None

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Applications

1. Address: 2777 Mannheim Road

Case Number: 23-014-CU-TSUB

The petitioner is requesting the following items: (i) a conditional use permit to allow two drive-through uses on the subject property that is next to residential properties; (ii) a tentative plat of subdivision to consolidate the five existing lots into three lots of record; and (iii) and any other variations, waivers, and zoning relief as may be necessary.

PINs: 09331080120000, 09-33-108-013-0000, 09-33-108-014-0000, 09-33-108-022-0000, and 09-33-108-023-0000

Petitioner: GW Properties (Mitch Goltz), 2211 N. Elston Avenue, Suite 400, Chicago, IL, 60614

Owner: Gus Sutter, 2777 Mannheim Road, Des Plaines, IL, 60018

Date: April 3, 2023

To: Planning and Zoning Board (PZB)

From: John T. Carlisle, AICP, Director of Community and Economic Development
Jonathan Stytz, AICP, Senior Planner

Subject: Request to Continue 23-014-CU-TSUB: 2777 Mannheim Road

Due to the quantity of comments addressed by staff in the Official Review Letter, the petitioner has requested to continue the hearing at the Board’s regular meeting on Tuesday, April 25, 2023. We recommend the Board grant this request, which is attached.

Date: March 30, 2023

City of Des Plaines
1420 Miner Street Des Plaines, IL 60016

RE: 2777 Mannheim Road, Des Plaines – Meeting Continuance Request

Dear City of Des Plaines,
GW Properties is requesting a continuance to our Planning and Zoning Board meeting for the property referenced above. We respectfully request that this meeting be continued to the April 25, 2023, Planning and Zoning Board meeting. Regards,
Mitch Goltz
Principal
GW Properties

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Chair Szabo swore in Mitch Goltz from GW Properties.

Petitioner Goltz stated that they are planning to redevelop the La Cave Banquet Hall. They are requesting a continuation to April 25,2023. They are adjusting their plans and adding a third drive through to the site.

Chair Szabo asked if there are any audience members that are present for this project. No one came forward.

A motion was made by Board Member Catalano, seconded by Board Member Fowler to recommend approval to continue the case to the April 25, 2023 meeting.

AYES: Catalano, Fowler, Weaver, Hofherr, Saletnik, Veremis, Szabo
 NAYES: None
 ABSTAIN: None

*****MOTION CARRIES UNANIMOUSLY****

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2. Address: 820-848 Lee Street

Case Number: 23-013-CU

The petitioner is requesting an amendment to a previously approved conditional use permit and variation, or a new conditional use permit and variation, whichever is necessary, related to the following items: (i) operating a Commercially Zoned Assembly Use in the C-5 Zoning District; (ii) operating a private elementary and high school in the C-5 Zoning District; and (iii) operating with a variation from the collective off-street parking requirements at the subject property; and any other variations, waivers, and zoning relief as may be necessary.

PINs: 09-17-425-029-0000, 09-17-425-030-0000, 09-17-425-031-0000, 09-17-425-032-0000, and 09-17-425-033-0000

Petitioner: Little Bulgarian School in Chicago, 832 Lee Street, Des Plaines, IL, 60016

Owner: Little Bulgarian School in Chicago, 832 Lee Street, Des Plaines, IL, 60016

Request to Continue 23-013-CU: 820-848 Lee St.

In order to address necessary comments related to their case, the petitioner has requested to continue the hearing at the Board’s regular meeting on Tuesday, April 25, 2023. We recommend the Board grant this request, which is attached.

Hi Samantha,

As you know, we represent the Little Bulgarian School in its application for a conditional use permit for a school and commercial assembly use at 820-848 Lee Street. Thank you for your review of the application, which has been noticed for a public hearing at the April 11, 2023, meeting of the Planning and Zoning Board. The Little Bulgarian School is in the process of working with you and other City staff to address certain comments and provide certain information in connection with its application. To facilitate that process and ensure that those matters are addressed before consideration by the Planning and Zoning Board, we respectfully request that the Planning and Board continue the public hearing to its next regularly scheduled meeting on April 25, 2023.

Thanks again for your work on this application. We look forward to continuing to work with the City on it.

Best,
Karl D. Camillucci
Partner

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Chair Szabo asked if the petitioner was present. They are not.

Samantha Redman, Associate Planner, stated that the petitioner has asked for a continuance to submit additional materials and finish their fire inspection.

Chair Szabo asked if any of the audience members are here for this case. Judy Fischer of 799 Graceland stated that she is curious about what they are planning on doing. Asked about whether there will be any construction and what the application is all about.

Ms. Redman stated the petitioner is looking for a Conditional Use permit that is an amendment from their original Conditional Use Permit from 2019. They are looking for an amendment of their 2019 Conditional Use permit for assembly uses for their school to allow for school related activities and cultural activities open to the public. The request is also for a conditional use permit for their private school and a parking variation. Ms. Redman stated that there is no proposed construction on the site.

A motion was made by Board Member Weaver, seconded by Board Member Veremis to approve the continuation of the case to the April 25, 2023 meeting.

AYES: Weaver, Veremis, Catalano, Fowler, Hofherr, Saletnik, Szabo
 NAYES: None
 ABSTAIN: None

*****MOTION CARRIES UNANIMOUSLY****

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3. Address: 827 Elmhurst Road

Case Number: 22-054-CU

The petitioner is requesting a conditional use permit to operate an auto service repair use in the C-3 zoning district and any other variations, waivers, and zoning relief as may be necessary.

PIN: 08-24-100-031-0000

Petitioner: GW Properties (Mitch Goltz), 2211 N. Elston Avenue, Suite 400, Chicago, IL 60614

Owner: RDK Ventures, LLC c/o Mac’s Convenience Stores, LLC, P.O. Box 347, 4080 W. Jonathan Moore Pike, Columbus, IN 47201

Ward: #8, Alderman Shamoan Ebrahimi

Existing Zoning: C-3 General Commercial District

Existing Land Use: Vacant Lot (previous auto fuel station)

Surrounding Zoning:
 North: C-3 General Commercial District
 South: C-3 General Commercial District
 East: C-3 General Commercial District
 West: C-3 General Commercial District

Surrounding Land Use:
 North: Grocery Store (Commercial)
 South: Bank (Commercial)
 East: Grocery Store (Commercial) / Shopping Center (Commercial)
 West: Shopping Center (Commercial)

Street Classification: Elmhurst Road is classified as another principal arterial road.

Comprehensive Plan: The Comprehensive Plan illustrates the site as commercial.

Zoning/Property History: Based on City records, the subject property was used as an auto filling station until 2019. Since then, the fuel station has been demolished and the property has been vacant. The subject 20,099-square-foot (0.46-acre) vacant property is in the C-3 General Commercial district. The subject property fronts Elmhurst Road but is accessed via a single access point through the Jewel-Osco parking lot at 811 Elmhurst Road. As such, a cross-access agreement exists between the property owner—RDK Ventures, LLC c/o Mac’s Convenience Stores, LLC— and Jewel-Osco.

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The PZB conducted a public hearing on January 10, 2023, to review the conditional use application for the proposed auto service business and voted 5-0 to recommend approval to the City Council. However, before the City Council could consider the necessary approving ordinance, the petitioner informed City staff that they would need to alter the Site Plan in a way that affected circulation and was notably different than what was presented in the public hearing. The petitioner elected to withdraw that petition and resubmit the new Site Plan for the PZB’s new consideration and public hearing, for which due noticing has occurred pursuant to the Zoning Ordinance.

Project Description: *Overview*

The petitioner has requested a Conditional Use Permit to allow the construction of a new automotive service repair use, Strickland Oil, at 827 Elmhurst Road. An oil change business falls underneath an auto service repair use, which requires a conditional use permit in the C-3 district. While the proposed use has stayed the same, the proposed Site Plan and site access has changed from the original Site Plan heard by the PZB. The first plan utilized the one existing full access curb-cut off the Jewel-Osco property in line with the existing drive aisle on the Jewel-Osco parking lot for all access to and from the subject property. After conversations with Jewel-Osco, the petitioner provided a new submittal on March 22, 2023 that changes the originally proposed entrance/exit curb-cut to an exit-only curb-cut and creates a new entrance/exit curb-cut at the rear of the subject property as a second connection to the Jewel-Osco parking. As a result, the attached Site Plan, Landscape Plan, and Photometric Plan have been updated to reflect the new submittal for the subject property.

Proposed Development

The petitioner proposes to redevelop the subject property by building a new 1,700-square-foot, single-story building with surface parking area, dumpster enclosure, and freestanding monument sign. The proposed building consists of three service bays, a lobby area, unisex restroom, and office/waste oil storage area. The proposal includes the addition of both three-foot-wide foundation landscape areas around the north and south elevations of the building, and five foot-wide parking lot landscaping areas around the perimeter of the parking area as required in Sections 12-10-8 and 12-10-10 of the Zoning Ordinance. The new submittal has reduced the amount of proposed landscaping on the subject property. However, the minimum requirements have been met. New exterior lighting is also proposed for the new development as shown on the attached Photometric Plan. Section 12-12-10 restricts the amount of excess light that can bleed into surrounding properties based on the zoning of the properties surrounding the subject property. Since the subject property is surrounded by C-3-zoned properties, a maximum of 2.0 foot-candles is allowed. The attached Photometric Plan indicates that the exterior lighting will not exceed 1.3 foot-candles in conformance with the applicable regulations. The new submittal has resulted in a proposed third freestanding light post located next to the rear entrance/exit access point and north property line. While this does add more lighting on the subject property, the revised and attached Photometrics Plan

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indicates that the requirements have been met. The change in site access adjusts the overall circulation of the subject property, directing a majority of the exiting traffic to the far southwest corner of the Jewel-Osco property in line with the existing Jewel-Osco parking lot drive aisle but does provide a secondary exit at the rear of the building as shown in the revised and attached Site Plan. While this revised plan does provide two exits as opposed to the one proposed in the original plan, there may be concerns with the single entrance at the rear of the property regarding potential vehicle stacking on the Jewel-Osco property and lack of vehicle-pedestrian separation, which is a factor reviewed in accordance with the Site Plan Review standards. Nonetheless, the proposed driveway widths for both the one-way and two-way drive aisles meet the standards in Section 12-9-6 of the Zoning Ordinance. However, the PZB may wish to have the petitioner elaborate on the reason for this change, the anticipated benefits of this design, and how potential issues with its design have been addressed.

Off-Street Parking

Auto repair facilities are required to provide two parking spaces per service bay, plus one space for every 200 square feet of accessory retail. As a result, a total of seven off-street parking spaces, including a minimum of one mobility impaired accessible parking space, are required. The Site Plan illustrates a total of 14 parking spaces, including one mobility-impaired accessible space, which meets this standard. All proposed standard parking spaces are proposed to be nine feet wide by 18-feet-long in conformance with Section 12-9-6 of the Zoning Ordinance. The one proposed accessible parking space is designed to be 18-feet-wide by 18-feet-long, which meets the standards in Section 12-9-8.

Business Operations

Strickland Oil proposes to operate from 8 a.m. to 7 p.m. Monday through Friday, 8 a.m. to 5 p.m. on Saturdays, and 10 a.m. to 5 p.m. on Sundays. Their services include stay-in-your-car oil changes, state inspections, tire rotations, air filter replacement, wiper blade replacement, and coolant and washer fluid refills. During normal operations, a total of 3-4 employees will be on site at a given time. Please see the attached Project Narrative for more information.

Conditional Use Findings:

Conditional Use requests are subject to the standards set forth in Section 12-3- 4.E of the Zoning Ordinance. Rationale for how the proposed amendments would satisfy the standards is provided below and in the petitioner’s response to standards. The PZB may use this rationale toward its recommendation, or the Board may adopt its own.

- 1. The proposed Conditional Use is in fact a Conditional Use established within the specific Zoning district involved:**

Comment: Auto service repair is a Conditional Use, as specified in Section 12-7-3.K. of the Zoning Ordinance for properties in the C-3 General Commercial District.

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PZB Additions or Modifications (if necessary): _____

2. The proposed Conditional Use is in accordance with the objectives of the City’s Comprehensive Plan:

Comment: The Comprehensive Plan illustrates this property as commercial. The Comprehensive Plan strives to foster growth and redevelopment of existing commercial corridors to retain new businesses in Des Plaines. The addition of a new commercial development meets this intent while also repurposing a vacant lot along a major commercial corridor in Des Plaines.

PZB Additions or Modifications (if necessary): _____

3. The proposed Conditional Use is designed, constructed, operated and maintained to be harmonious and appropriate in appearance with the existing or intended character of the general vicinity:

Comment: The petitioner proposes to repurpose the property with a new commercial development designed to be consistent with and complementary to the surrounding commercial uses in the area. The proposed improvements, including landscaping, will transform the vacant property into a new use that will benefit the site from both a functional and aesthetic standpoint.

PZB Additions or Modifications (if necessary): _____

4. The proposed Conditional Use is not hazardous or disturbing to existing neighboring uses:

Comment: The proposed automotive repair use will not be hazardous or distributing to neighboring uses because all operations will be conducted within this building. The proposed landscape screening and exterior lighting is designed to minimize the impact on surrounding properties. In addition, the new business will provide new services to Des Plaines’ residents.

PZB Additions or Modifications (if necessary): _____

5. The proposed Conditional Use is to be served adequately by essential public facilities and services, such as highways, streets, police and fire protection, drainage structures, refuse disposal, water and sewer, and schools; or agencies responsible for establishing the Conditional Use shall provide adequately any such services:

Comment: The subject property was adequately served by essential public facilities and services when the previous auto filling station was in operation. The proposed auto service repair use will also be adequately served by public facilities and services as the existing access point from Elmhurst Road via the Jewel-Osco parking lot will remain unchanged. However, the changes in the access from the Jewel-Osco parking lot to the subject property will alter the access into the property by limiting all entering traffic to the rear of the subject property.

6. PZB Additions or Modifications (if necessary): _____

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7. The proposed Conditional Use does not create excessive additional requirements at public expense for public facilities and services and will not be detrimental to the economic well-being of the entire community:

Comment: The proposed auto service repair facility will not create a burden on public facilities or be a detriment to the economic well-being of the community. When compared to the previous auto filling station, there is no anticipated increase in demand for public facilities as a result of the Conditional Use Permit for a new auto service repair use.

PZB Additions or Modifications (if necessary): _____

8. The proposed Conditional Use does not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property, or the general welfare by reason of excessive production of traffic, noise, smoke fumes, glare or odors:

Comment: The proposed auto service repair use is not anticipated to create additional traffic as compared to the previous auto filling station. None of the proposed activities occurring on site will be detrimental to the public. Staff has notified the petitioner of the required mechanical systems that will need to be installed to reduce the production of traffic, noise, smoke fumes, glare, and odors generating from this use.

PZB Additions or Modifications (if necessary): _____

9. The proposed Conditional Use provides vehicular access to the property designed so that it does not create an interference with traffic on surrounding public thoroughfares:

Comment: The proposed auto service repair use will not create an interference with traffic on surrounding public thoroughfares. There will be no changes to the existing access point onto the property through the Jewel-Osco parking lot from Elmhurst Road that was utilized by the previous auto filling station.

PZB Additions or Modifications (if necessary): _____

10. The proposed Conditional Use does not result in the destruction, loss, or damage of natural, scenic, or historic features of major importance:

Comment: The proposed auto service repair use would not cause the destruction, loss, or damage of any natural, scenic or historic features since the site was already developed for the use of an auto filling station.

The petitioner will redevelop the site with a freestanding building and add landscaping and screening to improve the aesthetics of the property.

PZB Additions or Modifications (if necessary): _____

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11. The proposed Conditional Use complies with all additional regulations in the Zoning Ordinance specific to the Conditional Use requested:

Comment: The proposed auto service repair use meets all other requirements of the Zoning Ordinance for the C-3 General Commercial District. No variations or additional actions are requested beyond the Conditional Use Permit.

PZB Additions or Modifications (if necessary): _____

PZB Procedure and Recommended Conditions:

Under Section 12-3-4.D (Procedure for Review and Decision for Conditional Uses) of the Zoning Ordinance, the PZB has the authority to *recommend* that the City Council approve, approve subject to conditions, or deny the above-mentioned conditional use for an auto service repair use at 827 Elmhurst Road. The City Council has final authority on the proposal. Consideration of the request should be based on a review of the information presented by the applicant and the findings made above, as specified in Section 12-3-4.E (Standards for Conditional Uses) of the Zoning Ordinance. If the PZB recommends and City Council ultimately approves the request, staff recommends the following conditions.

Conditions of Approval:

1. Vehicles related to the business cannot be stored or parked overnight on the surrounding streets.
2. No damaged or inoperable vehicles shall be parked or stored outside at any time.
3. A revised cross-access agreement between the ownership of the subject property and the property at 811 Elmhurst (Jewel-Osco) will be provided at the time of building permit approval and maintained throughout the operation of the conditional use.
4. All submitted permit documents shall be sealed and signed by a design professional licensed in the State of Illinois and must comply with all City of Des Plaines building codes.
5. That a Do Not Enter sign shall be installed at the front exit curb-cut and stop signs installed at both proposed exits on the subject property.

Attachments:

- Attachment 1: Location and Zoning Map
- Attachment 2: Site and Context Photos
- Attachment 3: ALTA/ACSM Land Title Survey
- Attachment 4: Petitioner’s Standards for a Conditional Use (*New Submittal on March 22, 2023*)
- Attachment 5: Petitioner’s Project Narrative (*New Submittal on March 22, 2023*)
- Attachment 6: Business Narrative (*New Submittal on March 22, 2023; Revised on March 31,2023*)
- Attachment 7: Site Plan (*New Submittal on March 22, 2023; Revised on March 31,2023*)
- Attachment 8: Floor Plan (*New Submittal on March 22, 2023*)
- Attachment 9: Elevations (*New Submittal on March 22, 2023; Revised on March 31,2023*)
- Attachment 10: Photometric Plan (*New Submittal on March 22, 2023; Revised on March 31,2023*)
- Attachment 11: Landscape Plan (*New Submittal on March 22, 2023; Revised on March 31,2023*)
- Attachment 12: Sign Plan (*New Submittal on March 22, 2023; Revised on March 31,2023*)

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Mitch Goltz from GW Properties represents this project and was previously sworn in. He stated that he was at the January 10, 2023 Planning and Zoning Board meeting for the Strickland Brothers proposal that passed unanimously. Since then, Jewel has requested a change to the site plan, including adding a secondary access point. This is the only change to the application. Both access points would be from the abutting Jewel-Osco parking, one entrance/exit curb-cut on the northeast corner of the site and one exit-only curb-cut on the northwest corner of the site.

Jonathan Stytz, Senior Planner, provided a recap of the changes since the last meeting. The main change is adding an additional curb cut to back of the property as requested by Jewel Osco. Mr. Stytz went over the presentation which included the old and new site plans. The floor plan and elevations are the same. Additional conditions have been added regarding signage – including stop signs and exit only signs to mitigate any issues with circulation.

Member Weaver asked if cars are pulling right up into the path of cars turning from Elmhurst Road. He asked if cars turning out of the facility have good sight onto Elmhurst Road to see who is coming in and out and if the cars are heading north on Elmhurst Road, heading into a conflict point, can see the cars coming from the site.

Mr. Stytz stated that there are existing trees, and a proposed monument sign may block the view.

Mr. Goltz stated that they do not feel that this is a concern. He stated that they looked at this with the tenant and our team. This site is not generating a lot of traffic and we were surprised by Jewel requesting this change. There is space to circulate around the rear. Cars will be able to look both ways to see.

Member Weaver stated there would be traffic from people pulling in and going into Jewel too, not just the oil change customers.

Mr. Goltz stated that there is an area with different traffic patterns at play. It is a not a high impact use with minimal traffic generated. It is 40 ft from the intersection of the parking lot.

Member Catalano asked if there is a stop sign leaving the site.

Mr. Goltz stated that they will have a Do Not Enter sign. If this was a drive-through restaurant, we would re-visit, but this use does not have a high impact on traffic.

Chair Szabo swore in Christine Newberry who represents Country Acres condos, behind the Jewel shopping center. She stated that there are two buildings that have existed for a long time and they are concerned about more traffic. Elmhurst Road is very dangerous right now because of all the entrances to the Subway, the condo entrance, Jewel, etc. The people pulling into the driveways ride in the central lane. There have been lots of accidents there and there is traffic generated by this use. It uses the same driveway as the Jewel. If cars go in there, and he is talking about entering in the back, the only thing in the back is an alleyway where trucks deliver to Jewel. There is no street or entryway, except for trucks going from Dempster Street to the back

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of the Jewel. She added that even if you put a stop sign in, people don't stop at signs in parking lots. She also asked if there will be overnight cars there, how will they get them in, and if there will be cars towed and left overnight. She asked if there will be places to park because of the existing concerns with another automotive shop located two blocks away. They are worried about property values, noise and congestion.

Chari Szabo asked if the petitioner can answer some of these questions.

Mr. Goltz stated that there is no overnight use, no parked vehicles, and it is low impact use. The previous gas station in this location was more intensive use than this. It has existing access and circulation throughout the site.

Chair Szabo asked when the Shell was knocked down and will this proposal take the same footprint as the gas station.

Member Veremis stated the gas station was demolished in 2019.

Mr. Goltz stated that the former footprint had a gas station with a convenience store. The amount of traffic going in and out of this facility will be considerably less. No overnight parking and no auto repair. This is limited to routine services as outlined in the packet.

Member Saletnik stated that this is an oil change facility. It is quick in and quick out and is different from auto repair.

Mr. Goltz confirmed that the proposed business is an oil change facility, not an auto repair.

A motion was made by Board Member Weaver seconded by Board Member Catalano to recommend approval of the Conditional Use to the City Council with the five conditions as recommended by staff.

AYES: Weaver, Catalano, Fowler, Hofherr, Veremis, Saletnik, Szabo
 NAYES: None
 ABSTAIN: None

*****MOTION CARRIES UNANIMOUSLY *****

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4. Address: 1378 Margret Street **Case Number: 22-055-APPEAL**
(continued from January 24, 2023 and February 28, 2023)

The petitioner is appealing a decision by the Zoning Administrator regarding the classification of a structure on the subject property as a trellis.

PIN: 09-20-314-012-0000

Petitioner: Jennifer Toner, 1368 Margret Street, Des Plaines, IL 60018

Owner: Patrick and Val Howe, 1378 Margret Street, Des Plaines, IL 60018

The subject of the appeal is a Zoning Administrator determination that a structure originally erected in 2022 at 1378 Margret Street is a trellis and subject to the rules of the Zoning Ordinance regarding trellises. Based on a recommendation from the PZB regarding zoning text amendments, the City Council passed Ordinance Z-6-23 on April 4, 2023. The Ordinance amended restrictions regarding trellises (e.g. height, width, minimum separation). Although the new regulations are in effect, City staff must conduct further investigation regarding the subject property. Therefore, I recommend the Board grant this continuance to the next regular meeting: April 25, 2023.

Attachments

Attachment 1: Petitioner’s Email Regarding Continuation of Hearing to April 25, 2023

1378 Margret Appellant; Request for Continuance

John,

We ask that you please continue our appeal to the April 25, 2023 meeting date.

Adam J. Findlay, J.D.

1378 Margret St Continuance

John Carlisle, CED Director stated that the Fence/Trellis text Amendment passed on the April 3, 2023 City Council meeting. The Petitioner has not fully withdrawn the appeal because it appears that the structure has been erected again. There is additional staff investigation into this case and if the determination of the appeal is still relevant. We learned about this late last week. The appellant asked for one more continuation to allow staff to have time to complete the investigation and see if the appellant sees if there is any purpose to it. It is necessary for staff to have more time with the rules.

Member Fowler asked to be reminded of the reason for the appeal.

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Mr. Carlisle stated that this is an appeal of my decision to classify this structure as a trellis. Staff through that process realized the most prudent policy action was to address the rules with a zoning text amendment. As with our recent observations, we have seen the structure is up. We need to decide if we are applying the old or new rules and if there needs to be a new determination. The timing is tricky. The person duly filed their appeal and wanted a clear answer on the implications of the new rule. As staff, we cannot answer questions about this structure at this point and need additional time to provide better advice.

Member Fowler asked if they put the structure up after they voted on the new trellis regulations.

Mr. Carlisle stated that it is possible it is grandfathered in but staff needs to be able to determine applicable rules.

Member Fowler stated that maybe we could be more lenient with residents, and let other petitioners have more leeway.

Member Weaver stated that we have residents with different opinions.

Mr. Carlisle stated that the neighbor is the appellant.

Member Saletnik asked if the appellant has legal counsel.

Mr. Carlisle stated that yes, the Appellant is represented.

No public comment.

A motion was made by Board Member Hofherr, seconded by Board Member Saletnik to approve the continuation to the April 25, 2023 meeting.

AYES: Hofherr, Saletnik, Weaver, Catalano, Fowler, Veremis, Szabo

NAYES: None

ABSTAIN: None

*****MOTION CARRIES UNANIMOUSLY ****

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5.Address: 930, 946, and 970 North Avenue Case Number: 23-012-TSUB

The applicant has requested a Tentative Plat of Subdivision under section 13-2-2 of the Subdivision Regulations to re-initiate and ultimately complete consolidation of multiple existing lots into one.

PINs: 09-17-103-031-0000, 09-17-103-041-0000, 09-17-103-042-0000,
09-17-103-044-0000, 09-17-103-045-0000, and 09-17-103-048-0000

Applicant: Blue Sky Residence, LLC (Representative: Dan R. Pontarelli,
1204 E. Central Road, Arlington Heights, IL 60005)

Owner: Old Second National Bank Land Trust No. 7923 via Chicago Title Land Trust
Company (Beneficiary: Blue Sky Residence, LLC, 1204 E. Central Road,
Arlington Heights, IL 60005)

Ward: #3, Alderman Sean Oskerka

Existing Zoning: R-3 Townhouse Residential District (*previously a mix of R-1
Single Family Residential and M-2 General Manufacturing
Districts*)

Existing Land Use: Vacant (previously a manufacturing building and surface parking
areas and a single-family lot)

Surrounding Zoning: North: M-2 General Manufacturing District
South: R-1 Single Family Residential District
East: M-2 General Manufacturing / R-1 Single Family Residential
Districts
West: M-2 General Manufacturing / R-1 Single Family Residential
Districts

Surrounding Land Use: North: Factory (Manufacturing) / ComEd (Public Utilities)
South: Single Family Dwellings (Residential)
East: Multi-Tenant Industrial Building (Manufacturing) / Single
Family Dwellings (Residential)
West: Factory (Manufacturing) / Single Family Dwellings
(Residential)

Street Classification: North Avenue is classified as a local road.

Comprehensive Plan: The Comprehensive Plan illustrates the site as multi-family
residential.

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Zoning/Property History:

Based on City records, the subject property was originally multiple zoning lots spread across two different zoning districts: M-2 General Manufacturing for a manufacturing building and its parking area and R-1 Single Family Residential for the portion of land fronting on North Avenue. The subject property was owned by two different entities. Beginning in 2018, the current applicant pursued approvals for a 67-unit (one-bedroom units) senior living facility known as Blue Sky Residence. It was previously proposed in two steps. First, in 2018, a preliminary planned unit development (PUD); Tentative Plat of Subdivision to consolidate six lots of record and multiple zoning lots, into one; and a map amendment (rezoning) from the M-2 and R-1 zoning districts to the R-3 Townhouse Residential district. The proposal then received Final Plat and Final PUD approval from City Council in 2019, pursuant to Ordinance Z-30-19. At the time, the developer demolished the manufacturing building and cleared the subject properties but was unable to initiate building construction before the PUD approval expired. Further, the applicant did not record the Plat, deposit the required parkland dedication fee-in-lieu, or file any public improvement performance securities as may have been required. Section 13-2-10 of the Subdivision Regulations sets forth time limits on plat approvals, and the previous approval expired.

However, the rezoning of the subject properties to the R-3 district via Ordinance Z-30-18 is in effect for all of the subject property. Currently, the subject lots are vacant, and the applicant is intending to build a different multifamily residential project, pursuant to all requirements of the R-3 district and the Zoning Ordinance overall (i.e., height, use, density, minimum parking, etc.).

As stated above, the Tentative and Final Plat of Subdivision process was not completed, despite approvals in 2018 and 2019 respectively. The City approved real estate transfers in early 2019 for two sales of multiple zoning lots that were intended to be consolidated into one zoning lot that would comprise the site. Therefore, the applicant’s entity/land trust took title to all of the land. However, the City expected when approving the transfers that the consolidation would be completed. Therefore, the applicant is completing the process at this time to ensure they can proceed to permitting for their new project.

Project Description: Overview

The petitioner has requested a Tentative Plat of Subdivision to create one consolidated zoning lot and lot of record toward the construction of a new multi-family residential development in the R-3 district. “Dwelling, Multiple Family” is a permitted use in R-3 (Section 12-7-2, Table 1, of the Zoning Ordinance; P = Permitted Use).

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| Uses | R-1 | R-2 | R-3 | R-4 |
|---------------------------|-----|-----|-----|-----|
| Dwelling, multiple-family | | | P | P |

The existing addresses and their respective PINs are listed in the table below. The proposed subdivision, titled 946 North Subdivision, will consist of one 3.55-acre lot with all existing lots combined as shown on the Tentative Plat.

| Address | PIN(s) | Lot Area(s) |
|------------------|--|---|
| 930 North Avenue | 09-17-103-045-0000; 09-17-103-048-0000 | 1,331 SF (0.03 acres); 10,038 SF (0.23 acres) |
| 946 North Avenue | 09-17-103-031-0000; 09-17-103-041-0000; 09-17-103-044-0000 | 42,697 SF (0.98 acres); 7,460 SF (0.17 acres); 33,863 SF (0.78 acres) |
| 970 North Avenue | 09-17-103-042-0000 | 58,788 SF (1.35 acres) |

The proposed 3.55-acre lot allows for a maximum of 55 dwelling units, based on the minimum lot area of 2,800 square feet per dwelling unit. An excerpt of Section 12-7-2, Table 2, of the Zoning Ordinance is provided below for interior lots in all residential districts.

| Bulk Controls | R-1 | R-2 | R-3 | R-4 |
|--------------------------------|---|---|--|--|
| Maximum height | 2½ stories to 35 ft | 2½ stories to 35 ft | 45 ft | 80 ft |
| Minimum lot area, interior lot | 6,875 sq. ft. | 2,800 sq. ft. per DU | 2,800 sq. ft. per DU | 10,000 sq. ft. |
| Minimum front yard | 25 ft | 25 ft | 25 ft | 12 ft |
| Minimum side yard | 5 ft | 5 ft | Buildings 35 ft and under: 5 ft; Over 35 ft: 10 ft | 5 ft, plus 2 ft for every 10 ft over 40 ft |
| Minimum rear yard | 25 ft. or 20% of lot depth, whichever is less | 25 ft. or 20% of lot depth, whichever is less | Buildings 35 ft and under: 25 ft or 20% of lot depth, whichever is less; Over 35 ft: 30 ft | Buildings 40 ft and under: 25 ft or 20% of lot depth, whichever is less; Over 40 ft: 25 ft, plus 2 ft for every 10 ft over 40 ft |

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The PZB may ask the applicant to share how many units are proposed, how tall the proposed building will be, or any other project details; however, these are not required submittals for Tentative Plat review.

Building Lines and Easements

The 946 North Subdivision shows the following easements and building lines: (i) a new 25-foot front building setback line along North Avenue where the proposed subdivision abuts the street and behind single family dwellings where the proposed subdivision abuts residential properties; (ii) a 30-foot rear building setback line along the north of the proposed subdivision; (iii) a ten-foot side building setback line along both the west and east boundaries of the proposed subdivision; (iv) a ten-foot-wide water main easement throughout the proposed subdivision spanning from the west entrance to the east entrance and including 5-foot water easements spaced for fire hydrants; (v) a 35-foot ingress, egress, and public utility easement along the west property line to be terminated; (vi) a ten-foot-wide public utility easement along the eastern side of the subdivision to the street; (vii) a five-foot drainage and public utilities easement near the center of the proposed subdivision to be terminated; and (viii) a ten-foot-wide ComEd easement and Illinois Bell Telephone easement to be terminated.

Subdivision Process, Required Public Improvements

Although the petitioner’s request is for a Tentative Plat only at this time, the Board and public may benefit from understanding the requirements of a Final Plat, which is the second step in the subdivision approval process. The steps for Final Plat are articulated in Sections 13-2-4 through 13-2-8 of the Subdivision Regulations. In summary, the Final Plat submittal requires engineering plans that must be approved by the City Engineer, in particular a grading and stormwater management plan. Ultimately a permit from the Metropolitan Water Reclamation District (MWRD) will be required for construction. Tentative Plat approval does not require submittal of engineering plans. Regardless, the Department of Public Works and Engineering has provided brief comments (attached) based on the submittal. The Engineering review is more detailed for plans at the Final Plat stage, as those are accompanied by civil drawings, which are not required at the Tentative Plat stage.

PZB Procedure:

Under Section 13-2-3 of the Subdivision Regulations, the PZB has the authority to approve, approve subject to conditions or deny the Tentative Plat. A Final Plat of Subdivision, to involve the review of more detailed engineering and public improvements, would be required at a later time. The Final Plat requires review and approval of both the PZB and the City Council.

Attachments:

- Attachment 1: Location and Zoning Map
- Attachment 2: Site and Context Photos
- Attachment 3: Boundary and Topographic Survey
- Attachment 4: Petitioner’s Project Narrative
- Attachment 5: Engineering Comments
- Attachment 6: Public Utility “Will-Serve” Letters
- Attachment 7: Tentative Plat of Subdivision

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Chair Szabo swore in Dan Pontarelli, petitioner for the project and Todd Shaffer, Engineer for the project from Haeger Engineering. Mr. Shaffar stated that the request is for approval of TSUB to consolidate six pins into a single lot of record and address various easements. This is a necessary step to move into the final plat for the project. He describes the existing and proposed site along with the proposed utilities, stormwater management facility, and the circulation pattern (two curb cuts on North Avenue). The proposed site plan is a three-story apartment building conforming to the R-3 zoning. It will have enclosed garages in the building footprint and surface parking. The connecting wing will serve as mechanical rooms and for fire sprinklers and for amenities in the apartment building. In the northwest is a flag lot between the single-family homes with more amenity spaces, specifically a landscaped area. Space is allotted for MWRD requirements.

Chair Szabo asked how many of the current board members were not on the board when this was previously heard saying that there may be additional questions as several of us heard this case 3-4 years ago.

Member Fowler asked if they have considered purchasing the homes affected by this as this is the strangest plat of survey. She also asked if the streets going in both directions, or will it be one way.

Mr. Pontarelli stated that we do not see why the drive cannot be two ways. It is wide enough. The idea of trying to assemble more properties has been considered but he does not see a need to acquire more land. He described the 5500 sq ft home in the middle, which is 15 years old. On the other side is a nice 50-year-old home. One home to the west too. We felt it can work well how it is.

Member Fowler asked if you could commit to something like a structure or barrier between the house to the west because there is a pool there in the back yard. Based on how the driveway curves out there, she is concerned with kids entering the driveway. She added that the petitioner had committed to something in this location, a landscaping area, guardrail, etc. with the previous project.

Mr. Shaffer stated that this is a transition parcel site. It was a manufacturing and residential site. When the previous petitioner came through, if the PUD expired, it would revert to R-3 zoning. You have the railroad on the north, manufacturing on the east. We have placed the drive aisle as far as we can from the existing residential. The stormwater management basin is a requirement from the MWRD. As you go to the west, between the houses, the park amenity will be available to the residents and the general public. As the road curves, we are perpendicular to North Ave and stormwater management is needed in this area. We are proposing a fence around the perimeter. We will work with staff on these safety concerns. This is a B6-12 curb, so it will be a barrier. There will be parkway trees, curb, and a fence present. The fence is 8' per City Code.

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Member Fowler asked if the petitioner would you also consider extra safety measures as discussed on the previous case on Elmhurst Road. She also asked if the development is age-restricted for individuals 55 and older.

Mr. Shaffar stated that this is a non-age restricted apartment. There are 51 apartments are proposed at this time. He asked if he can we go on the record that we will work with the City engineer.

Member Szabo stated that you may talk to them about speed bumps in this location.

Member Saletnik said that this is a roadway between a parking lot and a building. I don't see this as a high-speed travel way.

Member Catalano stated that I am a civil engineer. You cannot just put a guardrail in there.

Mr. Pontarelli said that we could put something there, such as an attractive landscape feature.

John Carlisle, CED Director, said that this has been productive back and forth. Your purview tonight is just to make sure it follows the subdivision ordinance. This is a by right project. Jonathan Stytz will review in greater detail. The site plan review for the apartment project will not come through your board because it doesn't require additional entitlements because it is a by right development. They need to consolidate the land and have this plat approved by you. The site plan review is completed by staff during the building permit process.

Member Veremis asked if this was approved previously – since it has now expired, why are we seeing it again?

Mr. Carlisle stated that Blue Sky development is no longer approved. On the screen is the concept site plan, it is not part of your packet, it is technically not within your decision-making power on the site plan. The consolidation was not completed with the previous project. Your purview is to make sure the plat meets the subdivision regulations. It is the board's role to approve that. It would come back for final review with the final plat when final design is ready.

Member Fowler asked if neighbors notified.

Mr. Carlisle stated that they were notified and there is only a requirement for a posted sign for a subdivision. No mailer or newspaper notice is required for a subdivision.

Member Saletnik asked if the final approval goes through council. There will be feedback then too.

Jonathan Stytz, Senior Planner gave the staff report. He explained the Tentative Plan. In 2018, a preliminary planned unit development (PUD), a Tentative Plat of Subdivision to consolidate six lots of record and multiple zoning lots, into one and a map amendment (rezoning) from the M-2 and R-1 zoning districts to the R-3 Townhouse Residential district were approved. The proposal then received Final Plat and Final PUD approval from City Council in 2019, pursuant to Ordinance Z-30-19. The construction was not done before the PUD entitlement expired. Mr. Stytz explains the previous Map Amendment from M-2 to R-3, allowing this type of apartment

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project to be permitted by right. Mr. Stytz went over the Location Map and Overview and site photos, The Tentative Plat of Subdivision explains the consolidation of six lots into one. Mr. Stytz reminded the Planning and Zoning Board of their options for action.

The PZB has the authority to approve, approve with conditions or deny the Tentative Plat. A Final Plat of Subdivision will be provided at a later time and it will include the review of more detailed engineering and public improvements.. The Final Plat requires review and approval of both the PZB and the City Council.

No public comment.

A motion was made by Board Member Saletnik seconded by Board Member Weaver to approve the Tentative Plat of Subdivision as recommended by staff.

AYES: Saletnik, Weaver, Catalano, Hofherr, Veremis, Szabo
 NAYES: Fowler
 ABSTAIN: None

*****MOTION CARRIES*****

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New Business:

1. Discussion of Potential PZB Workshop and Special Meeting

The PZB Rules of Procedure (Section 4.04) establishes that the Board may hold “workshop sessions as it determines to be necessary from time to time.”

Discussion: Staff has had preliminary discussions with a developer who is interested in having a public meeting to seek community input on potential projects for two separate sites. This public meeting would occur before the developer has due process rights under a formally filed petition and/or with the developer’s consent to employ this community meeting as the first step in the process, before formal public hearings. Senior City staff has discussed and believes the appropriate forum for this, given what may be proposed and the locations, is a meeting of the PZB. Further, hosting meetings can be interpreted to lie within the intent and purpose of the Board (Section 2-2-2 of the City Code), as the adopted PZB Rules of Procedure provide for “workshops.” Although the format is not prescribed, workshops are typically less formal and more flexible than a public hearing. Whoever presides over the workshop (i.e., Chair, Vice Chair) can outline the steps and set an agenda for what will occur. For example, all discussion and presentation *could* occur at the podium, similar to a regular meeting, or the developer could use easel boards and stations across the room to have multiple discussions about components of the projects. A key consideration is that ***no vote will be taken by the Board*** and also, neither the Board nor City are bound to the feedback provided. Nonetheless, staff believes a workshop will provide an optimal, upfront opportunity for community participation in the project and perhaps contribute to consensus-building and transparency from the early stages. The Rules of Procedure in Section 4.03 also allow for Special Meetings, or a meeting that might occur at a time or place different than Regular Meetings (2nd and 4th Tuesday of the month, City Hall). While staff recommends City Hall Council Chambers be the place, we suggest a different date, or special meeting, for the workshop. This will distinguish it from other development applications on a regular agenda. Staff will aid the Board in organizing and publicizing the meeting pursuant to the Open Meetings Act, and, less formally to “get the word out” to achieve the desired public participation.

PZB Action: Staff seeks feedback on potential format and Board availability for dates and times for a workshop.

Attachments

Attachment 1: Excerpt from Adopted Rules of Procedure

Section 3.03 Members. The PZB shall have seven Members, or such other number as may be established in the City Code. The Members shall be appointed to, serve on, and removed from, the PZB in the manner established in the City Code.

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Section 3.04 Officers. The PZB shall have a Chair, Vice Chair, and Secretary, all of whom shall be appointed as provided in the City Code. In the absence of the Chair and Vice Chair, the PZB shall elect a chair pro-tem to act as chair.

Section 3.05 Code of Ethics; Conflicts of Interest. The Members of the PZB shall be bound by the Code of Ethics set forth in Section 1-9-1. In addition, the Members of the PZB shall be bound by the Conflicts of Interest Provisions set forth in Article VIII below.

Article IV. MEETINGS

Section 4.01 Open Meetings Act. All meetings of the PZB shall be scheduled and held, and notice thereof shall be given, in the manner provided in the Open Meetings Act, 5 ILCS 120/1 et seq.

Section 4.02 Regular Meetings. PZB meetings shall be held on the second and fourth Tuesday of each month commencing at the hour of seven (7:00) P.M. in the City Council Chambers, and at such other times and places as may be called by the chair. The PZB shall establish a schedule of its meetings for each calendar year at the beginning of each calendar year in accordance with the requirements of the Open Meetings Act, 5 ILCS 120/2.03.

Section 4.03 Special Meetings. The PZB may conduct such special or emergency meetings as it determines may be necessary from time to time at the call of the Chair and in compliance with the Open Meetings Act, 5 ILCS 120/2.02.

Section 4.04 Workshops. The PZB may hold such workshop sessions as it determines may be necessary from time to time.

Section 4.05 Closed Meetings. The PZB may hold meetings or portions of meetings that are closed to the public, but only in compliance with the Open Meetings Act, 5 ILCS 120/2

Section 4.06 Quorum. No meeting of the PZB may be conducted without the presence of a quorum, which shall consist of a majority of the currently appointed PZB members.

Section 4.07 Attendance.

(a) A Member's unexcused absence without good cause shown from four PZB meetings in a six-month period shall be grounds for the Chair to recommend to the Mayor that the Member be removed.

(b) To help ensure the presence of a quorum at each meeting of the PZB, all Members of the PZB shall notify the Secretary, or his designee, if they are unable to attend any meeting of the PZB. Such notice shall be provided as far in advance as is practical under the circumstances.

| | | |
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| Case 23-013 -CU | 820-848 Lee | Conditional Use |
| Case 22-054-CU | 827 Elmhurst Rd | Conditional Use |
| Case 22-055- Appeal | 1378 Margret | Appeal |
| Case 23-012-TSUB | 930,946, & 970 North | Tentative Subdivision |

John Carlisle, CED Director stated that the Community Development Department is looking to work with the Planning and Zoning Board to hold Workshops when there are large construction project requests. The petitioner would be looking for general feedback. The workshop would be informative for the residents. The petitioner would bring the plan and the PZB would be the host and invite the residents. The City Manager likes the idea and thought the Planning and Zoning Board would be the best body to host the workshop. Ideas were discussed including presentations, displays, open communications, idea sharing and feedback. Communication ideas include creating a standalone website using Des Plaines communication channels to go to the whole community which includes social media, e-newsletter, and local access television.

For this workshop, May 9th is a regularly scheduled PZB meeting date which could be used since we do not have any cases scheduled. The PZB Chair would be the lead facilitator and the developer would put together the presentation. In terms of meeting style, the PZB provided recommendations including starting with an introduction, then break out into an open house style and then a wrap up which would include getting and giving feedback. The developer could pull together the comments and it could be part of the project file.

ADJOURNMENT

The next scheduled Planning & Zoning Board meeting is Tuesday April 25, 2023.

Chairman Szabo adjourned the meeting by voice vote at 8:55 p.m.

Sincerely,
 Margie Mosele, Executive Assistant/Recording Secretary
 cc: City Officials, Aldermen, Planning & Zoning Board, Petitioners



MEMORANDUM

Date: April 20, 2023
To: Planning and Zoning Board (PZB)
From: Jonathan Stytz, AICP, Senior Planner JS
Cc: John T. Carlisle, AICP, Director of Community and Economic Development
Subject: **Mannheim and Pratt Redevelopment (Northeast Corner) – Case #23-014-TSUB-CU**
Approval of a Tentative Plat of Subdivision and Conditional Uses for Drive-Through Facilities for a Restaurant Development at 2777 Mannheim Road in the C-3 District

Issue: Petitioner GW Properties is requesting the following for a proposed commercial redevelopment at 2777 Mannheim Road: (i) a Tentative Plat of Subdivision under Section 13-2-2 of the Subdivision Regulations to consolidate the existing lots into three lots of record; and (ii) a conditional use for with three drive-through facilities adjacent to residential .

Applicant/Petitioner: GW Properties, 2211 N. Elston Avenue, Suite 400, Chicago, IL 60614
Owner: Gus Sutter, 2777 Mannheim Road, Des Plaines, IL 60018
Case Number: 23-014-TSUB-CU
PINs: 09-33-108-012-0000; -013; -014; -022; -023
Ward: #6, Alderman Malcolm Chester (Alderman-elect Mark Walsten after May 1, 2023)
Existing Zoning: C-3 General Commercial District
Existing Land Use: Restaurant and Banquet Hall (Commercial)
Surrounding Zoning: North: C-3, General Commercial District
South: C-3, General Commercial District
East: R-1, Single Family Residential District
West: Commercial (Village of Rosemont)
Surrounding Land Use: North: Car wash (commercial)
South: Commercial restaurant and retail development under construction, also by GW Properties

East: Railroad; then Greco Avenue and single-family residences
West: Hotel (Commercial) in Village of Rosemont

Street Classification: Mannheim Road is an arterial road under IDOT jurisdiction; Pratt Avenue is a local road.

Comprehensive Plan : Commercial is the recommended use of the property.

Zoning/Property History: Based on City records, the subject property was annexed into the City in 1956. It was originally utilized as an office and warehouse building for Marland Oil Company until 1979, when the building was demolished and the site was cleared. In 1990, the footing and foundation was constructed for café La Cave, and a year later the restaurant/banquet hall was fully built out. The property has been utilized as a restaurant and banquet hall since.

Development Summary: Developer GW Properties, which is under construction for a multi-building restaurant-and-retail development on the *southeast* corner of Mannheim and Pratt (Outback Steakhouse, First Watch, Five Guys), is now also proposing a full redevelopment at the northeast corner—the former Café La Cave site. The proposed development is three new restaurants (“Class B” under the Zoning Ordinance) with indoor and outdoor seating and drive-throughs. The information for each proposed business is summarized below and described in detail in the attached Business Narratives.

- Guzman Y Gomez is a fast-casual restaurant proposed for the one-story stand-alone 2,850-square foot building positioned on the northern lot (Lot 1) of the commercial development. Their anticipated hours of operation are from 7:00 a.m. to 10:00 p.m. daily with five to ten employees on site at a given time. The building is designed with predominately brick material of varying colors, metal canopy structures, and an Exterior Insulation and Finish System (EIFS) accent.
- Cava is a fast-casual Mediterranean restaurant proposed for the one-story stand-alone 2,500-square foot building positioned in the center of the commercial development along Mannheim (Lot 2). Their anticipated hours of operation are from 10:45 a.m. to 10:00 p.m. daily with five to ten employees on site at a given time. The building is designed with predominately stucco material and finished wood accent.
- Raising Canes is a quick-service restaurant proposed for the one-story 3,300-square-foot building positioned on the southern lot (Lot 3) of the commercial development at the northeast corner of Mannheim and Pratt. Their anticipated hours of operation are from 9:30 a.m. to 1:30 a.m. Sunday through Thursday and 9:30 a.m. to 3:30 a.m. Friday to Saturday. There are expected to be eight to fifteen employees on site at a given time. The building is designed with predominately brick material of varying colors, finished wood accents, concrete masonry units, and metal canopy structures throughout.

Tentative Plat of Subdivision

Request Summary:

Overview

The subject property consists of five lots of record in the C-3 district totaling 2.39 acres, all under the address of 2777 Mannheim Road. The petitioner has requested a Tentative Plat, titled 2777 Mannheim Road Subdivision, to consolidate the existing five lots into three as shown on the attached Tentative Plat and detailed in the table below.

| Lot | Lot Type | Lot Width | Lot Depth | Lot Area |
|------------|-----------------|-------------------------------------|---------------------------------------|----------------------------|
| Lot 1 | Interior | 125 feet (west); 140 feet (east) | 208 feet (north); 244 feet (south) | 29,740 SF (0.68 acres); |
| Lot 2 | Interior | 123 feet (west); 126 feet (east) | 244 feet (north); 277 feet (south) | 31,835 SF (0.73 acres); |
| Lot 3 | Corner | 125 feet (west); 143 feet (east) | 277 feet (north); 288 feet (south) | 42,869 SF (0.98 acres) |

The subject property's unique shape is narrower on the north and gradually widens as it continues south. Thus, while the proposed lot widths are similar, the lot depths and areas increase substantially from proposed Lot 1 to proposed Lot 3. Nonetheless, all proposed lots meet the minimum lot depth requirement in Section 13-2-5.R of the Subdivision Regulations. Note that there are no lot width or lot area requirements for commercial districts.

Building Lines and Easements

The proposed subdivision shows the following building lines and easements: (i) a new 5-foot front building setback line along the west property line for all proposed lots where the proposed subdivision abuts Mannheim; (ii) a 25-foot rear building setback line along the east property line for all proposed lots of the subdivision; (iii) a five-foot side building setback line along the south of Lot 3, where the proposed subdivision abuts Pratt; and (iv) a new ten-foot-wide public utility and drainage easement extending throughout the development.

Subdivision Process, Required Public Improvements

Although the petitioner's request is for a Tentative Plat only at this time, the Board and public may benefit from understanding the requirements of a Final Plat, which is the second step in the subdivision approval process. The steps for Final Plat are articulated in Sections 13-2-4 through 13-2-8 of the Subdivision Regulations. In summary, the Final Plat submittal requires engineering plans that must be approved by the City Engineer, in particular a grading and stormwater management plan. Ultimately a permit from the Metropolitan Water Reclamation District (MWRD) will be required for construction. Tentative Plat approval does not require submittal of engineering plans. Regardless, the Department of Public Works and Engineering has provided brief comments (attached) based on the submittal. The Engineering review is more detailed for plans at the Final Plat stage, as those are accompanied by civil drawings.

Chapter 13-3 allows the City to require various right-of-way improvements based on criteria such as traffic and effect on adjacent properties. The attached Engineering memo explains that the project will require: (i) widening Pratt Avenue within existing right-of-way to allow a new turn lane from Pratt to Mannheim; and (ii) per IDOT comments, a widening of Mannheim Road for a center turn lane along the development’s west frontage, enabling left turns from southbound Mannheim to eastbound Pratt.

Conditional Uses

Request Summary:

Overview

The proposal includes three separate Class B restaurants, each with its own drive-through facility. “Restaurant, Class B” is a permitted use in the C-3 district. However, drive-through facilities on lots adjacent to residential properties require a conditional use permit. Although the properties are separated from the residential homes on Greco Avenue by both the Greco right-of-way and the railroad right-of-way, research indicated that in this area, specifically for the Starbucks at 2655 Mannheim, a conditional use for a drive-through was required by previous zoning administration. Further, the proposed drive-throughs are sited on the eastern portions of the properties, putting them closer to the residential lot lines than if they were on the other side of the lots or separated by a building or other development barrier (provided, however, that all of the drive-throughs would be well screened because of the railroad sound wall and the trees in the Greco right of way).

The table below summarizes the allowance of both uses identified above across all commercial districts (Section 12-7-3, Table 3, of the Zoning Ordinance; where P = Permitted Use and C = Conditional Use). As identified in Note 2, drive-through facilities are permitted only as an accessory use to a principal use, such as a restaurant. As the restaurant buildings will serve as the principal use on each lot for this development, this requirement is met.

| Use | C-1 | C-2 | C-3 | C-4 | C-5 | C-6 | C-7 |
|---|-----|-----------------|----------------|-----|-------------------|-----|----------------|
| Drive-Through Facility (located adjacent to residential use) | | C ¹³ | C ² | C2 | C ^{2,10} | | |
| Restaurant, Class B | | P ²¹ | P | P | P | | P ⁶ |

Notes:

- 2. When an accessory use only.
- 6. When incorporated within, or accessory to, an office/hotel use.
- 10. Except on Miner Street, Ellinwood Street or Lee Street.
- 13. When an accessory use to a financial institution.
- 21. When the total space/use is up to 2,500 square feet and is accessory to an office building/hotel.

All three lots will abut Mannheim on the west and the railroad tracks on the east. However, Lot 3 will also abut Pratt to the south. As a result, the designated front yard for all three lots will be the west property line along Mannheim, making the east property line the rear yard, and the north and south property lines the side yards.

Building Design Standards

All new construction must adhere to Section 12-3-11 of the Zoning Ordinance, which specifically focuses on transparency and exterior building materials. Plans submitted at this time show each of the three buildings will meet the exterior building material standards. However, the transparency regulations that will need to be addressed at time of building permit, or the proper relief would need to be obtained.

Proposed Site Plan

The attached Site Plan identifies the proposed improvements for all three lots in relation to access, circulation, parking, building footprints, outdoor seating areas, dumpster enclosures, and drive-through stacking. Note that the Site Plan refers to Lot 1 as Lot C, Lot 2 as Lot B, and Lot 3 as Lot A. For consistency, this report will refer to the proposed lots by their number on the Tentative Plat.

- **Access:** The subject property currently contains three access points (driveway curb-cuts)—two from Mannheim and one from Pratt. The proposal alters the site access by removing the northernmost curb-cut off Mannheim and adding a second curb-cut on Pratt. On the Pratt side, the changes align with curb-cuts for the under-construction commercial development at the southeast corner of the Mannheim-Pratt intersection. On Mannheim, the plan removes a curb-cut that is close to another; removing curb-cuts is generally viewed as a best practice. Lots 2 and 3 will have direct access onto Mannheim or Pratt. However, access to Lot 1 will require access through either Lot 2 or 3 via a cross-access easement. It is also important to note that the proposed changes on the access from Mannheim will require an Illinois Department of Transportation (IDOT) permit. IDOT's comments related to the proposed development are summarized in the *Traffic Study and IDOT Comment* section.
- **Circulation:** The lots in the proposed development are designed to be cohesive and connected so that motorists and pedestrians can pass through any of the lots to reach their destination. Each lot contains a 24-foot-wide east-west two-way drive aisle for circulation and parking access on the individual lots. However, two 24-foot-wide north-south two-way travel drive aisles are also proposed not only to provide direct access to additional parking spaces but also to provide direct connections to the other lots. The proposed drive aisle widths exceed the 22-foot-minimum-width requirement in Section 12-9-6.B of the Zoning Ordinance.
- **Drive-Through Stacking:** Given that all three restaurants will utilize drive-through facilities in their operations, appropriate drive-through stacking spaces are required. Section 12-9-4 of the Zoning Ordinance requires that all drive-through facilities provide a minimum of six stacking spaces per facility plus one stacking space per waiting area provided as part of the facility. The attached Site Plan indicates that all three restaurants will contain a single drive-through facility with seven or more stacking spaces provided in conformance with this section.

- Parking:** Ninety-degree off-street standard and accessible parking spaces are provided for each lot as identified in the table and illustrated on the attached Site Plan. Under Section 12-9-7 of the Zoning Ordinance, Class B restaurants are required to provide one parking space for every 50 square feet of net floor area, or one parking space for every four seats, whichever is greater, plus one parking space for every three employees. Sections 12-9-6.B and 12-9-8 of the Zoning Ordinance require a minimum of 8.5 feet in width and 18 feet in depth for standard spaces and a minimum of 16 feet in width and 18 feet in depth for accessible spaces. The attached Site Plan indicates that the proposed parking spaces met or exceed these requirements.

| Off-Street Parking | Required Spaces | Provided Spaces |
|------------------------|--|--|
| Lot 1 (Guzman Y Gomez) | 22 standard spaces; 2 accessible spaces | 29 standard spaces; 2 accessible spaces |
| Lot 2 (Cava) | 24 standard spaces; 2 accessible spaces | 38 standard spaces; 2 accessible spaces |
| Lot 3 (Raising Canes) | 23 standard spaces; 2 accessible spaces | 45 standard spaces; 2 accessible spaces |

- Building Footprints and Setbacks:** The attached Site Plan and respective civil plans for Lots 1-3 (also attached), identify the positioning and dimensions of the proposed building on each lot. Section 12-7-3.L of the Zoning Ordinance requires that commercial properties meet certain bulk controls as identified in the table, which are met by the proposed development. Note that there are no building coverage or lot coverage restrictions for the commercial districts and that a larger building setback distance is required for the rear (east) property line because it is adjacent to residences.

| C-3 District Bulk Controls | Required | Proposed Buildings | | |
|---|---------------------------|--------------------|----------|----------|
| | | Lot 1 | Lot 2 | Lot 3 |
| Maximum height | 45 Feet | 28 Feet | 19 Feet | 23 Feet |
| Minimum front yard (Adjacent Other) | 5 Feet | 89 Feet | 95 Feet | 64 Feet |
| Minimum side yard (North, Adjacent Other) | 5 Feet if abutting street | 31 Feet | 33 Feet | 28 Feet |
| Minimum side yard (South, Adjacent Other) | 5 Feet if abutting street | 54 Feet | 57 Feet | 62 Feet |
| Minimum rear yard (Adjacent residential) | 25 Feet | 51 Feet | 114 Feet | 146 Feet |

- **Outdoor Seating Areas:** Each of the three proposed restaurant developments include an outdoor seating area facing Mannheim as identified in the table below. Note that the outdoor seating areas are not factored into the required off-street parking calculation.

| Outdoor Seating | Lot 1 | Lot 2 | Lot 3 |
|--------------------------------|-----------|-----------|-----------|
| Seating Area (SF) | | | |
| • Indoor | ~918 SF | ~1,013 SF | ~900 SF |
| • Outdoor | ~522 SF | ~383 SF | ~157 SF |
| • Total | ~1,440 SF | ~1,396 SF | ~1,056 SF |
| Seating Area (number of seats) | | | |
| • Indoor | ~50 seats | ~44 seats | ~36 seats |
| • Outdoor | ~15 seats | ~13 seats | ~7 seats |
| • Total | ~65 seats | ~57 seats | ~42 seats |

- **Pedestrian access to public sidewalks:** Lot 3 (Raising Canes) has two sidewalk connections through the parking lot, one to each Mannheim and Pratt sidewalk. However, Lots 1 and 2 do not. A recommended staff approval condition is that these connections be added. Because the parking proposed around all three uses would exceed the required minimums, a loss of potentially 3-4 standard spaces would not create a deficiency, either practical or code, in staff’s opinion.
- **Dumpster Enclosures:** Dumpsters and their respective enclosures have been proposed for all three lots, positioned towards the back near the drive-through entrances. Section 12-10-11 of the Zoning Ordinance requires all dumpsters that are stored outside to be improved with a four-sided enclosure constructed of a solid wood or masonry fence a minimum of six feet—but no more than eight feet—in height. Elevation drawings will be required at time of building permit to ensure that all requirements are met for the dumpster enclosures.

Photometric Plan

The proposed development involves the installation of new exterior lighting, which must comply with the environmental and performance standards for lighting in Section 12-12-10 of the Zoning Ordinance summarized below. Based on the attached Photometric Plans, the maximum horizontal foot-candles given off by the neighboring property as measured at the abutting property line for all proposed lots meets this requirement.

| Districts | Maximum Foot-Candles Allowed | Highest Proposed Measurement at Property Line (in foot-candles) | | |
|---------------------------|------------------------------|---|--|--|
| | | Lot 1 | Lot 2 | Lot 3 |
| Single Family Residential | 0.1 | 0.0 (East) | 0.0 (East) | 0.0 (East) |
| Commercial | 2.0 | 0.4 (North); 1.8 (South); 1.0 (West) | 1.8 (North); 1.9 (South); 0.3 (West) | 1.2 (North); 1.5 (South); 1.6 (West) |

Traffic Study and IDOT Comments

The petitioner provided the attached Traffic Study from KLOA to analyze the anticipated impact the proposed development would have on traffic and the surrounding roadway network. Overall, the study concluded: (i) the proposed development will be consistent and compatible with existing traffic volumes and patterns in the area; (ii) there is no substantial net new traffic generated by the development; (iii) the internal circulation allows for adequate access and dispersion of traffic entering, exiting, and navigating the development; and (iv) that wayfinding, stop, and do not enter signs be installed to direct traffic.

This study did not recommend any roadway improvements on either Pratt or Mannheim. However, the Public Works and Engineering (PWE) department determined that a three-lane cross-section shall be provided Pratt to facilitate traffic flow and prevent back-up from west to east, toward the railroad tracks. The petitioner's attached Civil and Site Plans illustrate this requirement.

In addition, IDOT has required that Mannheim is widened in order to add a left-turn lane into the development for both curb-cuts off Mannheim. The petitioner is working with the City to address this comment, with a final resolution likely decided by the time of Final Plat submittal. Nonetheless, staff recommends an approval condition at this stage that the final development plans satisfy all permitting requirements of relevant agencies, specifically IDOT and the City.

Conditional Use Findings: Conditional Use requests are subject to the standards set forth in Section 12-3-4(E) of the Zoning Ordinance. Rationale for how the proposed amendments would satisfy the standards is provided below and in the attached petitioner responses to standards. The Board may use the provided responses as written as its rationale, modify, or adopt its own.

1. The proposed Conditional Use is in fact a Conditional Use established within the specific Zoning district involved:

Comment: The proposed development includes three Class B restaurants with drive-through facilities. A drive-through facility is a Conditional Use, as specified in Section 12-7-3.K of the Zoning Ordinance, for properties in the C-3 General Commercial District.

PZB Additions or Modifications (if necessary): _____

_____.

2. The proposed Conditional Use is in accordance with the objectives of the City's Comprehensive Plan:

Comment: The Comprehensive Plan illustrates this property as commercial. The Comprehensive Plan strives to foster growth and redevelopment of existing commercial corridors to retain existing businesses in Des Plaines. The subject property is large, and the existing restaurant and banquet hall is vacant after the closing of Café La Cave. This provides a prime opportunity for the redevelopment of the site. The proposed restaurant development with drive-through facilities meets this intent of the Comprehensive Plan while also repurposing a vacant space along a major commercial corridor in Des Plaines.

PZB Additions or Modifications (if necessary): _____

_____.

3. The proposed Conditional Use is designed, constructed, operated and maintained to be harmonious and appropriate in appearance with the existing or intended character of the general vicinity:

Comment: The property is located on a commercially-zoned property along an established commercial corridor in Des Plaines, and is surrounded on all sides by commercial development except the east where the Metra railroad and residences are located. The buildings and related drive-through facilities in the proposed development are designed to be harmonious and consistent with existing commercial developments along this corridor, many of which contain drive-through facilities and are adjacent to residential development to the east.

PZB Additions or Modifications (if necessary): _____

_____.

4. The proposed Conditional Use is not hazardous or disturbing to existing neighboring uses:

Comment: The existing property contains a restaurant and banquet hall, of which all activities occur inside the building. While the subject property will be redeveloped for three separate restaurant uses and drive-through queuing will occur at the rear of the property, the primary use on the subject property will remain commercial. In addition, the railroad located directly east of the subject property provides a separate buffer in between the proposed development and existing residences.

PZB Additions or Modifications (if necessary): _____

_____.

5. The proposed Conditional Use is to be served adequately by essential public facilities and services, such as highways, streets, police and fire protection, drainage structures, refuse disposal, water and sewer, and schools; or, agencies responsible for establishing the Conditional Use shall provide adequately any such services:

Comment: The existing restaurant and banquet hall is adequately served by three curb-cuts (two off Mannheim and one off Pratt). The proposed development proposes closing one of the existing curb-cuts on Mannheim and adding one on Pratt, maintaining three total curb-cuts for access. All curb-cuts are designed to be full-access. However, additional public improvements, including roadway widening on Pratt and Mannheim, will be necessary to obtain IDOT and City right-of-way permitting approval. The subject property is adequately served by essential public facilities and services, which the proposed development will not alter. However, the attached Tentative Plat identifies the new utility connections and easements proposed for the development.

PZB Additions or Modifications (if necessary): _____

_____.

6. The proposed Conditional Use does not create excessive additional requirements at public expense for public facilities and services and will not be detrimental to the economic well-being of the entire community:

Comment: The proposed drive-through facilities and restaurants overall will provide a net economic benefit for the City, residents, and visitors by providing additional services and tax revenue over the long term.

PZB Additions or Modifications (if necessary): _____

_____.

7. The proposed Conditional Use does not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property, or the general welfare by reason of excessive production of traffic, noise, smoke fumes, glare or odors:

Comment: Although drive-through facilities inherently consist of auto traffic and require vehicle turn movements, the attached Traffic Study points out that instead of generating new traffic, the businesses are likely to capture existing traffic to be their customers. Without question, the traffic counts in the area—Mannheim Road, close to O’Hare and Allstate Arena—are part of what makes the site appealing to the potential tenants. However, the Public Works and Engineering Department comments in its attached memo that it does not believe the peak traffic volumes from Allstate Arena events have been fully modeled and analyzed. Nonetheless, the required public improvements—specifically the turn lanes on both Mannheim and Pratt—are intended to control and manage traffic demand and should be adequate to serve the development. In addition, the design of the restaurant lots, location of the proposed drive-through facilities, and provided landscaping/screening on the subject property as a whole, will effectively reduce the production of noise, smoke fumes, glare, and odors generating from this use. The proposed public improvements, especially roadway widenings, will assist in managing and circulating traffic volumes throughout the site.

PZB Additions or Modifications (if necessary): _____

_____.

8. The proposed Conditional Use provides vehicular access to the property designed so that it does not create an interference with traffic on surrounding public thoroughfares:

Comment: The drive-through facilities are designed and positioned on the rear of each lot with the intention of minimizing any inference with off-street parking and drive aisles throughout the site. In addition, the public improvements included with this proposal will further minimize traffic inference on surrounding roadways by providing left turn lanes off Mannheim and Pratt.

PZB Additions or Modifications (if necessary): _____

_____.

9. The proposed Conditional Use does not result in the destruction, loss, or damage of natural, scenic, or historic features of major importance:

Comment: The subject property is already developed with a large restaurant and banquet hall. As such, the proposed redevelopment will not result in the destruction, loss, or damage of any natural, scenic, or historic features of the site. In fact, the attached Landscape Plans indicate that additional natural features, such as trees, shrubs, and perennials, will be installed throughout the subject property, where there is currently minimal landscaping present.

PZB Additions or Modifications (if necessary): _____

_____.

10. The proposed Conditional Use complies with all additional regulations in the Zoning Ordinance specific to the Conditional Use requested:

Comment: The proposed drive-through facilities will meet all other requirements of the Zoning Ordinance for the C-3 General Commercial District provided all operational and physical (i.e. installation or construction-related) conditions are met.

PZB Additions or Modifications (if necessary): _____

_____.

PZB Procedure and Recommended Condition: Given the separate conditional use and tentative plat requests, the PZB shall take two motions. First, pursuant to Section 13-2-3 of the Subdivision Regulations, the PZB may vote to approve, approve with conditions, or deny the Tentative Plat of Subdivision.

In regard to the conditional use request, the PZB may vote to *recommend* approval, approval with conditions, or denial of the conditional use for the drive-through facilities. If the PZB chooses to recommend approval for the conditional use request, staff recommends the following conditions.

Recommended Conditions of Approval:

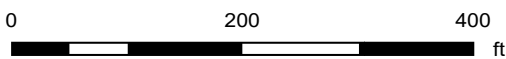
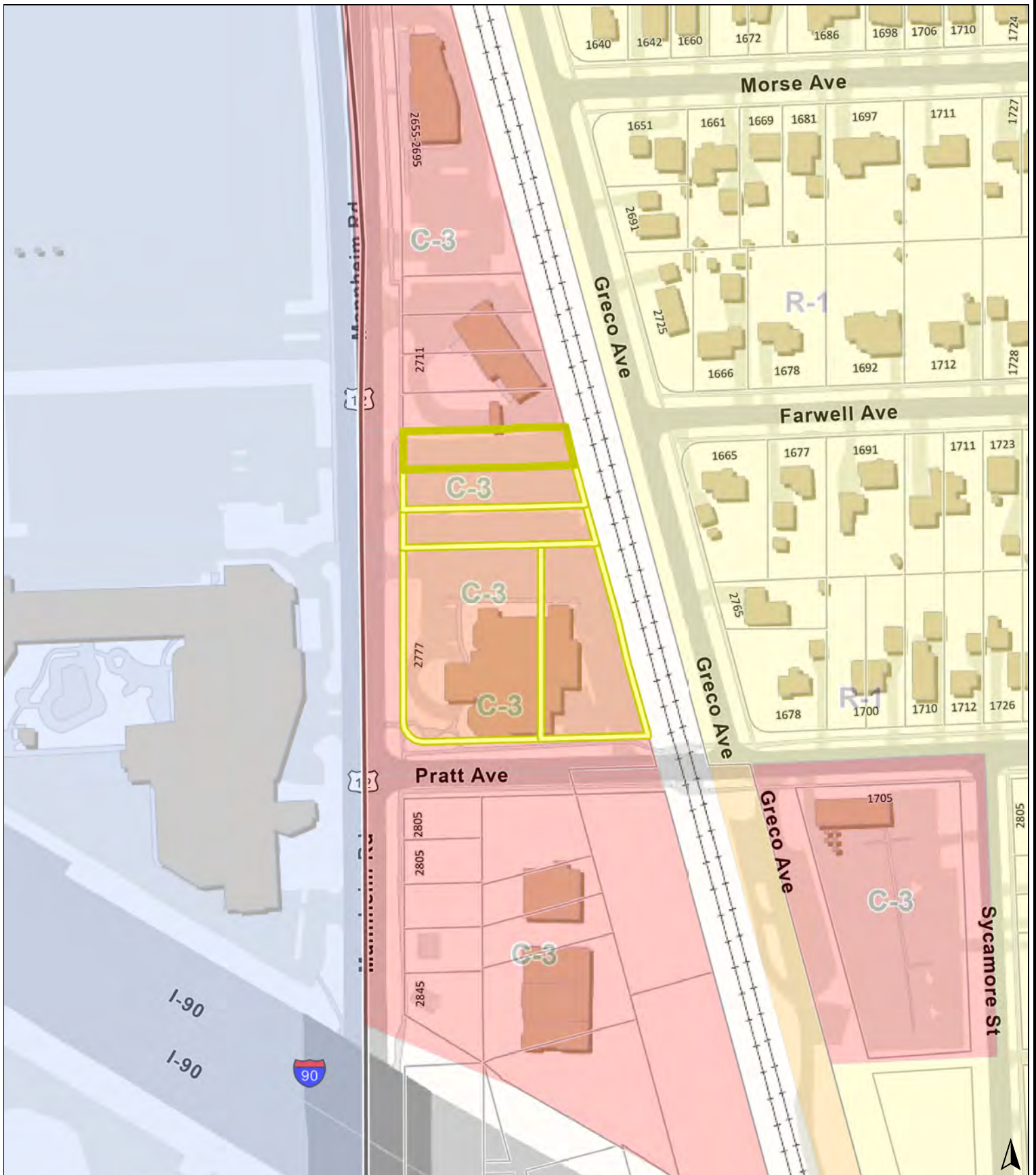
1. All proposed ground- and building-mounted signs must comply with all provisions of Section 12-11, or the petitioner must obtain necessary relief, such as but not limited to variation or approval of a conditional use for localized alternative sign regulations (LASR).
2. All proposed buildings must comply with all provisions of Section 12-3-11, or the petitioner must obtain necessary relief, such as a variation.

3. A lighting plan labeling all building-mounted and freestanding light fixtures and providing photometric details must be submitted and approved with the building permit.
4. Grading/drainage and other on-site infrastructure details are provided to the satisfaction of the Public Works and Engineering Department with the submission of the Final Plat of Subdivision.
5. Final engineering plans are prepared to fulfill requirements of the Public Works and Engineering Department and IDOT, and to illustrate all required public improvements, including but not limited to: (i) widening of Mannheim Road and installation of a left-turn lane for access onto the proposed development and Pratt Avenue pursuant to IDOT requirements; (ii) Pratt Avenue widening to accommodate both left- and right-turn lanes to Mannheim; and (iii) water main replacement on the west side of the development.
6. Marked pedestrian paths between the buildings and public sidewalk shall be provided for the buildings on Lots 1 and 2.
7. Commercial off-site parking, such as parking for Allstate Arena events or O'Hare travelers, shall not be permitted.

Attachments:

- Attachment 1: Location/Zoning Map
- Attachment 2: Site and Context Photos
- Attachment 3: ALTA/NSPS Land Title Survey
- Attachment 4: Responses to Standards for Conditional Uses
- Attachment 5: Project Narrative
- Attachment 6: Business Narratives
- Attachment 7: Site Plan
- Attachment 8: Floor Plans
- Attachment 9: Elevation and Signage Plans
- Attachment 10: Civil Plans
- Attachment 11: Photometric Plans
- Attachment 12: Landscape Plans
- Attachment 13: Nicor "Will-Serve" Letter
- Attachment 14: Tentative Plat of Subdivision
- Attachment 15: Engineering Memo
- Attachment 16: Traffic Impact Study without appendices¹

¹ Full copy available upon request to staff.



Print Date: 4/21/2023

Notes

Disclaimer: The GIS Consortium and MGP Inc. are not liable for any use, misuse, modification or disclosure of any map provided under applicable law. This map is for general information purposes only. Although the information is believed to be generally accurate, errors may exist and the user should independently confirm for accuracy. The map does not constitute a regulatory determination and is not a base for engineering design. A Registered Land Surveyor should be consulted to determine precise location boundaries on the ground.



1773 Webster Ln – Public Notice and Front of Property



1773 Webster Ln – Looking Southeast at Existing Building



1773 Webster Ln – Looking North at Rear of Property



1773 Webster Ln – Looking East at Existing Parking Area

STANDARDS FOR CONDITIONAL USES

The Planning and Zoning Board and City Council review the particular facts and circumstances of each proposed Conditional Use in terms of the following standards. Keep in mind that in responding to the items below, you are demonstrating that the proposed use is appropriate for the site and will not have a negative impact on surrounding properties and the community. Please answer each item completely and thoroughly (two to three sentences each).

1. The proposed conditional use is in fact a conditional use established within the specific zoning district involved;

Response: The proposed conditional uses (restaurant with drive-thru or pickup window) is a conditional use established within the C-3 zoning district.

2. The proposed conditional use is in accordance with the objectives of the city's comprehensive plan and this title;

Response: To the best of our knowledge, we are proposing to comply with all objectives of the City's Comprehensive Plan, associated with Lots 1 and 2.

3. The proposed conditional use is designed, constructed, operated, and maintained so as to be harmonious and appropriate in appearance with the existing or intended character of the general vicinity;

Response: The proposed development is in close vicinity to other drive-thru restaurants, such as Starbucks, McDonald's, and Culvers along Mannheim Road. We're merely fitting in with current uses of the area. We will be upgrading the landscaping and beauty of the site for visual improvement. The proposed uses will allow for more convenience to the community. The operations will be handled on site in order to avoid complications with other neighbors / adjacent sites.

4. The proposed conditional use is not hazardous or disturbing to existing neighboring uses;

Response: The proposed developments are replacing a similar use. The proposed developments are in close proximity to other restaurants such as Starbucks, Potbelly, Culvers, McDonald's, and Which Wich along Mannheim Road. The proposed conditional uses will not be hazardous or disturbing to existing neighboring uses.

5. The proposed conditional use is to be served adequately by essential public facilities and services such as highways, streets, police and fire protection, drainage structures, refuse disposal, water and sewer, and schools; or the persons or agencies responsible for the establishment of the proposed conditional use shall provide adequately any such services;

Response: The property is served by all necessary utilities via public utility main lines on the adjacent roads. This is evident by both the ALTA/TOPO survey as well as the existing operational Cafe La Cave that was previously developed on this property. This site provides direct access to Pratt Avenue.

6. The proposed conditional use does not create excessive additional requirements at public expense for public facilities and services and not be detrimental to the economic welfare of the community;

Response: The proposed developments will not require any additional public facilities or services at public expense. The property will be served by existing public facilities and services. The proposed conditional uses will not be detrimental to the economic welfare of the community.

7. The proposed conditional use does not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property, or the general welfare by reason of excessive production of traffic, noise, smoke, fumes, glare or odors;

Response: The Raising Cane's drive-thru and CAVA pickup window will promote a safe environment for customers to place and pick up their orders. The site design is oriented to provide safe and efficient traffic circulation for the public.

8. The proposed conditional use provides vehicular access to the property designed that does not create an interference with traffic on surrounding public thoroughfares;

Response: Similar to existing conditions, the proposed site will provide direct access to Pratt Avenue. The proposed site layout is designed to maximize drive-thru and pickup stacking on site which will minimize traffic congestion in the public streets. Double drive-thru lanes are also utilized to help minimize site congestion.

9. The proposed conditional use does not result in the destruction, loss, or damage of a natural, scenic, or historic feature of major importance; and

Response: The site is currently occupied by the Cafe La Cave restaurant, along with associated parking and landscaping. The proposed conditional uses do not result in the destruction, loss, or damage of a natural, scenic, or historic feature of major importance.

10. The proposed conditional use complies with all additional regulations in this title specific to the conditional use requested

Response: To the best of our knowledge, the proposed conditional uses comply with all additional regulations specific to the conditional uses requested.



February 16, 2023

City of Des Plaines
1420 Miner Street
Des Plaines, IL 60016

RE: 2777 Mannheim Road, Des Plaines – Project Narrative

Dear Village of Des Plaines,

GW Properties is proposing a new development on the Southwest corner of Mannheim Road and Pratt Street in Des Plaines, Illinois. The site would include a 3,000sf free standing quick service restaurant with drive-thru, a 3,500sf building with a pick-up window that can accommodate an additional restaurant and/or retail tenant and finally a 2,850sf free standing quick serve restaurant building with a drive-thru. Our complete development application provides the finding of facts with regard to our site plan, conditional uses and variance requests.

The proposed project if approved would be designed and constructed per the development site plan as submitted. Upon completion, the project would be subdivided into three lots to allow the proposed buildings to be on its own legal lots.

Furthermore, we have spoken with our traffic consultant, KLOA, although not directly with IDOT, in order to determine where to locate the curb cuts for our development. After various discussions with KLOA, we believe that the access drives as shown would be acceptable to IDOT, but still needs further review. Ultimately, all development activities would be completed in accordance with the City of Des Plaines, IDOT and all other governmental/municipal requirements.

This property would ultimately be transformed from its current state of a banquet hall into a vibrant development with several new businesses and offerings that will be complementary to the surrounding area. This project would create well over a dozen of new full-time jobs in Des Plaines and generate hundreds of thousands of dollars in property and sales tax for the City on an annual basis that do not exist today. This project entails the highest and best use for the property and is a rare opportunity to create one cohesive development that has been thoughtfully designed and considered.

We look forward to working with the City of Des Plaines on all aspects of the proposed development and welcome the opportunity to discuss it soon. If you have any questions, please feel free to contact me.

Regards,

A handwritten signature in black ink that reads 'Mitch Goltz'.

Mitch Goltz
Principal
GW Properties



Guzman Y Gomez Project Description Mannheim Road & Pratt Avenue in Des Plaines, IL

Guzman Y Gomez is an Australian multinational Mexican-style restaurant chain. Founded in 2009, Guzman Y Gomez has expanded to 151 locations throughout Australia, Singapore, Japan, and the United States. The franchise is Australia's fastest-growing QSR company, serving authentic, freshly made-to-order Mexican-style cuisine with a relevant menu of burritos, bowls, tacos, and salads as well as a variety of alcoholic beverages. The restaurants contemporary décor delivers a Latin pop aesthetic with vibrant colored artwork, Latin mixed music, and dim lighting. For those looking to eat on-the-go, Guzman Y Gomez offers drive through lanes and pick up stations. In 2021 the company launched "GYG Mobile", a proprietary mobile app that allows for faster, contact-free ordering as well as a loyalty program which provides users with added incentives. The franchise has received numerous accolades for its success including "Best Multi-Site Restaurant of the Year" by QSR Media and "Best Customer Service Award in QSR" by Roy Morgan Research.

The GYG structure will be approximately up to 2,850 square feet with a patio for outdoor dining and will be located on Lot C of the proposed development. The main building will utilize various materials to articulate a dynamic visual appearance, but be complimentary to our neighbors.

The daily hours are 7:00 am - 10 pm, with 5 to 10 team members on site at any given time.



CAVA Project Description

Mannheim Road & Pratt Avenue in Des Plaines, IL

CAVA is a fast casual restaurant that was founded in 2010, by its founders, Ike Grigoropoulos, Chef Dmitri Moshovitis, and Ted Xenohristos. Cava is grounded in the Mediterranean way of life, where food is at the center of culture and connection.

Ike Grigoropoulos, Chef Dmitri Moshovitis, and Ted Xenohristos cared deeply about creating restaurants that paid homage to the Mediterranean traditions they grew up with, where food equals connection, and what's good for you is also what tastes good too. However, it is not only about the food, though. CAVA's inspiration is rooted in the entire Mediterranean meal experience, notably that feeling-so-cared-for genuine expression of hospitality and warmth. Which is why, at CAVA, generosity is our cornerstone. In fact, we open our doors one day early to fee lunch and dinner to anyone in the community, free of charge. We are eager to meet our neighbors and welcome all to share a meal with us. Additionally, during Community Days, we also partner with and sponsor a local nonprofit organization.

At CAVA we serve delicious, healthy, and affordable meals that can be individually customized and ordered ahead of time so that customers can easily pickup their meal through our pickup window. Ultimately, this pickup window allows for our customers to have safe and easy access to our food without the need for menu boards or waiting in a line.

The CAVA structure will be approximately up to 3,500 square feet with a patio canopy for outdoor dining and will be located in the middle lot of the proposed development. The main building will utilize various materials to articulate a dynamic visual appearance, but be complimentary to our neighbors.

The daily hours are 10:45 am - 10 pm, with 5 to 10 team members on site at any given time.



Raising Cane's Project Description Mannheim Road & Pratt Avenue in Des Plaines, IL

Raising Cane's is a quick serve restaurant that was founded in 1996 in Baton Rouge, LA by its founder, Todd Graves. The restaurant has ONE LOVE, which is chicken, as evident by the minimal menu that includes: chicken fingers, fries, coleslaw and Texas toast. The food is always cooked to order to ensure the best quality and flavor possible.

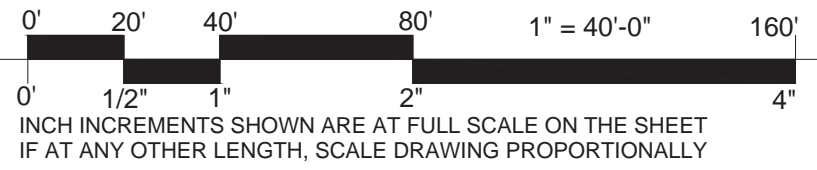
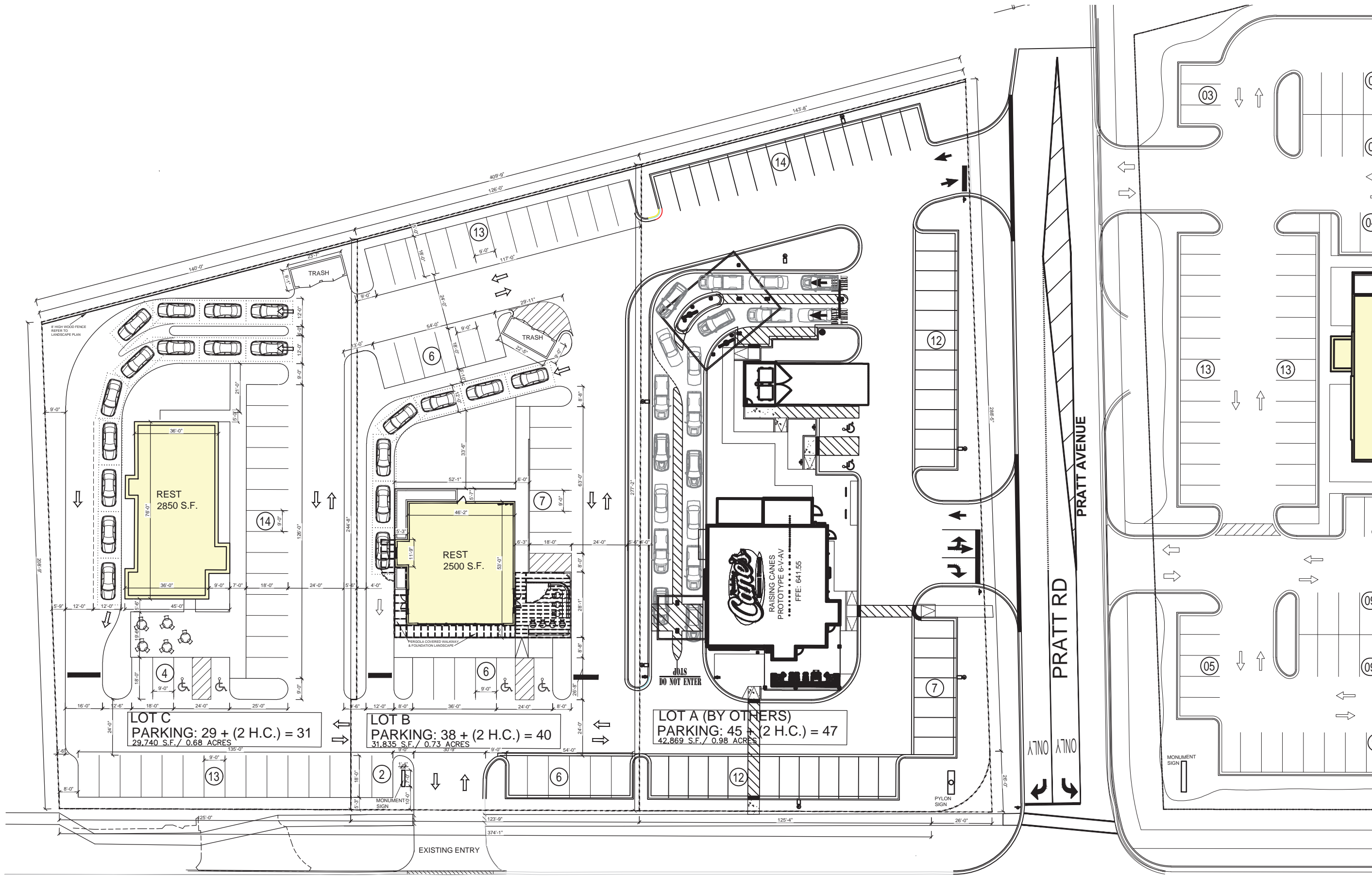
Raising Cane's is not just another 'chain' restaurant. It is part of the community. The restaurant not only employs people from the area but gives back to the community by promoting education, feeding the hungry, pet welfare, local community involvement and more.

The building itself promotes visual esthetics with the variations not only in the architecture, but the building materials themselves. All of this with the variations in the heights of the building, along with the ample landscaping around the building provides an aesthetically pleasing structure that accents the development unlike the fast food 'box' style of many businesses.

The structure is approximately 3,300 square feet with a patio canopy for outdoor dining. The main building will utilize various materials, such as brick and composite paneling, to articulate a dynamic visual appearance. The primary height of the building is 20'-6" from the finish floor.

Raising Cane's will have approximately 55 employees at this location, with an average of 8-15 employees per shift. The restaurant will be open Sun - Thu: 9:30am - 1:30am; Fri - Sat: 9:30am – 3:30am. The drive thru and outdoor patio are proposed to have the same hours of operation.

Raising Cane's will be the southern-most lot of an overall development that is being led by GW Properties. The building plans will be submitted for permitting by Raising Cane's after entitlement approvals with an anticipated construction start date of October 2023 and an anticipated opening date of June 2024.



MANNHEIM RD

SITE STUDY - MULTI TENANT

APPROX. TOTAL LOT AREA: 104,444 SF / 2.4 ACRES
 SCALE: 1" = 40'



SITE STUDY - MULTI TENANT
GW PROPERTIES
MANNHEIM & PRATT
DES PLAINES
 DESIGN STUDIO 24 LLC
 ARCHITECTS
 2211 N. Elston Ave. Chicago, IL 60614
 Tel.: 847.885.7751 Fax: 847.885.9300

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CAUTION: Contractor to verify all existing conditions, dimensions, etc. PRIOR to bid and construction. Contractor to notify owner and architect of any discrepancies. No site of ignorance to existing conditions found to be different from drawings will be accepted.

CAUTION: This drawing may have been reproduced at a size different than originally drawn. The owner and architect assume no responsibility for the use of incorrect scale. Drawings are NOT to be scaled. Not published. All rights reserved. Notify architect of any discrepancies.

STATEMENT OF COMPLIANCE
 I have prepared, or caused to be prepared under my supervision, the attached plans and specifications and state that, to the best of my knowledge and belief and to the extent of my contractual obligation, they are in compliance with the Codes and Ordinances of DES PLAINES, ILLINOIS

STATEMENT OF COMPLIANCE
 I have prepared, or caused to be prepared under my supervision, the attached plans and specifications and state that, to the best of my knowledge and belief and to the extent of my contractual obligation, they are in compliance with the Environmental Barriers Act (Ill. Rev. Stat. 1985, ch. 111 1/2, pars. 3711 as amended) and the Illinois Accessibility Code, 111 ILCS Code 406.

| LAST UPDATED | REVISIONS |
|--------------|-----------|
| 04.10.2023 | |

SCALE AS SHOWN
 DRAWN BY: MITESH PATEL

MARK T. DiGANCI
 REG. ARCHITECT

ST1.0
 REST & RETAIL OPT
 APR 2023

GEN. CONSTRUCTION PLN. NOTES:

- ALL DIMENSIONS ARE FROM FACE OF GYPSUM BOARD, UNLESS OTHERWISE NOTED
- ALL GYPSUM BOARD USED SHALL BE 1/2" OR 5/8" TYPE "X" FIRE TAPED. DEMISING PARTITIONS SHALL BE OF FIRECODE "X" SHEETROCK, TAPED AND SEALED TO DECK ABOVE.
- ALL PLYWOOD, PLYWOOD BACKING, PARTICLE BOARD, WOOD BLOCKING, AND FRAMING USED SHALL BE FIRE RETARDANT AND STAMPED NON-COMBUSTIBLE.
- PLACE AND FINISH ALL NEW MATERIALS SO AS TO PROVIDE A SMOOTH AND INTEGRAL TRANSITION TO ADJACENT FINISHES AND MATERIALS.
- PROVIDE ADDITIONAL FRAMING / BRACING FOR ALL WALL MOUNTED EQUIPMENT. CHECK CUTSHEETS OF ALL EQUIPMENT PRIOR TO INSTALLATION.
- PROVIDE FIRE EXTINGUISHER(S) PER LOCAL CODE REQUIREMENTS. ALL FIRE EXTINGUISHERS SHALL BE STATE CERTIFIED AND BE PROPERLY MOUNTED SO ALL OPERABLE PARTS ARE A MAXIMUM OF 48" AFF. ALL PROPERLY MOUNTED FIRE EXTINGUISHERS SHALL NOT EXCEED 75' OF TRAVEL DISTANCE APART. FIRE EXTINGUISHERS SHALL BE LOCATED WHERE THEY ARE READILY ACCESSIBLE AND IMMEDIATELY AVAILABLE IN THE EVENT OF A FIRE. FIRE EXTINGUISHERS SHALL BE LOCATED ALONG NORMAL PATHS OF TRAVEL, INCLUDING EXITS FROM AREAS. FIRE EXTINGUISHERS SHALL NOT BE OBSTRUCTED OR OBSCURED FROM VIEW.
- SEE MECHANICAL AND ELECTRICAL SHEETS FOR ALL FIXTURE SPECIFICATION, WIRING, AND POWER REQUIREMENTS.
- FLOORS IN FOOD PREPARATION AREA, DISH WASHING AREA, SERVICE AREA, JANITORIAL AREA, STORAGE AREA, AND ANY AREA WHERE EQUIPMENT IS PLACED SHALL BE SMOOTH, EASILY CLEANABLE, WASHABLE, DURABLE, AND OF COMMERCIAL GRADE MATERIAL AND SHALL HAVE A MINIMUM 3/8" RADIUS INTEGRAL COVE BASE EXTENDING AT LEAST 4" UP THE WALL. GROUT SPACING FOR TILE SHALL NOT EXCEED 1/4" AND SHALL BE SEALED.
- FLOOR SINKS SHALL BE INSTALLED FLUSH WITH THE FLOOR SURFACE AND BE LOCATED SO AS TO BE READILY ACCESSIBLE FOR CLEANING AND REPAIR.
- ALL EXITS SHALL BE OPERABLE FROM INTERIOR OF BUILDING WITHOUT THE USE OF A KEY OR SPECIAL KNOWLEDGE OR EFFORT. REPLACE NON-COMPLIANT HARDWARE.
EXCEPTION: MAIN ENTRY / EXIT DOORS MAY BE KEY OPERATED FROM INTERIOR IF SIGNAGE WITH 1" LETTERING IS PROVIDED ON OR ADJACENT TO DOOR STATING "THIS DOOR TO REMAIN UNLOCKED WHEN BUILDING IS OCCUPIED". PROVIDE SIGNAGE AT FRONT DOOR AS APPLICABLE.
- PROVIDE ADA RESTROOM SIGN ADJACENT TO DOOR PER CODE.
- FINISH INTERIOR WALLS PER GYG SPECS - SEE ELEVATIONS
- PROVIDE FULL HEIGHT FRP AT ALL WALLS IN KITCHEN (TYP)
- FASTENERS, ANCHORS, CLIPS, STRAPS, ECT WHICH ARE IN CONTRACT WITH PRESERVATIVE AND / OR FIRE TREATED WOOD SHALL BE OF G-185 HOT DIPPED ZINC-COATED GALVANIZED STEEL, STAINLESS STEEL, OR AN APPROVED EQUAL.
- REFER TO CIVIL AND LANDSCAPE FOR LOCATIONS OF WALKS, BOLLARS, LANDSCAPING AREAS AND OTHER ITEMS.

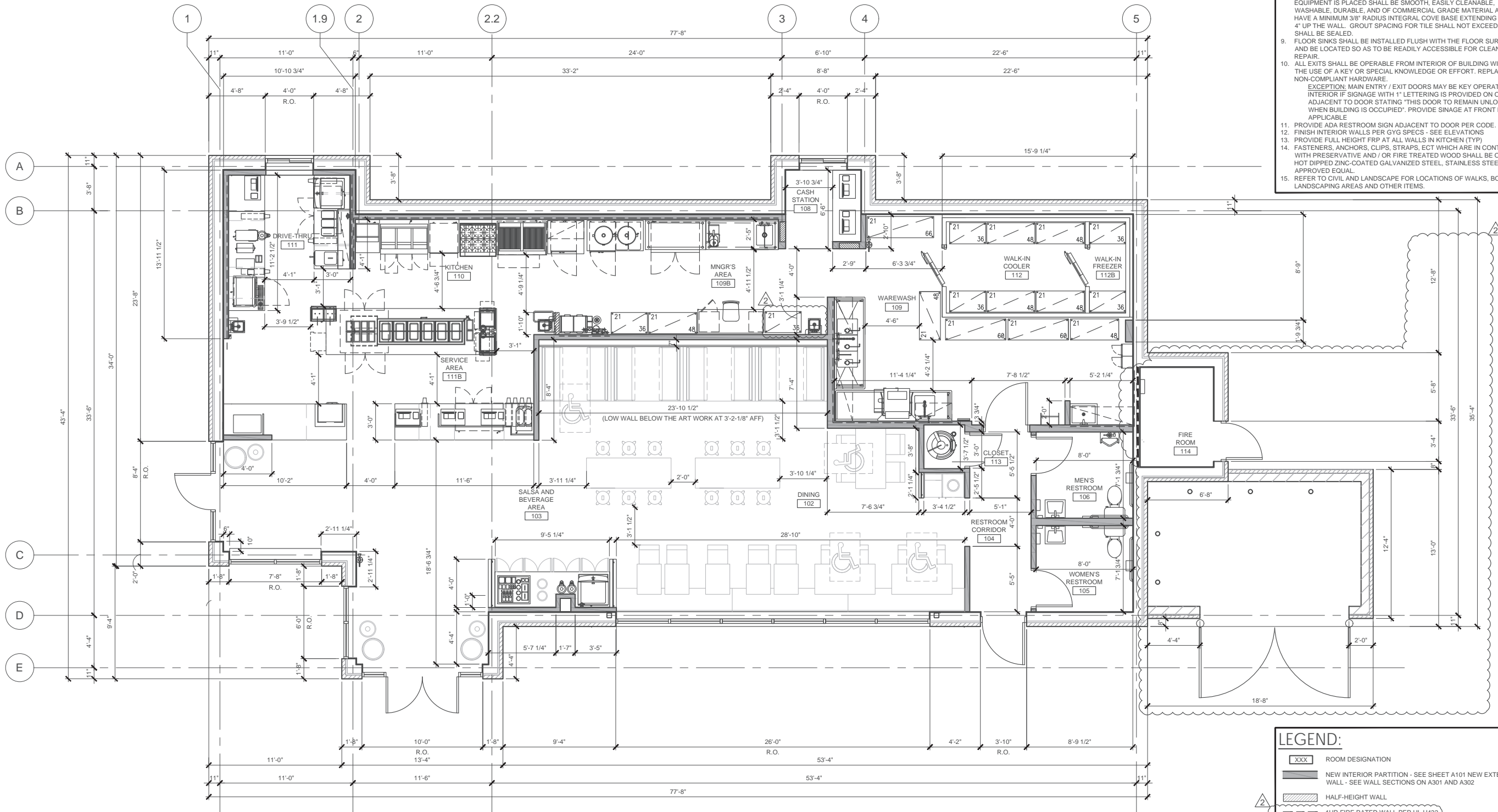
INTERPLAN
INTERPLAN LLC
DESIGN FIRM REGISTRATION
#184003341-0004

ARCHITECTURE
1 SOUTH 280 SUMMIT AVE, STE D
OAKBROOK TERRACE, IL 60181
630.932.2336

ENGINEERING
PERMITTING
220 E. CENTRAL PKWY, STE 4000
ALTIMONTE SPRINGS, FL 32701
407.645.5008

SEAL:

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1 FLOOR PLAN

LEGEND:

- XXX ROOM DESIGNATION
- NEW INTERIOR PARTITION - SEE SHEET A101 NEW EXTERIOR WALL - SEE WALL SECTIONS ON A301 AND A302
- HALF-HEIGHT WALL
- 1HR FIRE RATED WALL PER UL U432

NORTH

0 1' 2' 4' 8'

SCALE: 1/4" = 1'-0"

2 03.21.22 PERMIT COMMENTS
1 03.15.22 HEALTH COMMENTS
NO DATE REMARKS

REVISIONS



GUZMAN Y GOMEZ
5459 NORTHWEST HWY
CRYSTAL LAKE, IL 60014

PROJECT NO: 2021.0383
DATE: 04.02.21

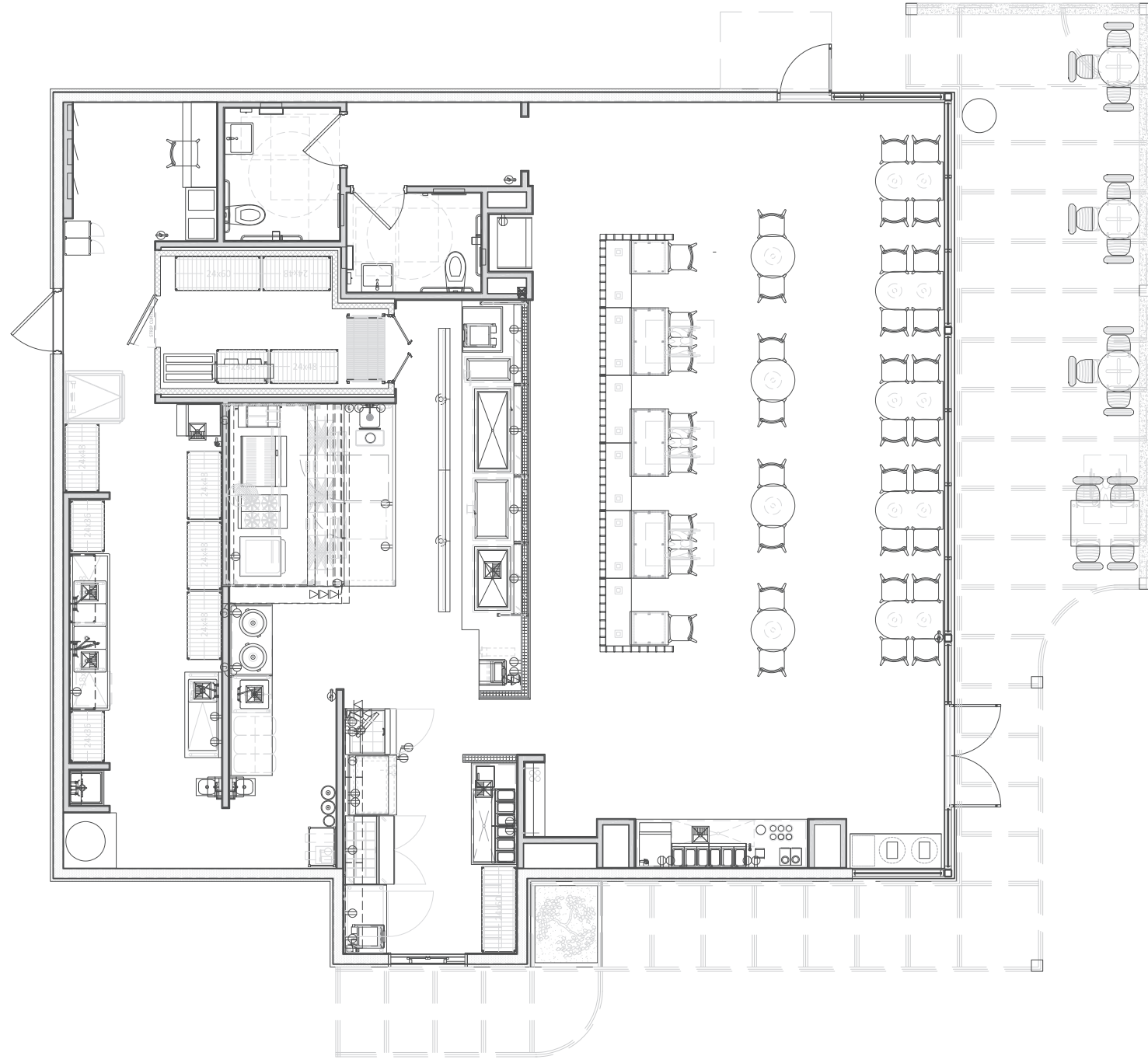
A100
FLOOR PLAN

CHECKED: MW DRAWN: DC

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4/3/2023 5:15 PM

 **FLOOR PLAN**
1
A-FP 1/4" = 1'-0"



ferris+sloane

100 N. Howard Street, Suite 4503 Spokane, WA 99201

CAVA

CAVA #010408 - DES PLAINES, IL
2777 Manheim Rd, Building B
Des Plaines, IL 60018
FOR
CAVA
702 H STREET, 2ND FLOOR, WASHINGTON, DC 20001

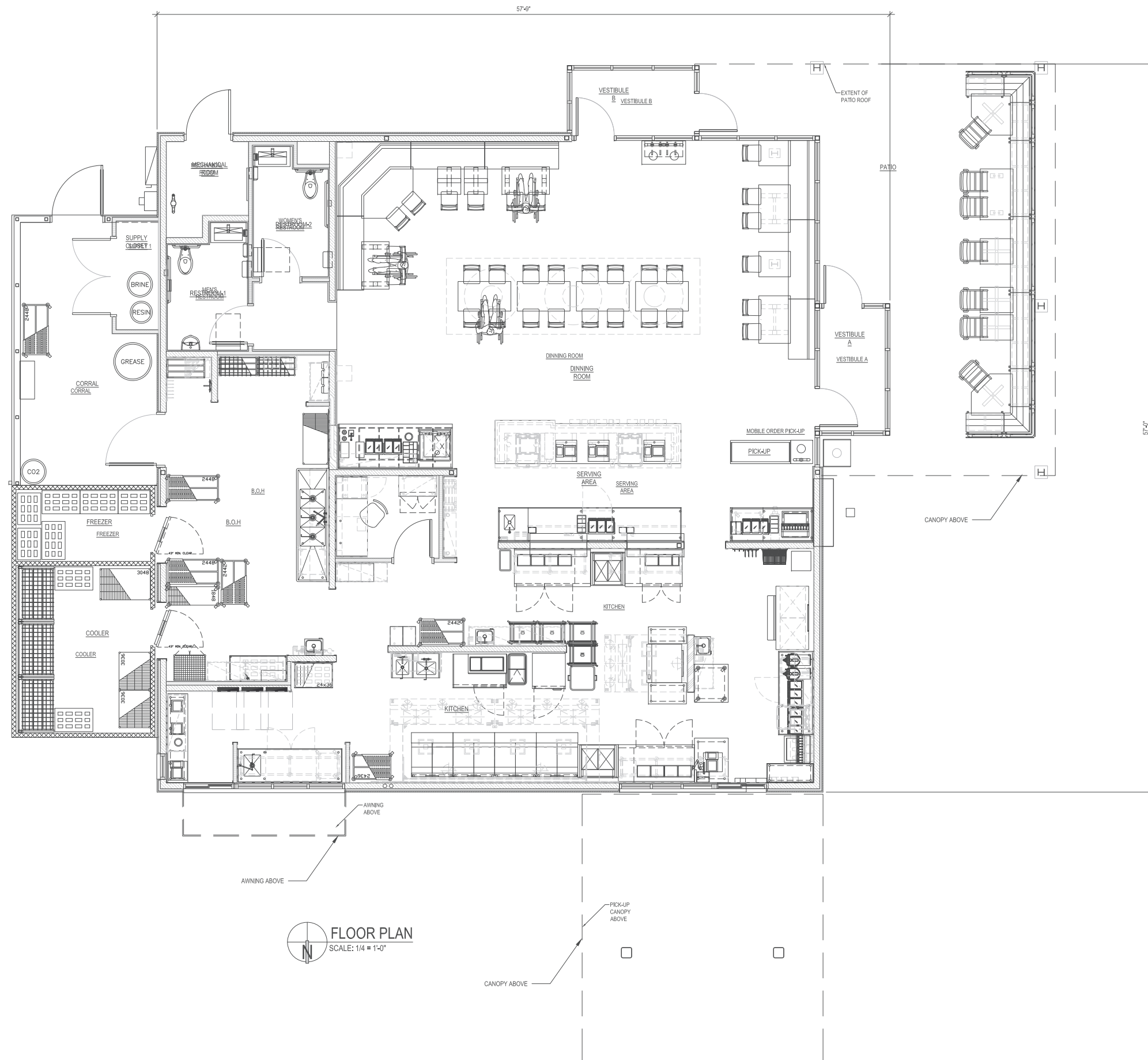
AOR PROJECT NUMBER:
CAV0018

| ISSUE | DATE |
|----------|--------------|
| TEST FIT | FEB 15 2023 |
| ZONING | APRIL 3 2023 |

ARCHITECTURAL
FLOOR PLAN

SHEET:

A-FP



FLOOR PLAN
SCALE: 1/4" = 1'-0"



Restaurant Support Office
6800 Bishop Road, Plain, TX 75024
Tel: 972-769-3100 Fax: 972-769-3101

STORE:
RAISING CANE'S RESTAURANT
2777 MANHEIM RD
DES PLAINES, IL 60018
PROTOTYPE: P6-V-Av
SCHEME: B
RESTAURANT #RC1009



17710 Detroit Avenue Lakewood, Ohio 44107
Phone (216) 521-5734 Fax (216) 521-4824
www.adaarchitects.com

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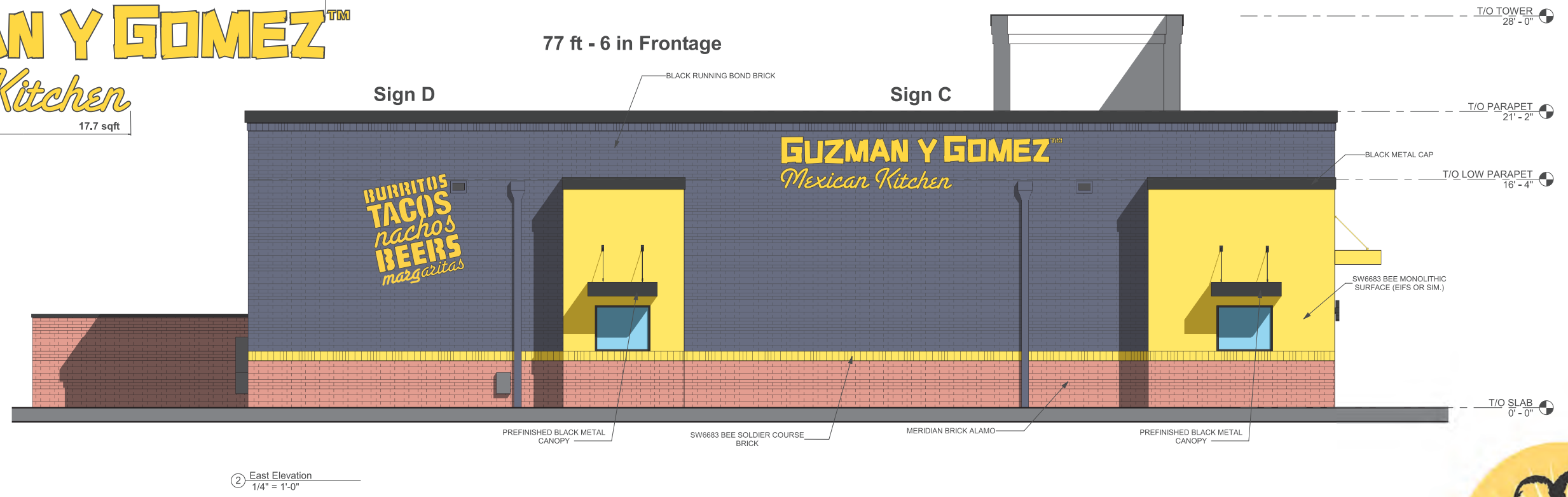
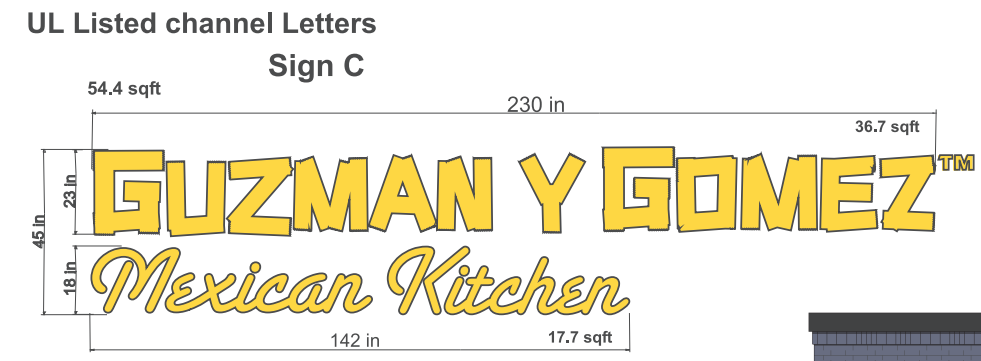
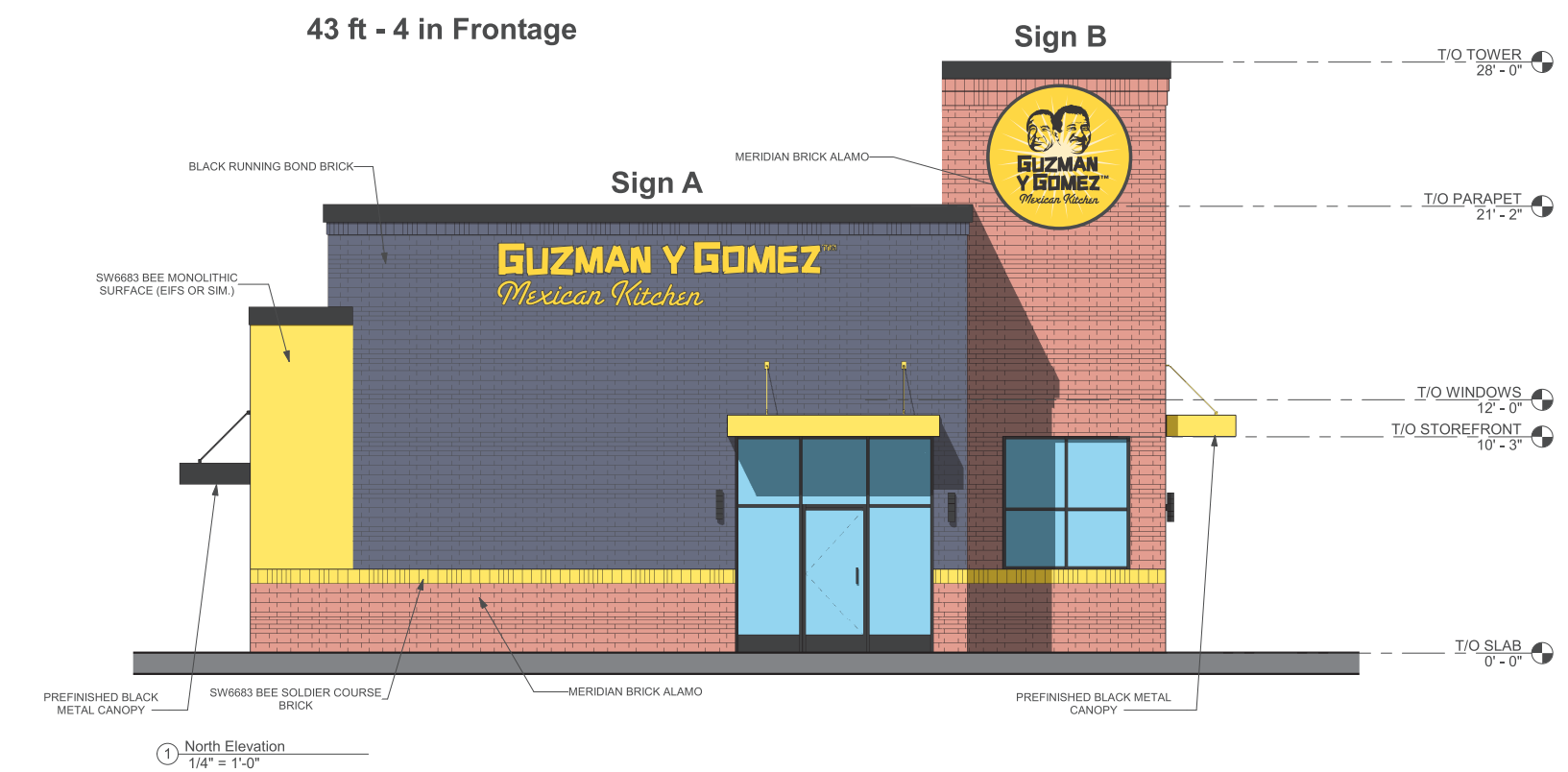
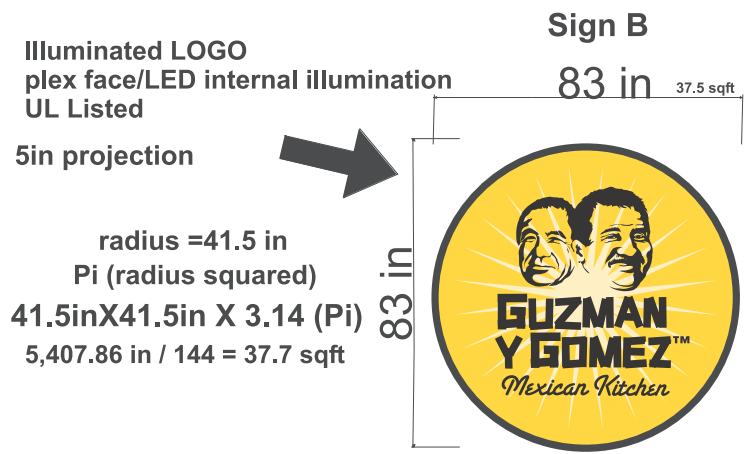
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FLOOR PLAN

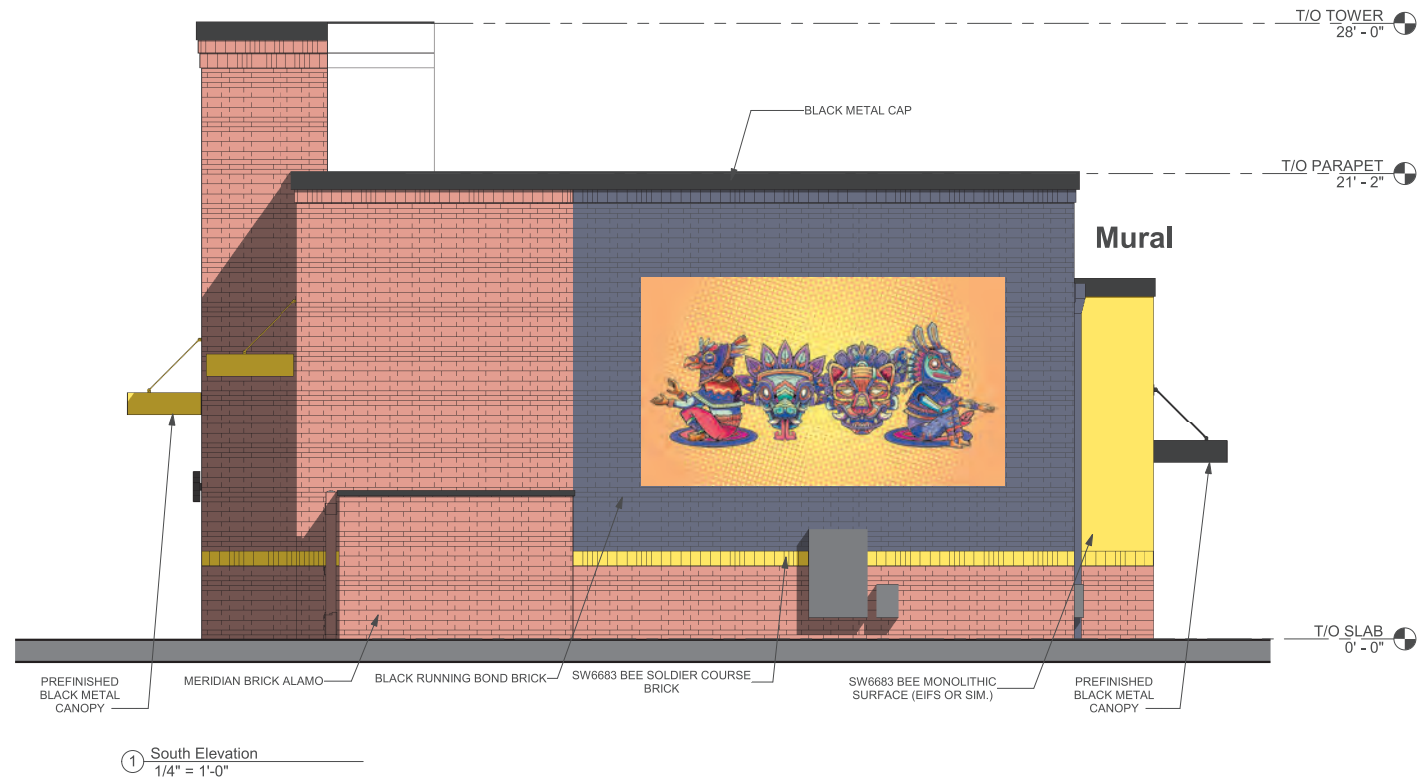
ISSUE DATE: 04/14/2023

JOB NO. 22297

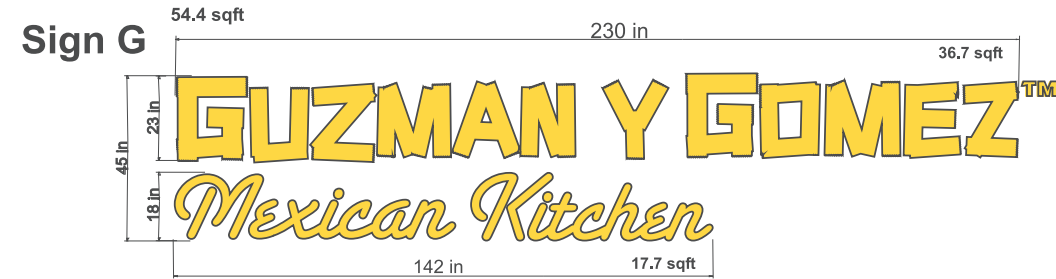
SK-1
SHEET NO.



Removed from plan
Sign E



UL Listed channel Letters



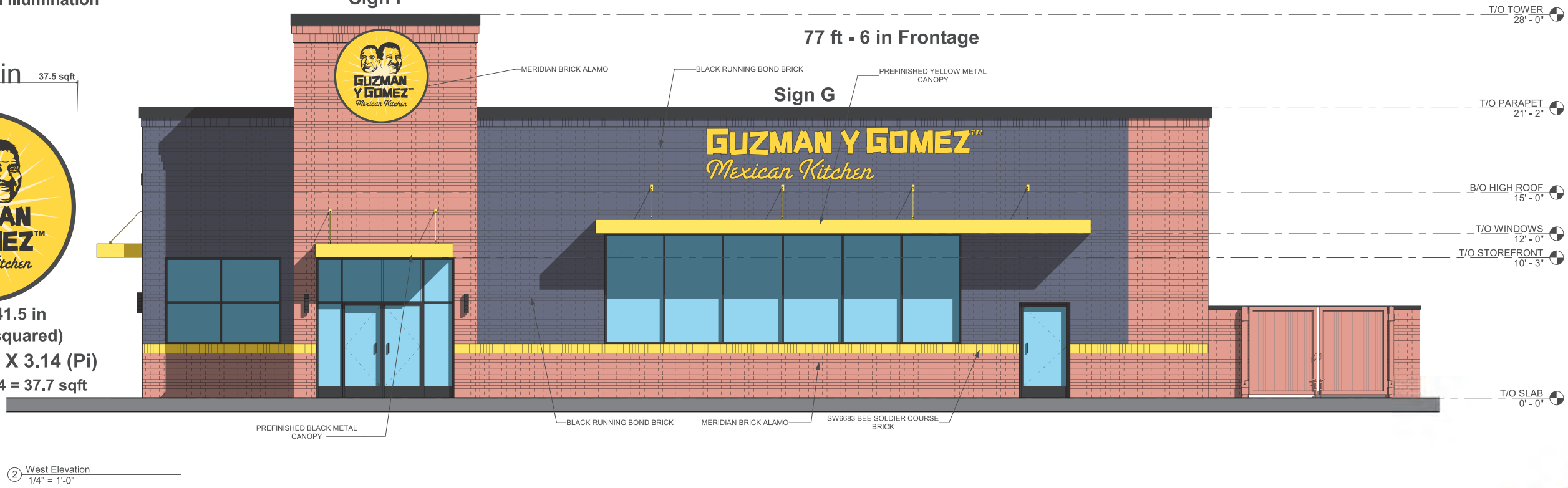
Sign F

Illuminated LOGO
plex face/LED internal illumination
UL Listed

5in projection



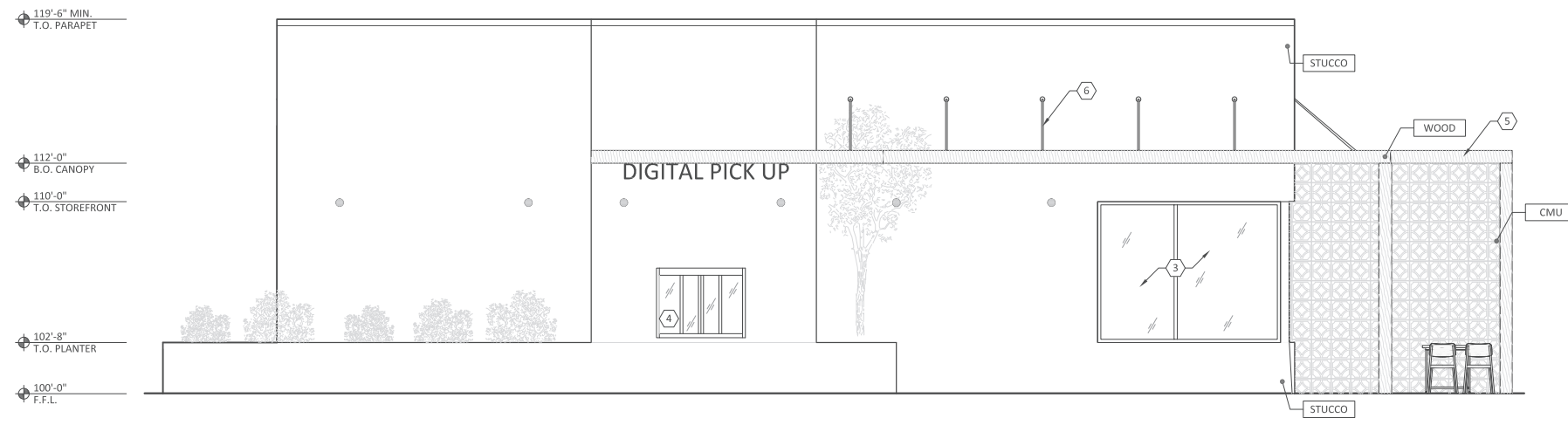
Sign F



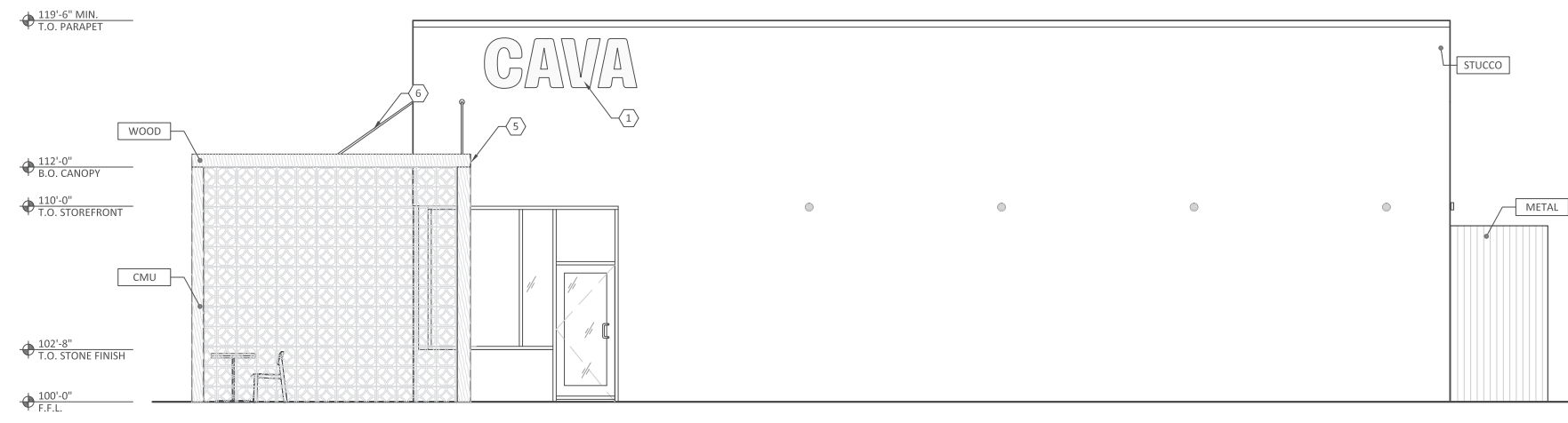
- EXT. ELEVATION CODED NOTES** #
1. REFERENCE SIGN SHOP DRAWINGS FOR LOCATION AND SIZE OF NEW SIGN. SIGNAGE UNDER SEPARATE PERMIT, GC TO PROVIDE BLOCKING AS NEEDED AND PULL POWER.
 2. NEW DOOR VINYL BY SIGN VENDOR.
 3. STOREFRONT SYSTEM.
 4. PICK UP BY CAR WINDOW.
 5. CANOPY.
 6. CREAM PAINTED TIE BACKS TO MATCH STUCCO FINISH.

EXT. FINISH MATERIAL LEGEND

| | | |
|--|--------|--|
| | STUCCO | PAINT/COLOR MATCH TO SW 7106- HONIED WHITE |
| | WOOD | |
| | METAL | |
| | CMU | |



1
A210
SIDE ELEVATION
1/4" = 1'-0"



2
A210
SIDE ELEVATION
1/4" = 1'-0"

ferris+sloane

100 N. Howard Street, Suite 4503 Spokane, WA 99201

CAVA

CAVA #010408 - DES PLAINES, IL
2777 Manheim Rd, Building B
Des Plaines, IL 60018
FOR CAVA
702 H STREET, 2ND FLOOR, WASHINGTON, DC 20001

AOR PROJECT NUMBER:
CAV0018

| ISSUE | DATE |
|----------|--------------|
| TEST FIT | FEB 15 2023 |
| ZONING | APRIL 3 2023 |

EXTERIOR ELEVATIONS

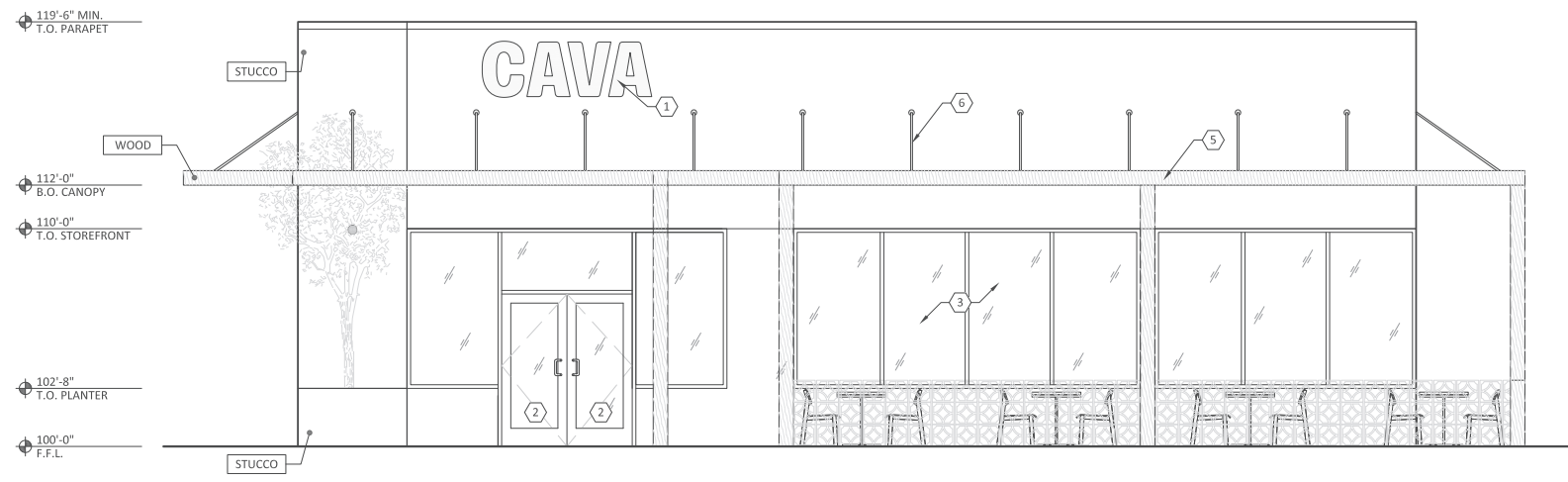
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A210

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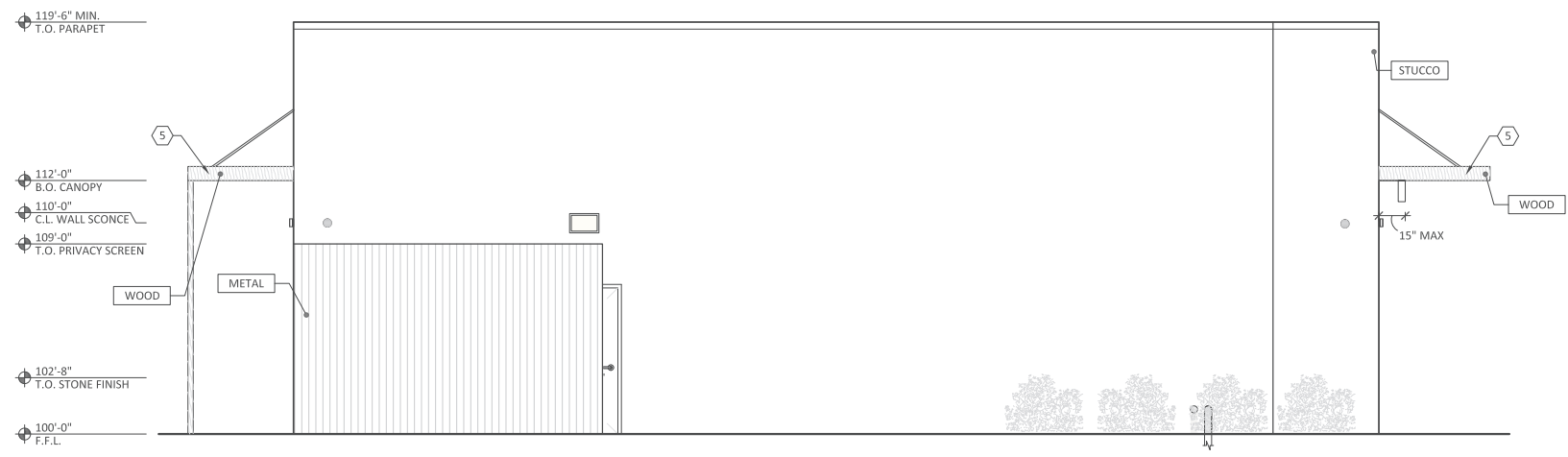
- ### EXT. ELEVATION CODED NOTES
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EXT. FINISH MATERIAL LEGEND

| | | |
|--|--------|--|
| | STUCCO | PAINT/COLOR MATCH TO SW 7106- HONIED WHITE |
| | WOOD | |
| | METAL | |
| | CMU | |



1 FRONT ELEVATION
A211 1/4" = 1'-0"



2 BACK ELEVATION
A211 1/4" = 1'-0"

ferris+sloane

100 N. Howard Street, Suite 4503 Spokane, WA 99201

CAVA

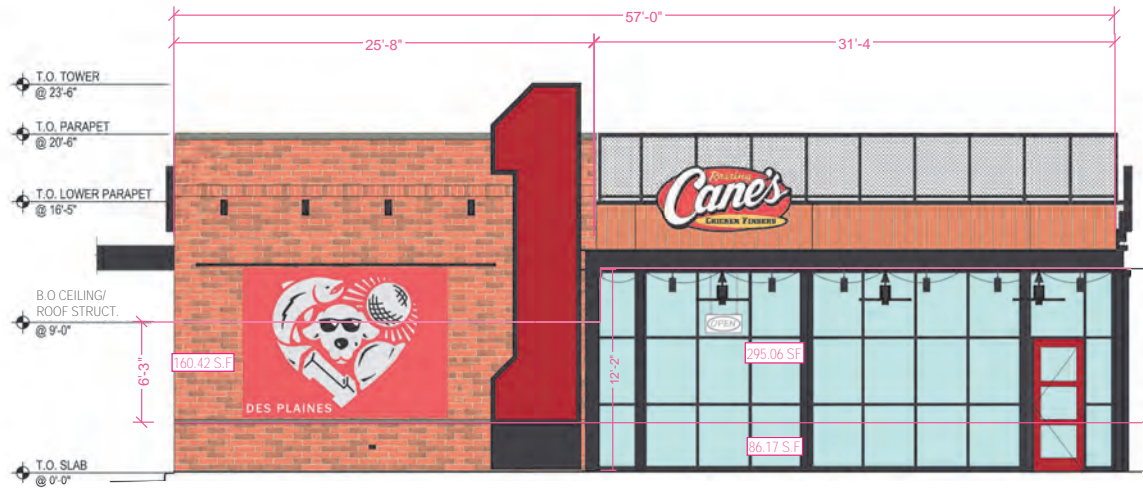
CAVA #010408 - DES PLAINES, IL
2777 Manheim Rd, Building B
Des Plaines, IL 60018
FOR CAVA
702 H STREET, 2ND FLOOR, WASHINGTON, DC 20001

| | |
|--------------------------------|--------------|
| AOR PROJECT NUMBER: CAV0018 | |
| ISSUE | DATE |
| TEST FIT | FEB 15 2023 |
| ZONING | APRIL 3 2023 |

EXTERIOR ELEVATIONS

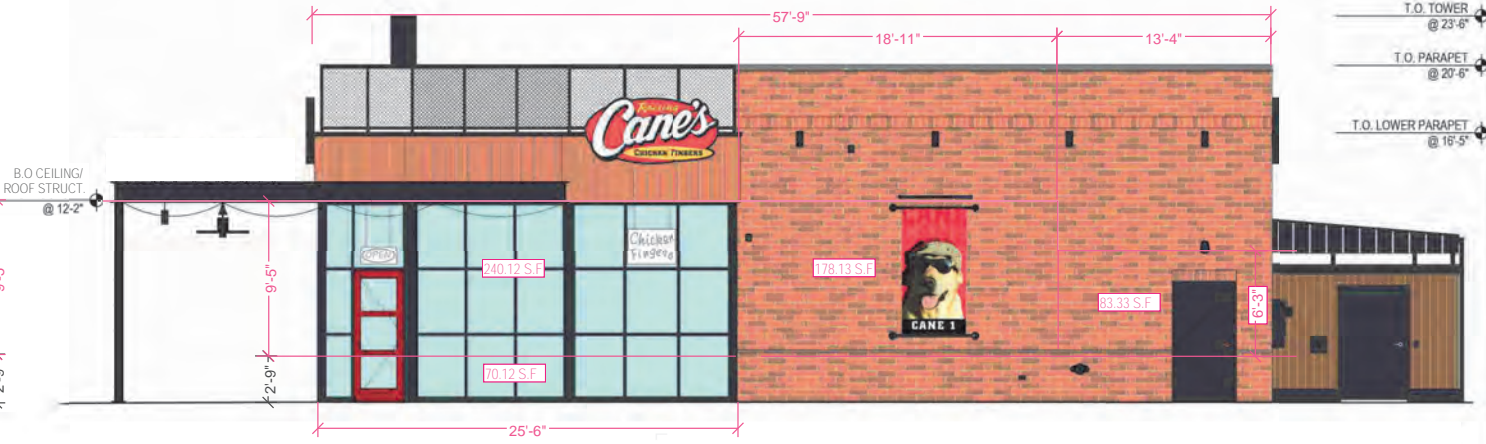
SHEET:
A211

3/28/2023 10:39 PM



1 FRONT ENTRY (WEST ELEVATION)- MANHEIM ROAD
 EL-1 SCALE: 3/16" = 1'-0"

STREET FRONTAGE AREA: 455 SF
 TRANSPARENCY AREA: 295 SF
 PERCENTAGE: 64.8%
 TRANSPARENCY BELOW 2'-9": 86 SF
 TOTAL TRANSPARENCY: 83.7%



2 SIDE ENTRY (SOUTH ELEVATION)- PRATT AVENUE
 EL-1 SCALE: 1/8" = 1'-0"

STREET FRONTAGE AREA: 502 SF
 TRANSPARENCY: 239 SF
 PERCENTAGE: 48%
 TRANSPARENCY BELOW 2'-9": 70 SF (14%)
 TOTAL TRANSPARENCY: 62%



3 DRIVE- THROUGH (NORTH ELEVATION)
 EL-1 SCALE: 1/8" = 1'-0"

TRANSPARENCY REQUIREMENT DOES NOT APPLY TO ELEVATIONS NOT ON A STREET FRONTAGE



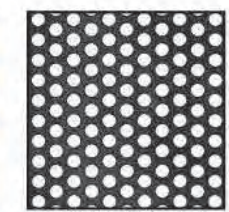
4 REAR (EAST ELEVATION)
 EL-1 SCALE: 1/8" = 1'-0"

TRANSPARENCY REQUIREMENT DOES NOT APPLY TO ELEVATIONS NOT ON A STREET FRONTAGE

MATERIAL FINISHES



Metal Panel 1



McNICHOLS Perforated Metal Panel
 Powder Coat: Black



Architectural Panels
 Nichiha: Vintage Cedar
 Wood Look Material
 (May need to match center design color)



Belden norman Brick
 Masonry Medium range,
 smooth, Iron Spot. Mortar to
 match solomon products io h.
 weathered horizontal
 strike. vertical joints are flush



Boral: "Alamo" modular
 brick, mortar to match
 solomon products io h.
 light buff Sack rub finish.

NOTE:
 A SEPARATE SIGN PERMIT WILL BE SUBMITTED FOR THE PROPOSED SIGNS

*12-3-11 TRANSPARENCY REQUIREMENTS:
 MINIMUM OF 75% TRANSPARENCY FROM 2'-9" ABOVE SIDEWALK FOR RETAIL,
 RESTAURANT OR SERVICE USES. 50% FOR ALL OTHER COMMERCIAL USES
 (FOR ELEVATIONS FACING A STREET).



Restaurant Support Office
 6800 Bishop Road, Plano, TX 75024
 Tele: 972-769-3100 Fax: 972-769-3101

STORE:
RAISING CANE'S RESTAURANT
 2777 MANHEIM RD
 DES PLAINES, IL 60018
 PROTOTYPE: P6-V-AV
 SCHEME: B
 RESTAURANT #RC1009
 VERSION: 2022-1.0



17710 Detroit Avenue, Lakewood, Ohio 44107
 Phone (216) 521-3134 Fax (216) 521-4624
 www.adaarchitects.com

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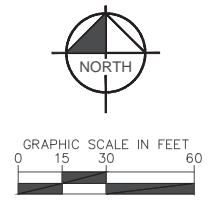
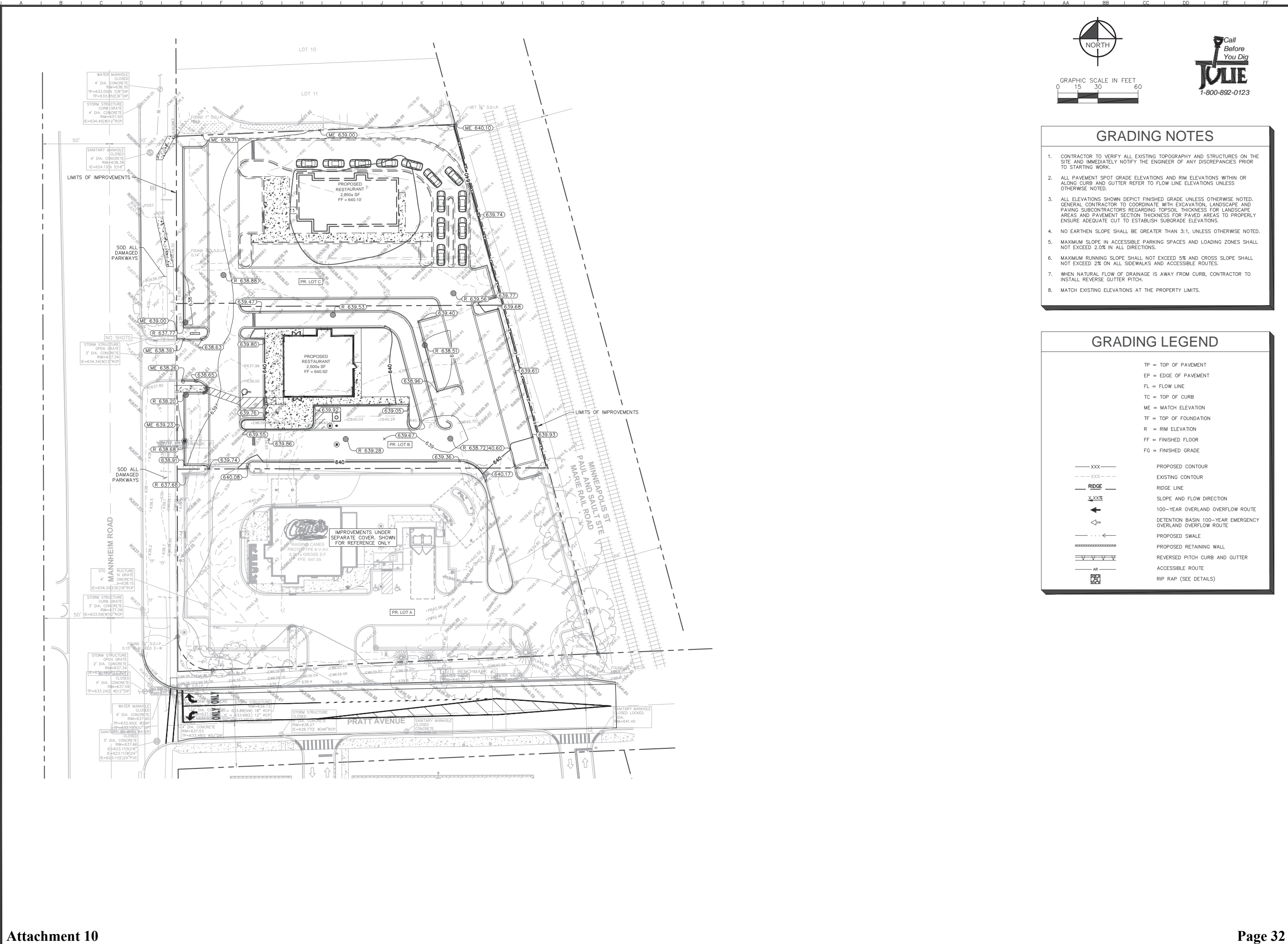
| SHEET REVISIONS | | |
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PROPOSED EXTERIOR ELEVATIONS

ISSUE DATE: 1/9/2023
 JOB NO. 22297

EL-1
 SHEET NO.

Drawing name: K:\GIS_DEV\168726020_gw_properties\des\plains_ii\A2_Design\CAD\plansheets\C4.0 GRADING PLAN.dwg C:\0 Apr 10, 2023 4:11pm By: Ian Spence
 This document, together with the concepts and designs presented herein, is an instrument of service. It is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



GRADING NOTES

1. CONTRACTOR TO VERIFY ALL EXISTING TOPOGRAPHY AND STRUCTURES ON THE SITE AND IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING WORK.
2. ALL PAVEMENT SPOT GRADE ELEVATIONS AND RIM ELEVATIONS WITHIN OR ALONG CURB AND GUTTER REFER TO FLOW LINE ELEVATIONS UNLESS OTHERWISE NOTED.
3. ALL ELEVATIONS SHOWN DEPICT FINISHED GRADE UNLESS OTHERWISE NOTED. GENERAL CONTRACTOR TO COORDINATE WITH EXCAVATION, LANDSCAPE AND PAVING SUBCONTRACTORS REGARDING TOPSOIL THICKNESS FOR LANDSCAPE AREAS AND PAVEMENT SECTION THICKNESS FOR PAVED AREAS TO PROPERLY ENSURE ADEQUATE CUT TO ESTABLISH SUBGRADE ELEVATIONS.
4. NO EARTHEN SLOPE SHALL BE GREATER THAN 3:1, UNLESS OTHERWISE NOTED.
5. MAXIMUM SLOPE IN ACCESSIBLE PARKING SPACES AND LOADING ZONES SHALL NOT EXCEED 2.0% IN ALL DIRECTIONS.
6. MAXIMUM RUNNING SLOPE SHALL NOT EXCEED 5% AND CROSS SLOPE SHALL NOT EXCEED 2% ON ALL SIDEWALKS AND ACCESSIBLE ROUTES.
7. WHEN NATURAL FLOW OF DRAINAGE IS AWAY FROM CURB, CONTRACTOR TO INSTALL REVERSE GUTTER PITCH.
8. MATCH EXISTING ELEVATIONS AT THE PROPERTY LIMITS.

GRADING LEGEND

| | | |
|-----------|--|------------------------|
| —XXX— | PROPOSED CONTOUR | TP = TOP OF PAVEMENT |
| - - -XXX- | EXISTING CONTOUR | EP = EDGE OF PAVEMENT |
| —RIDGE— | RIDGE LINE | FL = FLOW LINE |
| X XXX | SLOPE AND FLOW DIRECTION | TC = TOP OF CURB |
| ← | 100-YEAR OVERLAND OVERFLOW ROUTE | ME = MATCH ELEVATION |
| ↕ | DETENTION BASIN 100-YEAR EMERGENCY OVERLAND OVERFLOW ROUTE | TF = TOP OF FOUNDATION |
| — · · · — | PROPOSED SWALE | R = RIM ELEVATION |
| ▬▬▬▬▬ | PROPOSED RETAINING WALL | FF = FINISHED FLOOR |
| ▬▬▬▬▬ | REVERSED PITCH CURB AND GUTTER | FG = FINISHED GRADE |
| AR | ACCESSIBLE ROUTE | |
| ▣ | RIP RAP (SEE DETAILS) | |

| | | |
|---------------------------|----------|-----|
| REVISED PER CITY COMMENTS | DATE | BY |
| | 04/10/23 | INS |
| | | INS |

Kimley-Horn
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 570 LAKE COOK ROAD, SUITE 200
 DEERFIELD, IL 60015-7624
 WWW.KIMLEY-HORN.COM

SCALE: AS NOTED
 DESIGNED BY: INS
 DRAWN BY: INS
 CHECKED BY: EJT

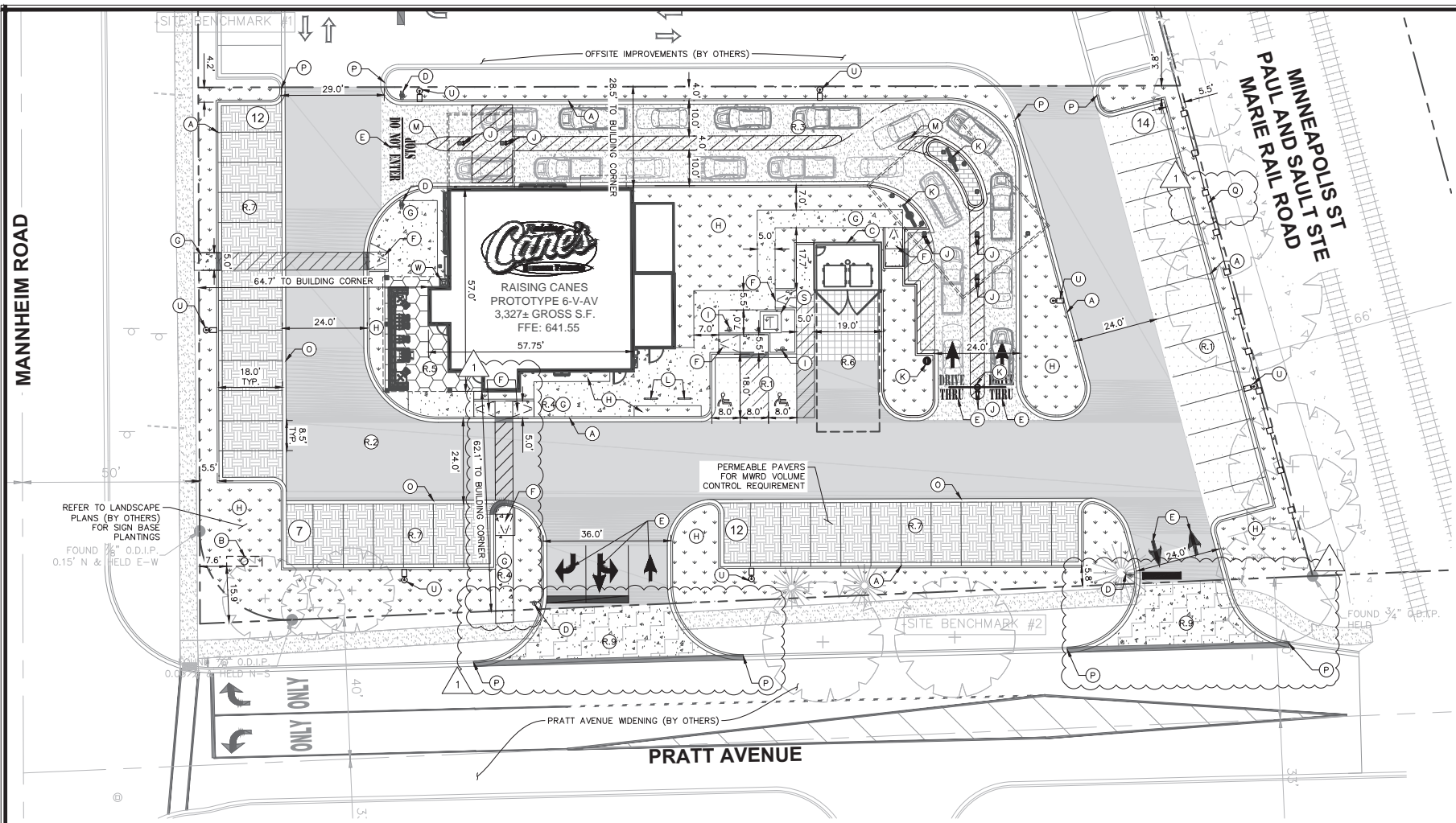


GRADING PLAN

GW PROPERTIES
DES PLAINES II
 2777 MANNHEIM ROAD
 DES PLAINES, IL 60018

ORIGINAL ISSUE:
 03/06/2023
 KHA PROJECT NO.
 168726020
 SHEET NUMBER

C4.0



SITE LEGEND

| | |
|------|---|
| (A) | B6.12 BARRIER CURB AND GUTTER. SEE CONSTRUCTION DETAILS. |
| (B) | PROPOSED PYLON SIGN. |
| (C) | RECYCLING/TRASH DUMPSTER LOCATION. SEE ARCHITECTURAL PLANS FOR DETAILS OF SCREENING, GATES, BOLLARDS AND MASONRY. |
| (D) | SITE DIRECTIONAL SIGN. SEE SHEET. |
| (E) | DIRECTIONAL PAVEMENT MARKING. SEE SHEET. |
| (F) | BARRIER FREE RAMP. SEE CONSTRUCTION DETAILS. |
| (G) | CONCRETE SIDEWALK. SEE CONSTRUCTION DETAILS. |
| (H) | LANDSCAPE AREA. SEE LANDSCAPE PLANS FOR DETAILS. |
| (I) | ACCESSIBLE PARKING SIGN. |
| (J) | 4" PIPE BOLLARD. SEE ARCHITECTURAL PLANS FOR DETAILS. |
| (K) | DRIVE THRU ORDER BOARD, PRE-ORDER BOARD OR HEIGHT DETECTOR. SEE ARCHITECTURAL PLANS FOR DETAILS. |
| (L) | BIKE RACK. SEE ARCHITECTURAL PLANS FOR DETAILS. |
| (M) | PAVEMENT STRIPING. |
| (N) | PROPOSED FIRE HYDRANT. SEE SHEET C4.0 - UTILITY PLAN FOR DETAILS. |
| (O) | 1" CONCRETE CURB RIBBON. SEE CONSTRUCTION DETAILS. |
| (P) | TIE PROPOSED CURB AND GUTTER TO EXISTING CURB & GUTTER WITH SMOOTH, CONTINUOUS TRANSITIONS. |
| (Q) | 8" WOOD FENCE. SEE LANDSCAPE PLANS FOR DETAILS. |
| (R1) | STANDARD DUTY ASPHALT PAVEMENT. SEE CONSTRUCTION DETAILS. |
| (R2) | HEAVY DUTY ASPHALT PAVEMENT. SEE CONSTRUCTION DETAILS. |
| (R3) | HEAVY DUTY CONCRETE PAVEMENT. INTEGRAL COLOR 860 GRAPHITE (IRON OXIDE) SEE GENERAL NOTES - PAVING NOTES. |
| (R4) | STANDARD DUTY CONCRETE FOR SIDEWALKS. SEE CONSTRUCTION DETAILS. |
| (R5) | PATIO PAVEMENT. SEE ARCHITECTURAL AND STRUCTURAL PLANS. |
| (R6) | HEAVY DUTY CONCRETE PAVEMENT AT TRASH ENCLOSURE. INTEGRAL COLOR 860 GRAPHITE (IRON OXIDE) SEE CONSTRUCTION DETAILS. |
| (R7) | PERMEABLE PAVERS. SEE CONSTRUCTION DETAILS. |
| (R8) | PAVERS, BELGARD HOLLAND STONE, LEGACY PAVERS IN HERRINGBONE/BASKET WEAVE PATTERN, CHARCOAL OR SIMILAR GREY COLOR. |
| (R9) | HEAVY DUTY CONCRETE APRON. |
| (S) | TRANSFORMER PAD LOCATION. SEE ELECTRICAL PLANS FOR DETAILS. |
| (T) | NOT USED. |
| (U) | LIGHT POLE. SEE LIGHTING PLANS FOR DETAILS. |
| (V) | NOT USED. |
| (W) | TRASH RECEPTACLE. SEE ARCHITECTURAL PLANS FOR DETAILS. |
| (X) | NOT USED. |
| (Y) | NOT USED. |

SITE ANALYSIS TABLE

| | | |
|---|--------------------------|-----------|
| EXISTING ZONING | C-3 | |
| PROPOSED USE | RESTAURANT W/ DRIVE THRU | |
| LOT AREA | 47,338 SF / 1.09 AC | |
| GROSS BUILDING AREA | 2,818 SF | |
| PARKING STALL REQUIREMENT | | |
| NET FLOOR AREA (EXCLUDING MECH. ROOMS, FOOD PREP. AND RESTROOMS) = 1,863 SF | | |
| 1 SPACE PER 50 SF = 38 SPACES | | |
| 1 SPACE PER 3 EMPLOYEES = 5 SPACES (ASSUMES 15 EMPLOYEES) | | |
| TOTAL PARKING | 43 SPACES | 47 SPACES |
| ACCESSIBLE | 2 SPACES | 2 SPACES |

LEGEND

--- PROPERTY/LEASE/LOT LINE
 --- ADJACENT PROPERTY LINE
 PROPOSED CONCRETE CURB & GUTTER
 EXISTING CURB & GUTTER
 DEPRESSED CURB AND GUTTER
 PROPOSED PARKING COUNT
 STANDARD DUTY ASPHALT PAVEMENT. SEE CONSTRUCTION DETAILS.
 HEAVY DUTY ASPHALT PAVEMENT. SEE CONSTRUCTION DETAILS.
 HEAVY DUTY CONCRETE PAVEMENT. INTEGRAL COLOR 860 GRAPHITE (IRON OXIDE) SEE GENERAL NOTES - PAVING NOTES.
 STANDARD DUTY CONCRETE FOR SIDEWALKS. SEE CONSTRUCTION DETAILS.
 PATIO PAVEMENT. SEE ARCHITECTURAL AND STRUCTURAL PLANS.
 HEAVY DUTY CONCRETE PAVEMENT AT TRASH ENCLOSURE. INTEGRAL COLOR 860 GRAPHITE (IRON OXIDE) SEE CONSTRUCTION DETAILS.
 PERMEABLE PAVERS. SEE CONSTRUCTION DETAILS.
 PAVERS, BELGARD HOLLAND STONE, LEGACY PAVERS IN HERRINGBONE/BASKET WEAVE PATTERN, CHARCOAL OR SIMILAR GREY COLOR.
 HEAVY DUTY CONCRETE APRON

- ### SITE NOTES
- ALL WORK AND MATERIALS SHALL COMPLY WITH ALL CITY/COUNTY REGULATIONS AND CODES AND O.S.H.A. STANDARDS.
 - CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING & ELECTRICAL PLAN.
 - REFERENCE ARCHITECTURAL PLANS FOR DUMPSTER ENCLOSURE DETAILS.
 - EXISTING UTILITY STRUCTURES WITHIN CONSTRUCTION LIMITS ARE TO BE ABANDONED, REMOVED OR RELOCATED AS NECESSARY. ALL COSTS SHALL BE INCLUDED IN BASE BID.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR ALL RELOCATIONS, UNLESS OTHERWISE NOTED ON PLANS INCLUDING BUT NOT LIMITED TO, ALL UTILITIES, STORM DRAINAGE, SIGNS, TRAFFIC SIGNALS & POLES, ETC. AS REQUIRED. ALL WORK SHALL BE IN ACCORDANCE WITH GOVERNING AUTHORITIES REQUIREMENTS AND PROJECT SITE WORK SPECIFICATIONS AND SHALL BE APPROVED BY SUCH. ALL COSTS SHALL BE INCLUDED IN BASE BID.
 - EXISTING SITE BOUNDARY, TOPOGRAPHY, UTILITY AND ROAD INFORMATION TAKEN FROM A SURVEY BY COMPASS SURVEYING, LTD. DATED SEPTEMBER 5TH 2022.
 - ALL PROPOSED PAVING IN CITY R.O.W. AND EASEMENTS TO CONFORM TO CITY OF DES PLAINES STANDARDS.
 - CONTRACTOR SHALL ADJUST EXISTING VALVES, MANHOLE RIMS, ETC. AS NECESSARY TO MATCH FINISHED GRADE.
 - REFERENCE LANDSCAPE PLANS FOR PROPOSED BUFFERS, SCREENING, AND PLANTING.
 - THERE ARE EXISTING TREES ONSITE. REFER TO LANDSCAPE PLAN (BY OTHERS) FOR TREES TO REMAIN.
 - CONTRACTOR SHALL SAWCUT ALONG ALL EXISTING PAVEMENT TO PROVIDE CLEAN UNIFORM SURFACE TO TIE INTO PROPOSED PAVEMENT.
 - CONTRACTOR SHALL RESTORE ALL DAMAGED PARKWAYS WITH SOG.

BENCHMARKS

LOCATIONS SHOWN ON SURVEY

REFERENCE BENCHMARK:
 VERTICAL DATUM - NAVD88

ALL ELEVATIONS SHOWN HEREON WERE ESTABLISHED BY USING RTK/GPS OBSERVATIONS BASED ON TRIMBLE VRS HARN NETWORK

SITE BENCHMARKS:

SITE BENCHMARK #1
 SOUTH-EAST BOLT ON FIRE HYDRANT ON NORTH OF PRATT AVENUE ON EAST SIDE OF ROUTE 45
 ELEVATION = 639.81

SITE BENCHMARK #2
 BOLT ON FIRST FIRE HYDRANT WEST OF RAIL ROAD TRACKS ON NORTH SIDE OF PRATT AVENUE
 ELEVATION = 641.85



Restaurant Support Office
 6800 Bishop Road, Plano, TX 75024
 Tele: 972-769-3100 Fax: 972-769-3101

STORE:
RAISING CANE'S RESTAURANT
 2777 MANNHEIM RD
 DES PLAINES, IL 60018
 PROTOTYPE P6-V-AV
 SCHEME: B
 RESTAURANT #RC1009

ADA ARCHITECTS

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KH JOB NO. 168418039

SHEET REVISIONS

| # | DATE | TYPE |
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| 1 | 4/5/23 | PER CITY COMMENTS |
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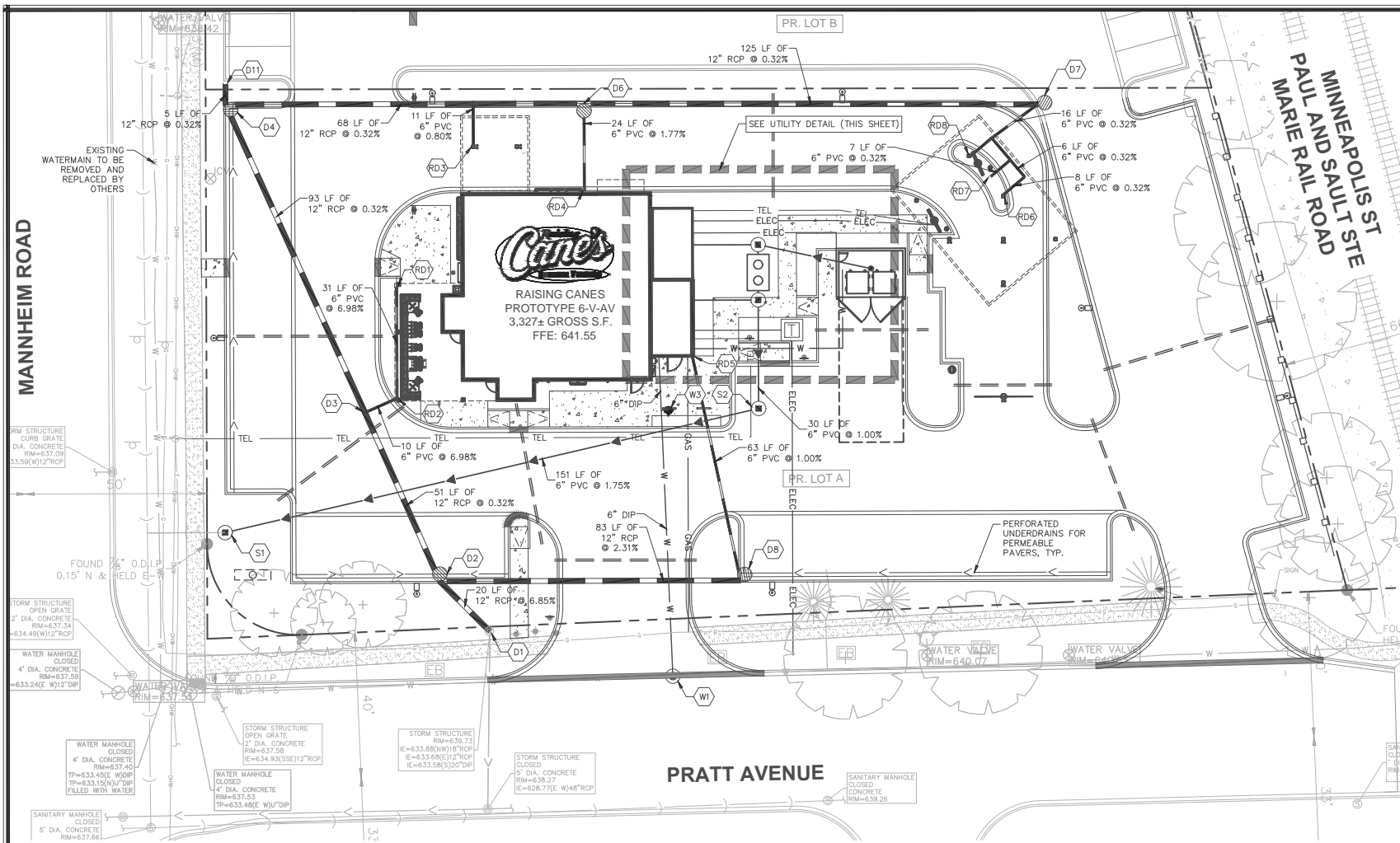
PRELIMINARY SITE KEYNOTE PLAN

ISSUE DATE: 01/09/2023

JOB NO. 22297

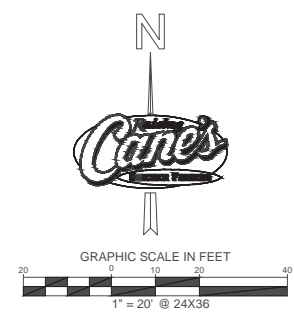
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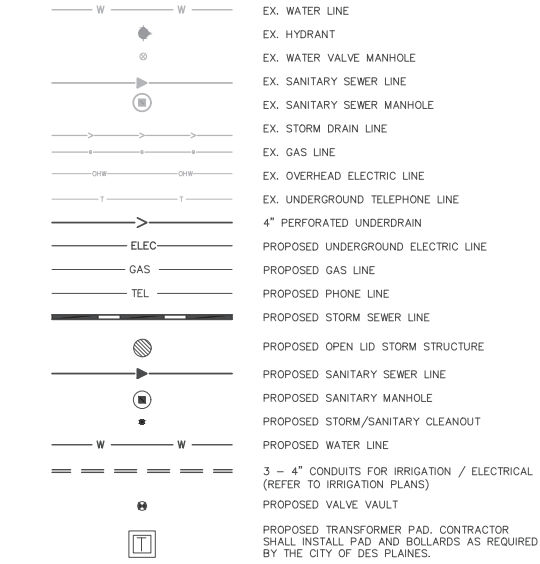


BENCHMARKS

LOCATIONS SHOWN ON SURVEY
 REFERENCE BENCHMARK:
 VERTICAL DATUM - NAVD83
 ALL ELEVATIONS SHOWN HEREON WERE ESTABLISHED BY USING RTK/GPS OBSERVATIONS BASED ON TRIMBLE VRS HARN NETWORK
 SITE BENCHMARK #1
 SOUTH EAST BOLT ON FIRE HYDRANT ON NORTH OF PRATT AVENUE ON EAST SIDE OF ROUTE 45
 ELEVATION = 639.81
 SITE BENCHMARK #2
 BOLT ON FIRST FIRE HYDRANT WEST OF RAIL ROAD TRACKS ON NORTH SIDE OF PRATT AVENUE
 ELEVATION = 641.85

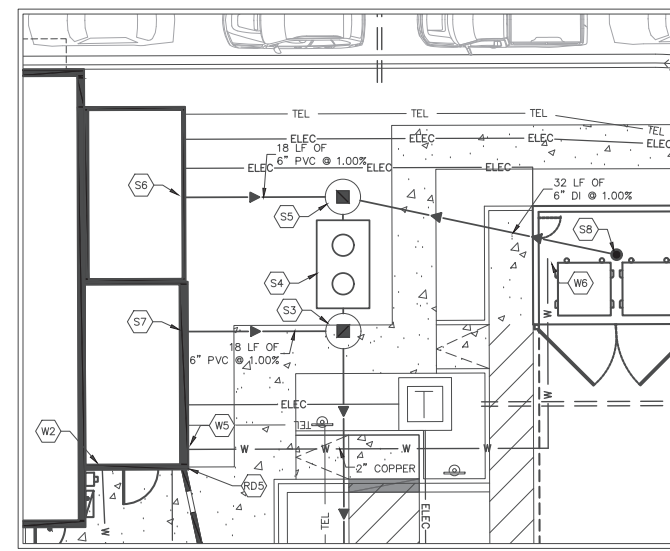


UTILITY LEGEND



UTILITY NOTES

- SEE MEP PLANS FOR ALL UTILITY CONNECTIONS INTO BUILDING.
- REFER TO CITY STANDARD AND DETAILS FOR TRENCHING, BEDDING, BACKFILL, AND TRENCH COMPACTION REQUIREMENTS.
- REFER TO ARCHITECTURE PLANS FOR LOCATION AND SIZING OF SLEEVES FOR FRANCHISE UTILITIES, IRRIGATION, ETC.
- ALL FILL MATERIAL IS TO BE IN PLACE, AND COMPACTED BEFORE INSTALLATION OF PROPOSED UTILITIES.
- CONTRACTOR SHALL NOTIFY THE UTILITY AUTHORITIES INSPECTORS 72 HOURS BEFORE CONNECTING TO ANY EXISTING LINE.
- ALL SANITARY SEWER LINES SHALL BE PVC MEETING ASTM D-3034 SDR 26 EXCEPT FOR SANITARY SEWER THAT CROSSES ABOVE WATER MAIN. THIS PIPE SHALL BE AWWA C900 (UNLESS WATER MAIN CASING IS UTILIZED). PROVIDE 42\"/>
- WATER LINES SHALL BE AS FOLLOWS:
 PRIVATE (LARGER THAN 3 INCHES): DUCTILE IRON PIPE, CLASS 52 OR POLYVINYL CHLORIDE PLASTIC PIPE (ASTM D-3212 AND AWWA C900)
 PRIVATE (3 INCHES OR SMALLER): TYPE "K" SOFT COPPER, ASTM B88
- CONTRACTOR IS RESPONSIBLE FOR COMPLYING TO THE SPECIFICATIONS OF THE CITY OF DES PLAINES WITH REGARDS TO MATERIALS AND INSTALLATION OF THE WATER LINE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO DEFLECT ELECTRIC, GAS, CABLE, AND TELEPHONE CONDUIT AND PIPING AS REQUIRED TO AVOID UTILITY CONFLICTS.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
- CONTRACTOR IS RESPONSIBLE FOR ALL NECESSARY PERMITS, INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CITY CODES AND/OR UTILITY SERVICE COMPANIES.
- CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES FOR INSTALLATION REQUIREMENTS AND SPECIFICATIONS IN REGARDS TO TAPS, HYDRANTS, VALVES, ETC.
- CONTRACTOR IS RESPONSIBLE FOR PAVEMENT REPLACEMENT REQUIRED FOR ALL UTILITY INSTALLATIONS PER CITY OF DES PLAINES STANDARDS.
- WATER TIGHT CONNECTION SHALL BE MADE USING A RESILIENT CONNECTOR "SEAL BOOT" PER ASTM C-923.
- CONTRACTOR SHALL MAINTAIN A MINIMUM OF 5.5 FEET COVER ON ALL WATER LINES.
- CONTRACTOR TO PROVIDE POWER TO MONUMENT SIGN. REFER TO ELECTRICAL SITE PLAN FOR SIZE AND TYPE.
- CONTRACTOR SHALL ADJUST EXISTING VALVES, MANHOLE RIMS, ETC. AS NECESSARY TO MATCH FINISHED GRADE.
- ALL PARKING LOT CATCH BASINS & MANHOLES SHALL BE 48\"/>



UTILITY DETAIL

SCALE: 1" = 10'
 NOTE: REFERENCE MEP PLANS FOR EXACT LOCATION OF ALL UTILITY BUILDING ENTRIES.

| STRUCTURE NAME | DETAILS |
|----------------|--|
| D1 | CONNECT TO EXISTING STORM SEWER MH. CONTRACTOR TO FIELD VERIFY LOCATION, DEPTH AND SIZE OF EX. SEWER PRIOR TO ORDERING MATERIALS. RIM: 635.89 INV IN: 633.88 (NW, 12") INV OUT: 633.58 (S, 12") |
| D2 | OPEN-LID MANHOLE RIM: 639.98 INV IN: 635.22 (NW, 12") INV IN: 635.22 (E, 12") INV OUT: 635.22 (SE, 12") |
| D3 | INSERT A TEE ROOF DRAIN CONNECTION RIM: 640.80 INV IN: 635.38 (NW, 12") INV IN: 635.63 (NE, 6") INV OUT: 635.38 (SE, 12") |
| D4 | OPEN-LID MANHOLE RIM: 639.65 INV IN: 635.68 (E, 12") INV IN: 635.68 (N, 12") INV OUT: 635.68 (SE, 12") |
| D6 | OPEN-LID MANHOLE RIM: 640.63 INV IN: 635.99 (E, 12") INV IN: 638.02 (S, 6") INV OUT: 635.99 (W, 12") |
| D7 | OPEN-LID MANHOLE RIM: 640.07 INV IN: 636.39 (SW, 6") INV OUT: 636.39 (W, 12") |
| D8 | OPEN-LID MANHOLE RIM: 640.34 INV IN: 637.63 (N, 6") INV OUT: 637.14 (W, 12") |

| STRUCTURE NAME | DETAILS |
|----------------|--|
| S1 | DOGHOUSE MANHOLE CONNECTION TO EXISTING SEWER SERVICE. CONTRACTOR TO TELEPHONE REMAINDER OF EXISTING SERVICE LINE WEST TO PUBLIC MAIN. SUBMIT VIDEO TO KIMLEY-HORN FOR REVIEW PRIOR TO ORDERING MATERIALS. RIM: 639.40 INV IN: 632.72 (E, 6") INV OUT: 632.34 (W, 8") |
| S2 | 4\"/> |

| STRUCTURE NAME | DETAILS |
|----------------|---|
| W1 | PRESSURE CONNECT TO EXIST WATER IN VALVE VAULT. CONTRACTOR TO FIELD VERIFY LOCATION, DEPTH, AND SIZE OF EXISTING WATER LINE PRIOR TO ORDERING MATERIALS. FG ELEV: 640.00 |
| W2 | WATER CONNECTION FG ELEV: 641.55 |
| W3 | FIRE HYDRANT FG ELEV: 641.09 |
| W5 | WATER CONNECTION FG ELEV: 641.55 |
| W6 | POST HYDRANT (FOR HOSE BIB) FG ELEV: 641.16 |

| STRUCTURE NAME | DETAILS |
|----------------|--|
| D11 | STORM SEWER STUB INV OUT: 635.70 (S, 12") |
| RD1 | PATIO CANOPY DRAIN INV OUT: 638.45 (S, 6") |
| RD2 | PATIO CANOPY DRAIN INV IN: 636.31 (N, 6") INV OUT: 636.31 (SW, 6") |
| RD3 | DT CANOPY DRAIN INV OUT: 637.88 (N, 6") |
| RD4 | ROOF DRAIN INV OUT: 638.45 (N, 6") |
| RD5 | ROOF DRAIN INV OUT: 638.27 (S, 6") |
| RD6 | DT CANOPY DRAIN INV OUT: 636.51 (NE, 6") |
| RD7 | DT CANOPY DRAIN INV OUT: 636.49 (NE, 6") |
| RD8 | DT CANOPY DRAIN INV OUT: 636.47 (NE, 6") |



STORE:
RAISING CANES RESTAURANT
 2777 MANNHEIM RD
 DES PLAINES, IL 60018
 PROTOTYPE P6-V-AV
 SCHEME: B
 RESTAURANT #RC1009



FOR REVIEW ONLY

ENGINEER INFORMATION:
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 4201 WINFIELD ROAD, SUITE 600, IL 60555
 PHONE: 630-487-5550
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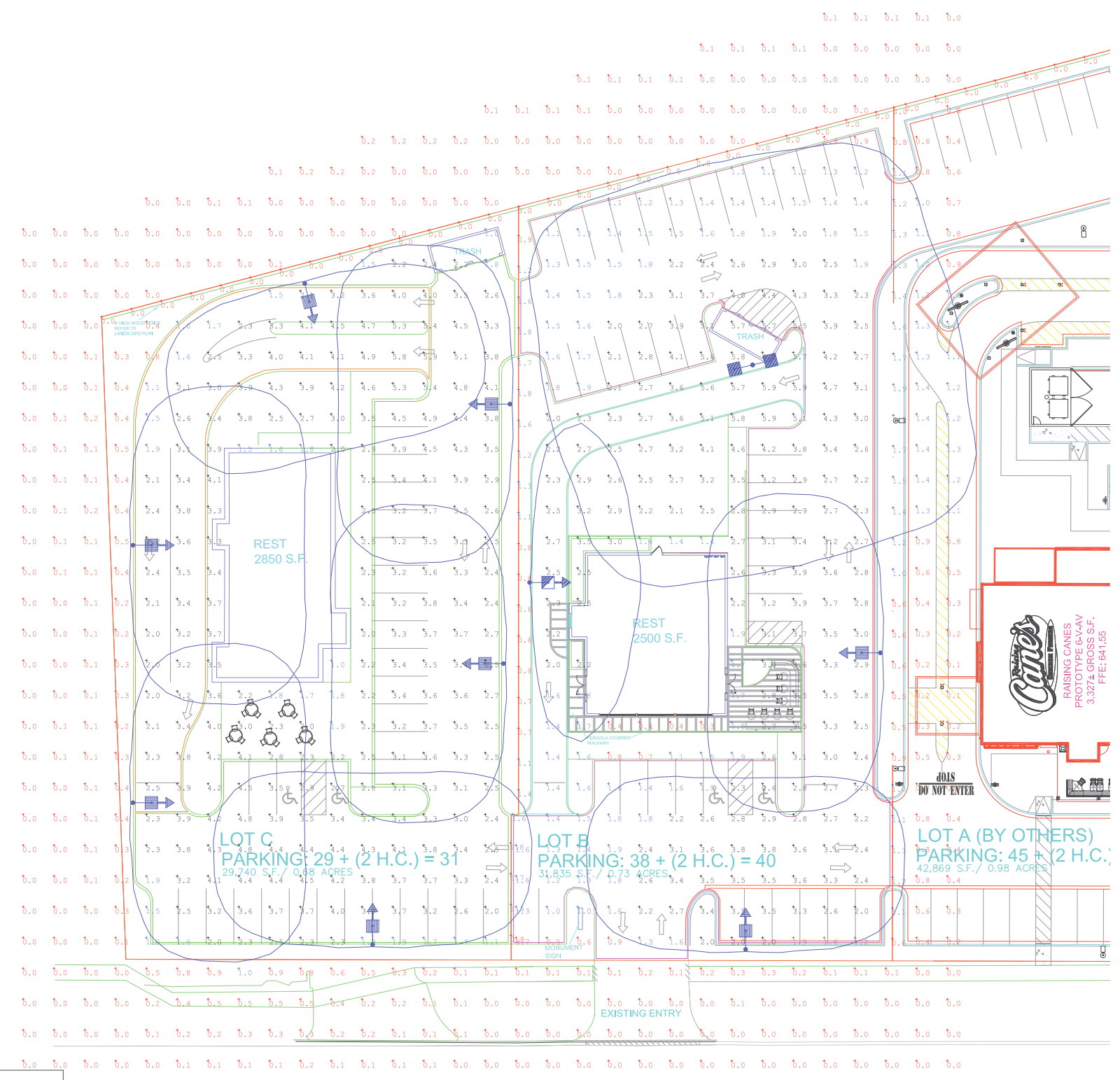
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PRELIMINARY UTILITY PLAN
 ISSUE DATE: 01/09/2023
 JOB NO. 22297
C4.0
 SHEET NO.



| REVISIONS | | |
|-----------|---------|------|
| REV # | DATE | BY: |
| 1 | 2/6/23 | J.P. |
| 2 | 2/20/23 | J.P. |
| 3 | 2/28/23 | J.P. |
| 4 | 3/20/23 | J.P. |
| 5 | 4/10/23 | J.P. |



6820 CORPORATION PKWY
FORT WORTH, TX 76126
WWW.WLSLIGHTING.COM

WLS LIGHTING a WLS company

2777 MANNEHEIM STREE
DES PLAINS, IL

WLS-17278 DATE -1/25/23 SCALE: 1"=20'
 817-731-0020 BY: J.P. PM:HOLLY SHEET 1 OF 1

| Calculation Summary | | | | | | | | |
|---------------------|-------|------|-----|-----|---------|---------|---------|---------|
| Label | Units | Avg | Max | Min | Avg/Min | Max/Min | PtSpcLr | PtSpcTb |
| LOT LINES | Fc | 1.16 | 1.9 | 0.0 | N.A. | N.A. | 10 | N.A. |
| REAR PROPERTY LINE | Fc | 0.00 | 0.0 | 0.0 | N.A. | N.A. | 10 | N.A. |
| LOT B DRIVE THRU | Fc | 2.90 | 5.9 | 1.4 | 2.07 | 4.21 | 10 | 10 |
| LOT B PARKING | Fc | 2.50 | 5.7 | 0.5 | 5.00 | 11.40 | | |
| LOT C DRIVE THRU | Fc | 3.15 | 5.9 | 1.1 | 2.86 | 5.36 | | |
| LOT C PARKING | Fc | 3.35 | 5.1 | 1.1 | 3.05 | 4.64 | | |

BASED ON THE INFORMATION PROVIDED, ALL DIMENSIONS AND LUMINAIRE LOCATIONS SHOWN REPRESENT RECOMMENDED POSITIONS. THE ENGINEER AND/OR ARCHITECT MUST DETERMINE APPLICABILITY OF THE LAYOUT TO EXISTING OR FUTURE FIELD CONDITIONS.

THIS LIGHTING PATTERN REPRESENTS ILLUMINATION LEVELS CALCULATED FROM LABORATORY DATA TAKEN UNDER CONTROLLED CONDITIONS UTILIZING CURRENT INDUSTRY STANDARD LAMP RATINGS IN ACCORDANCE WITH ILLUMINATING ENGINEERING SOCIETY APPROVED METHODS. ACTUAL PERFORMANCE OF ANY MANUFACTURER'S LUMINAIRE MAY VARY DUE TO VARIATION IN ELECTRICAL VOLTAGE, TOLERANCE IN LAMPS AND OTHER VARIABLE FIELD CONDITIONS.

| Luminaire Schedule | | | | | | |
|--------------------|-----|-------|-------------|-------|---|------------|
| Symbol | Qty | Label | Lum. Lumens | LLF | Description | Lum. Watts |
| | 1 | A | 25037 | 0.950 | WLS-MIM-LED-24L-SIL-5W-50-70CRI-SLW 25' POLE 3' BASE | 176 |
| | 8 | B | 16436 | 0.950 | WLS-MIM-LED-24L-SIL-FT-50-70CRI-IL-SLW 25' POLE 3' BASE | 176 |
| | 1 | C | 7984 | 0.950 | WLS-MIM-LED-12L-SIL-2-50-70CRI-IL-SLW 25' POLE 3' BASE | 85 |



Restaurant Support Office
6800 Bishop Road, Plano, TX 75024
Tel: 972-769-3100 Fax: 972-769-3101

STORE:
RAISING CANE'S RESTAURANT
2777 MANHEIM RD
DES PLAINES, IL 60018
PROTOTYPE: P6-V-AV
SCHEME: B
RESTAURANT #RC1009



17710 Detroit Avenue - Lakewood, Ohio, 44107
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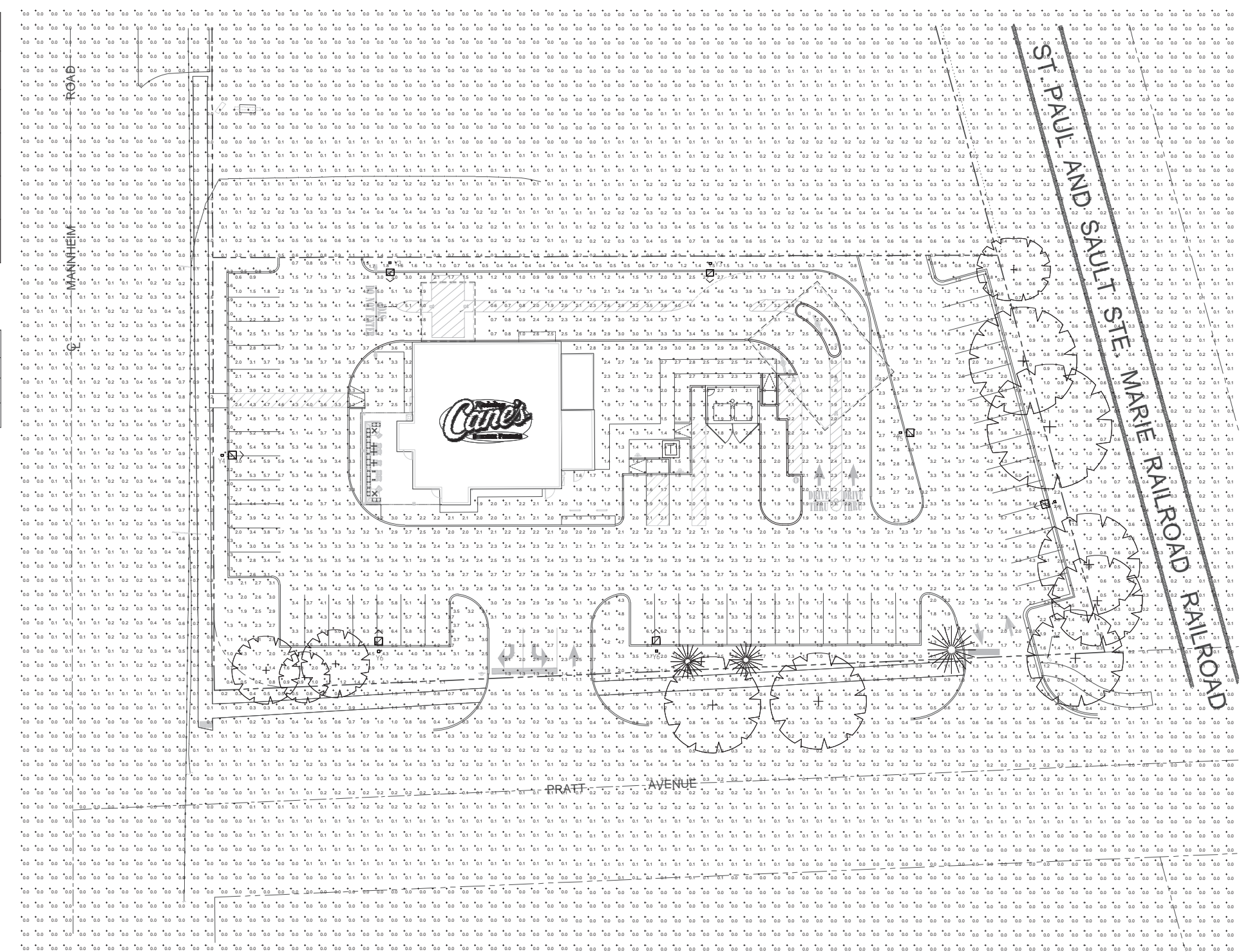
ENGINEER INFORMATION:

| SHEET REVISIONS | | |
|-----------------|------|------|
| # | DATE | TYPE |
| 1 | | |
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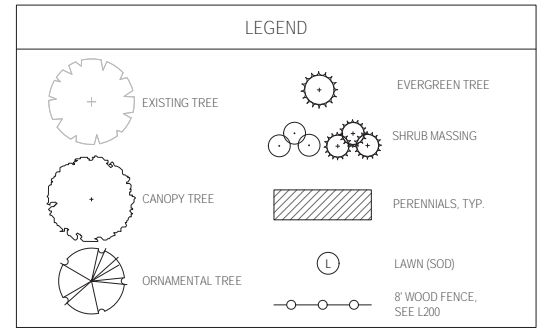
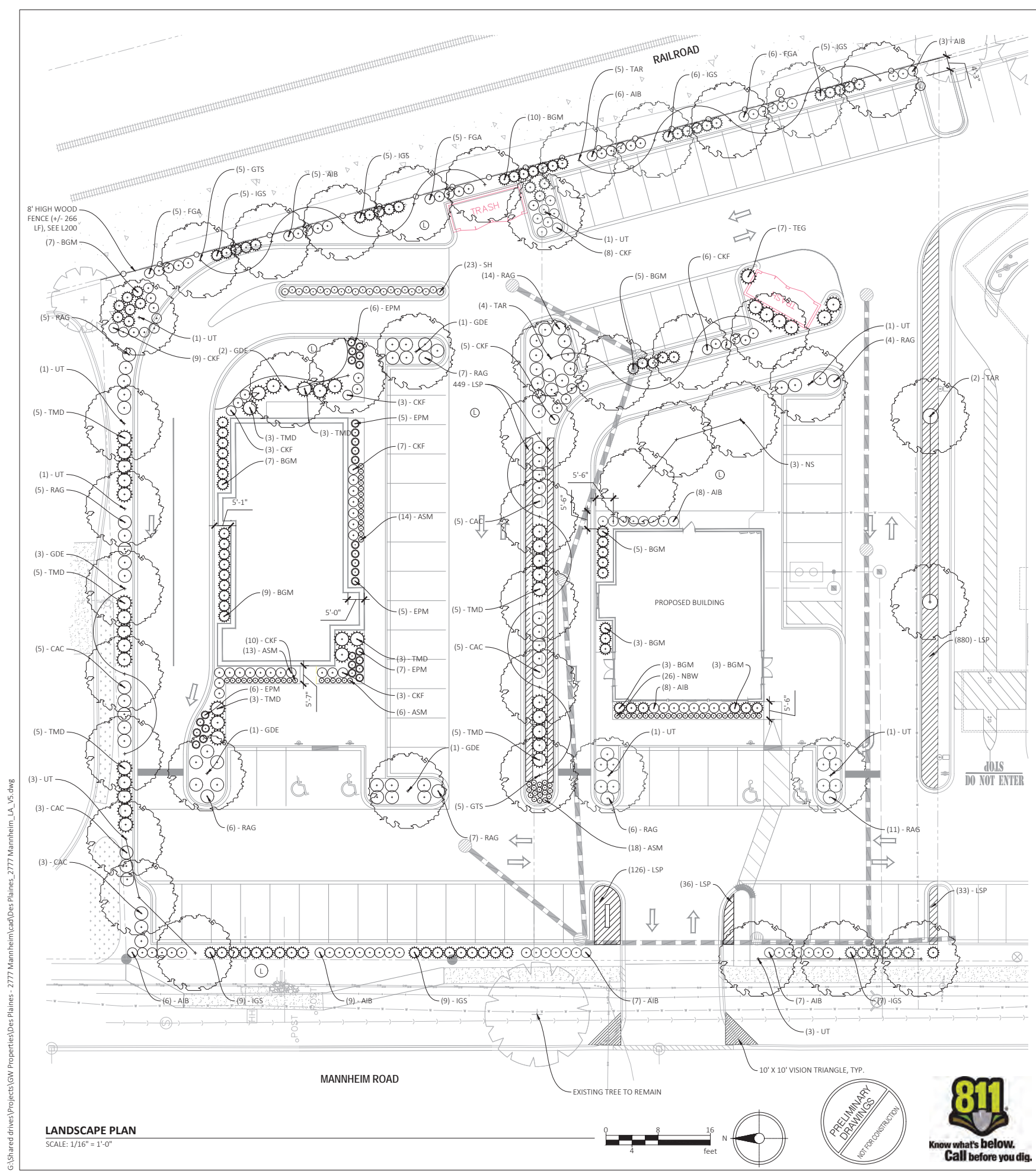
PHOTOMETRIC SITE PLAN
ISSUE DATE: 04/03/2023
JOB NO. 22297
PH1.0
SHEET NO.

| EXTERIOR LIGHTING FIXTURE SCHEDULE | | | | |
|------------------------------------|------|-------|---|--|
| TYPE | LAMP | WATTS | DESCRIPTION | CATALOG NO. |
| Y3 | LED | 189 | LED POLE MOUNTED FIXTURE WITH TYPE III DISTRIBUTION, HOUSE SIDE SHIELD & DARK BRONZE FINISH, MOUNTED ON A 25'-0" TALL SQUARE STRAIGHT STEEL POLE. | LITHONIA® RSX2-LED-P4-50K-R3-MVOLT- HS-SPA-DOBKD POLE-LITHONIA® SSS-25-SG-DMFRAS-DOBKD |
| Y4 | LED | 189 | LED POLE MOUNTED FIXTURE WITH TYPE IV DISTRIBUTION, HOUSE SIDE SHIELD & DARK BRONZE FINISH, MOUNTED ON A 25'-0" TALL SQUARE STRAIGHT STEEL POLE. | LITHONIA® RSX2-LED-P4-50K-R4-MVOLT- HS-SPA-DOBKD POLE-LITHONIA® SSS-25-SG-DMFRAS-DOBKD |
| Y5 | LED | 189 | LED POLE MOUNTED FIXTURE WITH TYPE V DISTRIBUTION & DARK BRONZE FINISH, MOUNTED ON A 25'-0" TALL SQUARE STRAIGHT STEEL POLE. | LITHONIA® RSX2-LED-P4-50K-R5-MVOLT- SPA-DOBKD POLE-LITHONIA® SSS-25-SG-DMFRAS-DOBKD |
| Y6 | LED | 189 | LED POLE MOUNTED FIXTURE WITH TYPE IV DISTRIBUTION & DARK BRONZE FINISH, MOUNTED ON A 25'-0" TALL SQUARE STRAIGHT STEEL POLE. | LITHONIA® RSX2-LED-P4-50K-R4-MVOLT- SPA-DOBKD POLE-LITHONIA® SSS-25-SG-DMFRAS-DOBKD |

| STATISTICS (VALUES ARE MAINTAINED) | | | | | |
|------------------------------------|--------|--------|--------|---------|---------|
| Description | Avg | Max | Min | Max/Min | Avg/Min |
| PARKING FIELD | 2.8 fc | 6.7 fc | 0.3 fc | 22.3:1 | 9.3:1 |
| PROPERTY LINE | 0.0 fc | 1.9 fc | 0.0 fc | N/A | N/A |



PHOTOMETRIC SITE PLAN
SCALE 1" = 20'-0"



| PLANTING SCHEDULE | | | | | | |
|--------------------------------------|-------|---------------------------------|----------------------------------|-----------|----------|--------------|
| KEY | QTY. | BOTANICAL NAME | COMMON NAME | SIZE | SPACING | COMMENTS |
| DECIDUOUS TREES | | | | | | |
| GDE | 8 | GYMNOCLADUS DIOCUS 'ESPRESSO' | ESPRESSO KENTUCKY COFFEETREE | 2.5" CAL. | 35" O.C. | B&B SPECIMEN |
| GTS | 10 | GLEDITSIA TRIACANTHOS 'SKYCOLE' | SKYLINE HONEYLOCUST | 2.5" CAL. | 30" O.C. | B&B SPECIMEN |
| NS | 3 | NYSSA SYLVATICA | BLACK GUM | 2.5" CAL. | 25" O.C. | B&B SPECIMEN |
| TAR | 11 | TILIA AMERICANA 'REDMOND' | REDMOND AMERICAN LINDEN | 2.5" CAL. | 30" O.C. | B&B SPECIMEN |
| UT | 13 | ULMUS 'MORTON GLOSSY' | TRIUMPH ELM | 2.5" CAL. | 40" O.C. | B&B SPECIMEN |
| EVERGREEN TREES | | | | | | |
| TEG | 7 | THUJA OCCIDENTALIS 'SMARAGD' | EMERALD GREEN ARBORVITAE | 6'-8" HT. | 4' O.C. | B&B SPECIMEN |
| DECIDUOUS SHRUBS | | | | | | |
| AIB | 59 | ARONIA MELANOCARPA 'MORTON' | IROQUOIS BEAUTY CHOKEBERRY | #3 CONT. | 36" O.C. | |
| CAC | 21 | CLETHRA ALNIFOLIA 'CALEB' | VANILLA SPICE SUMMERSWEET | #3 CONT. | 48" O.C. | |
| FGA | 16 | FOTHERGILLA GARDENII | DWARF FOTHERGILLA | #3 CONT. | 36" O.C. | |
| RAG | 65 | RHUS AROMATICA 'GRO LOW' | GRO-LOW SUMAC | #3 CONT. | 48" O.C. | |
| EVERGREEN SHRUBS | | | | | | |
| BGM | 52 | BUXUS 'GREEN MOUNTAIN' | GREEN MOUNTAIN BOXWOOD | #3 CONT. | 36" O.C. | |
| IGS | 47 | ILEX GLABRA 'SHAMROCK' | SHAMROCK INKBERRY | #3 CONT. | 36" O.C. | |
| TMD | 37 | TAXUS x MEDIA 'DENSIFORMIS' | DENSE YEW | #3 CONT. | 48" O.C. | |
| ORNAMENTAL GRASSES | | | | | | |
| CKF | 54 | CALAMOGROSTIS X 'KARL FOERSTER' | KARL FOERSTER FEATHER REED GRASS | #3 CONT. | 36" O.C. | |
| SH | 23 | SPOROBOLUS HETEROLEPIS | PRAIRIE DROPSEED | #1 | 24" O.C. | |
| PERENNIALS & GROUNDCOVERS | | | | | | |
| ASM | 51 | ALLIUM 'MILLENIUM' | MILLENIUM ALLIUM | #1 CONT. | 18" O.C. | |
| EPM | 29 | ECHINACEA 'CBG CONEZ' | PIXIE MEADOWBRITE CONEFLOWER | #1 CONT. | 24" O.C. | |
| LSP | 1,524 | LIRIOPE SPICATA | CREeping LILYTURF | QUARTS | 12" O.C. | |
| NBW | 26 | NEPETA 'BLUE WONDER' | BLUE WONDER NEPETA | #1 CONT. | 18" O.C. | |

| LANDSCAPE ORDINANCE REVIEW: DES PLAINES, ILLINOIS | | | |
|---|---|--|---|
| SPECIFIC ORDINANCE | CODE REQUIRES | CALCULATION | COMPLIANCE |
| 12-10-8: PARKING LOT LANDSCAPING | INTERIOR PARKING LOT LANDSCAPING: NOT LESS THAN FIVE PERCENT OF THE INTERIOR OF PARKING LOT SHALL BE DEVOTED TO LANDSCAPING. LANDSCAPED AREAS ALONG THE PERIMETER OF THE PARKING LOT BEYOND THE CURB OF EDGE OF PAVEMENT SHALL NOT BE COUNTED TOWARD THE SATISFACTION OF THIS REQUIREMENT. | INTERIOR OF PARKING LOT = 38,570 SF; 5% OF 38,570 SF = 1,929 SF REQUIRED. | 2,479 SF PROVIDED. |
| | INTERIOR PARKING LOT LANDSCAPING: A MINIMUM OF 50 PERCENT OF EVERY INTERIOR PARKING LOT LANDSCAPING AREA SHALL BE PLANTED WITH AN APPROVED GROUND COVER IN THE APPROPRIATE DENSITY TO ACHIEVE COMPLETE COVER WITHIN TWO YEARS. MULCH MAY BE SUBSTITUTED FOR OTHER GROUND COVER IF IT IS APPLIED TO 100 PERCENT OF THE LANDSCAPING AREA AT A THICKNESS OF AT LEAST TWO INCHES. | ALL INTERIOR LANDSCAPING AREAS PLANTED WITH GROUND COVER ON GROUND PLANE. | 100% PROVIDED. |
| | INTERIOR PARKING LOT LANDSCAPING: ONE SHADE TREE SHALL BE PROVIDED FOR EVERY ONE HUNDRED SF OF LANDSCAPING AREA. | 2,479 SF OF LANDSCAPING AREA PROVIDED; 2,479 / 100 = 25 TREES REQUIRED. | 21 TREES PROVIDED. TREES HAVE BEEN MAXIMIZED GIVEN THE CONSTRAINTS OF THE SITE. 4 ADDITIONAL TREES PROVIDED ALONG THE PERIMETER WHERE MORE SPACE IS AVAILABLE TO ACHIEVE THE 25 TREE QUANTITY REQUIREMENT. SEE ROW BELOW. |
| | PERIMETER PARKING LOT LANDSCAPING - SHADE TREES: ONE SHADE TREE FOR EVERY 40 LF OF PERIMETER LANDSCAPE AREA LENGTH. TREES MAY BE CLUSTERED. | TOTAL PERIMETER LANDSCAPE AREA LENGTH = 600 LF. 600/40 = 15 TREES REQUIRED. | 24 TREES PROVIDED (INCLUDES 4 EXTRA TREES FROM ROW ABOVE). UNDERGROUND AND OVERHEAD UTILITIES PREVENT PERIMETER TREES FROM BEING PLANTED ALONG MANNHEIM ROAD. |
| | PERIMETER PARKING LOT LANDSCAPING - SHRUBS: ONE SHRUB, MEASURING A MINIMUM OF 18 INCHES AT PLANTING AND NOT TO EXCEED FOUR FEET AT MATURITY, FOR EVERY THREE FEET OF PERIMETER LANDSCAPE AREA LENGTH, CLUSTERED OR SPACED LINEARLY. THE LANDSCAPE TREATMENT SHALL RUN 75 PERCENT OF THE YARD WHEN ABUTTING A NON-RESIDENTIAL USE. | TOTAL PERIMETER LANDSCAPE AREA LENGTH = 600 LF. 600 LF * 75% = 450 LF; 450 LF / 3 = 150 SHRUBS REQUIRED. | 150 SHRUBS PROVIDED. |
| 12-10-10: FOUNDATION PLANTING | INTERIOR LOTS: ALL INTERIOR LOTS SHALL BE IMPROVED WITH A MINIMUM LANDSCAPE AREA OF 25% OF THE FOUNDATION OF THE PRINCIPLE STRUCTURES WITH AN EMPHASIS ON STREET-FACING ELEVATIONS. | FOUNDATION = 425 LF; 512 LF * 25% = 107 LF OF FOUNDATION PLANTING REQUIRED. | 252 LF OF FOUNDATION PLANTING PROVIDED. |

PROJECT NAME:
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2777 MANNHEIM DES PLAINES, IL

OWNER NAME:
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DATE: 01.26.23
REVISION: 02.03.23
DATE: 03.01.23
REVISION: 03.20.23
DATE: 04.10.23
REVISION: 04.10.23

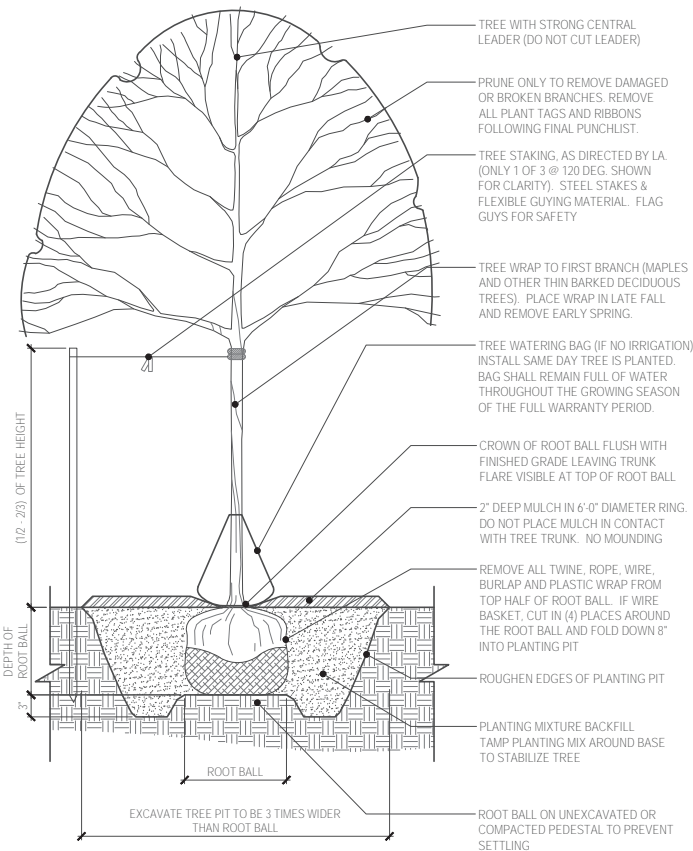
STAMP:
JAMES J. POWERS
PROFESSIONAL LANDSCAPE ARCHITECT
STATE OF ILLINOIS

TITLE:
LANDSCAPE PLAN

SHEET:
L100

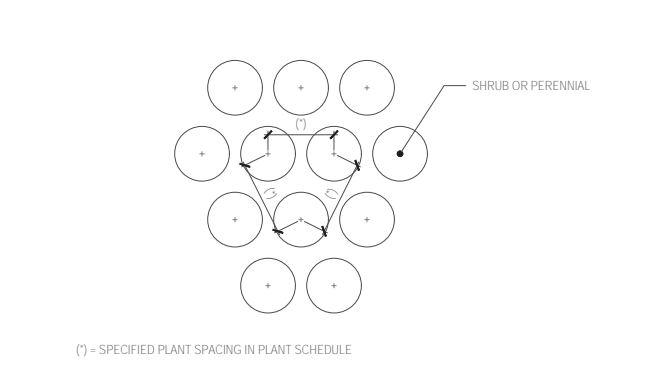
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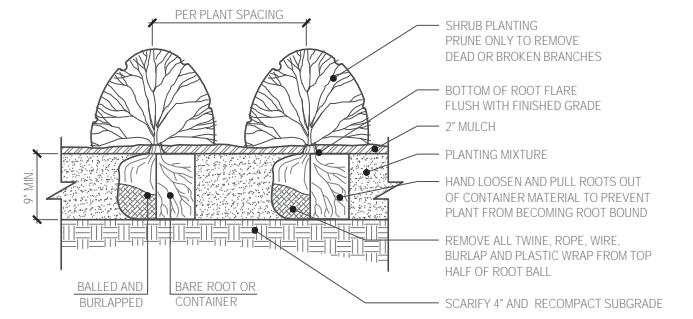
1 TREE PLANTING

SCALE: 1/2" = 1'-0"



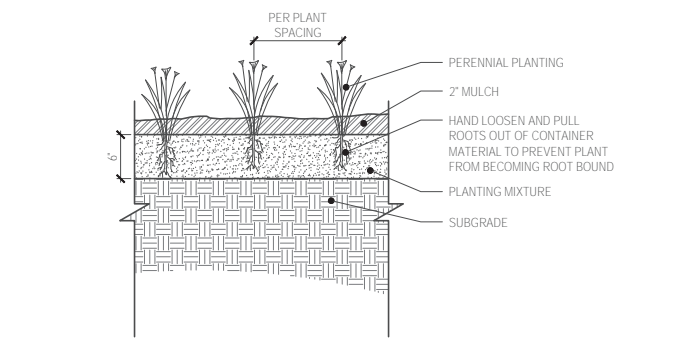
4 PLANT SPACING

NOT TO SCALE



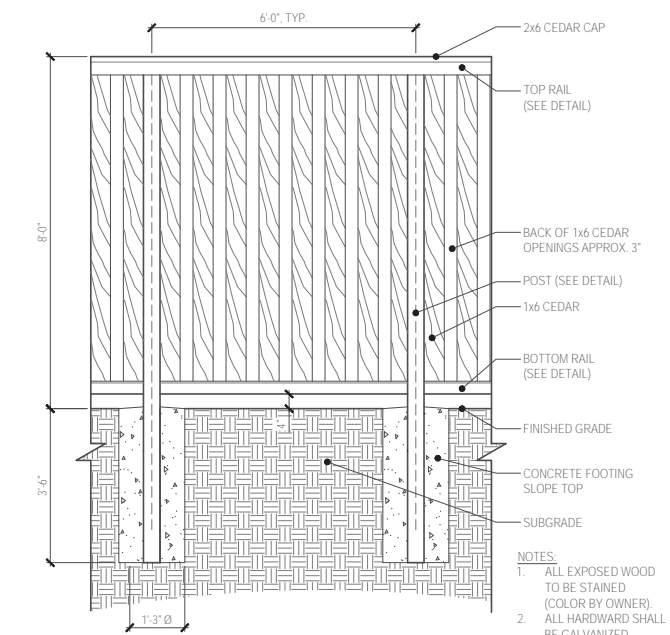
2 SHRUB PLANTING

SCALE: 1" = 1'-0"



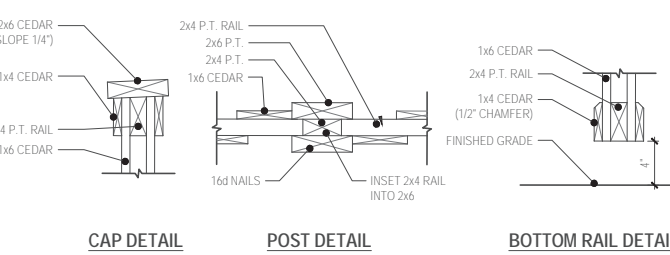
3 PERENNIAL PLANTING

SCALE: 1" = 1'-0"



5 WOOD FENCE (VERTICAL SLATS)

SCALE: 1/2" = 1'-0"



6 WOOD FENCE (VERTICAL SLATS) - ENLARGEMENTS

SCALE: 1-1/2" = 1'-0"

PLANTING NOTES

- SEE SHEET L100 FOR LANDSCAPE ORDINANCE REVIEW DATA TABLE, SWORN STATEMENTS AND PLANT SCHEDULE.
- THE CONTRACTOR SHALL OBTAIN AND PAY FOR ALL PERMITS AND FEES THAT MAY BE REQUIRED FOR HIS PORTION OF WORK.
- THE CONTRACTOR SHALL CONTACT 811 PRIOR TO WORK.
- IN CASE OF DISCREPANCIES BETWEEN THE PLAN AND THE PLANT LIST, THE GRAPHIC SYMBOLS SHOWN ON THE PLAN SHALL DICTATE.
- PLANT MATERIALS:
 - ALL PLANT MATERIALS SHALL MEET OR EXCEED THE AMERICAN STANDARDS FOR NURSERY STOCK, MOST CURRENT EDITION, AS SET FORTH BY AMERICAN ASSOCIATION OF NURSERYMEN.
 - PLANTS SHALL BE EQUAL TO OR EXCEED THE MEASUREMENTS SPECIFIED IN THE PLANT LIST.
 - PLANTS SHALL BE SOUND, HEALTHY, VIGOROUS AND FREE FROM INSECT PESTS, PLANT DISEASES, AND INJURIES.
 - TREES SHALL HAVE STRAIGHT TRUNK WITH LEADER INTACT, UNDAUNTED AND UNMAY. BRANCHING MUST BE WELL DEVELOPED.
 - ALL PLANT MATERIAL AND SEED SHALL BE PROVIDED FROM A NURSERY (WITHIN 200 MILES) WITH A SIMILAR PLANT HARDINESS ZONE AS PROJECT LOCATION.
 - NO SUBSTITUTIONS OF PLANT MATERIALS WILL BE ALLOWED. IF PLANTS ARE NOT AVAILABLE, THE CONTRACTOR SHALL NOTIFY OWNER AND LANDSCAPE ARCHITECT PRIOR TO BID IN WRITING.
 - ALL PLANTS ARE SUBJECT TO INSPECTION AND APPROVAL. THE LANDSCAPE ARCHITECT AND OWNER RESERVE THE RIGHT TO SELECT AND TAG ALL PLANT MATERIAL AT THE NURSERY PRIOR TO PLANTING AND REJECT UNACCEPTABLE PLANT MATERIAL AT ANY TIME DURING THE PROGRESS OF THE PROJECT.
 - CONTRACTOR SHALL NOTIFY LANDSCAPE ARCHITECT IN WRITING PRIOR TO BID DATE OF ANY PLANTS THEY FEEL MAY NOT SURVIVE IN LOCATIONS NOTED ON PLANS.
- IRRIGATION:
 - CONTRACTOR SHALL PROVIDE BID ALTERNATE FOR IRRIGATION PER THE IRRIGATION PERFORMANCE SPECIFICATIONS.
 - IF BID ALTERNATE OF IRRIGATION SYSTEM IS NOT SELECTED BY OWNER, CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHMENT WATERING THROUGH TEMPORARY FACILITIES, WATERING BAGS, ETC., AS APPROVED BY OWNER FOR PLANT WARRANTY.
- TOPSOIL & PLANTING MIXTURES:
 - ENSURE THAT SOIL CONDITIONS AND COMPACTION ARE ADEQUATE TO ALLOW FOR PROPER DRAINAGE AROUND THE CONSTRUCTION SITE. UNDESIRABLE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO BEGINNING OF WORK. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE PROPER SURFACE AND SUBSURFACE DRAINAGE IN ALL AREAS
 - SALVAGE TOPSOIL FROM THE EARTHWORK AREAS AS APPROPRIATE AND/OR AS DIRECTED BY LANDSCAPE ARCHITECT AND STOCKPILE FOR REUSE IN LOCATION APPROVED BY OWNER.
 - TOPSOIL SHALL BE MATERIALS CONSISTING OF FERTILE, FRIABLE, FINE SANDY LOAM, UNIFORM IN COMPOSITION AND FREE OF SUBSOIL, STONES, LUMPS, CLODS OF HARD EARTH, PLANTS, PLANT ROOTS, STICKS, NOXIOUS WEEDS, SLAG, CINDERS, DEMOLITION DEBRIS OR OTHER EXTRANEIOUS MATTER OVER 1" IN LARGEST DIMENSION.
 - EXISTING TOPSOIL SHALL BE PREPARED BY THOROUGHLY MIXING IN COMPOST AT THE RATE OF 1/3 VOLUME OF SOIL REPLACED.
 - TOPSOIL SHALL BE TESTED AND AMENDED (AS SPECIFIED BY THE TESTING AGENCY) TO THE FOLLOWING:
 - ADJUST SOIL TO A pH OF 6.0 TO 6.5.
 - ORGANIC MATTER: 4% MIN, 10% MAX
 - AVAILABLE PHOSPHORUS: 25 PPM, MIN
 - EXCHANGEABLE POTASSIUM: 125 PPM, MIN
 - THE FOLLOWING FERTILIZERS SHALL BE USED AS FOLLOWS, OR ALTERNATIVES SUBMITTED BY CONTRACTOR TO OWNER AND LANDSCAPE ARCHITECT FOR APPROVAL:
 - TREES & SHRUBS = 14-4-6 BRIQUETTES @ 17g
 - LAWN = HIGH NITROGEN STARTER FERTILIZER
 - LAWN SEED & SOD AREAS SHALL RECEIVE A MINIMUM OF 4" DEPTH OF TOPSOIL.
 - PLANTING BEDS SHALL RECEIVE MINIMUM 6" DEPTH OF AMENDED TOPSOIL.
 - NATIVE LANDSCAPE SEEDING AREAS SHALL RECEIVE A MINIMUM 18" DEPTH OF TOPSOIL.
- MULCH MATERIALS:
 - ALL MULCH MATERIALS SHALL BE PROCESSED DOUBLE SHREDDED HARDWOOD BARK MULCH OF UNIFORM SIZE. NO UTILITY MULCH OR PROCESSED TREE TRIMMINGS WILL BE ALLOWED. SUBMIT SAMPLE TO ARCHITECT.
 - MULCH SHALL BE 2-INCH THICK MINIMUM COVERAGE IN ALL AREAS OF TREE PITS OR PLANTING BEDS, UNLESS OTHERWISE NOTED.
 - MULCH SHALL BE HELD 1" BELOW SURFACE ELEVATION OF DOWNHILL SIDE OF WALK, SLAB, CURB, LAWN, ETC.
- LANDSCAPE BED EDGING:
 - ALL LANDSCAPE BED EDGING SHALL BE SHOVEL-CUT SPADE EDGE BETWEEN LAWN AREAS UNLESS OTHERWISE NOTED.
- STORAGE & INSTALLATION:
 - CONFIRM LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO START OF CONSTRUCTION.
 - EXISTING TREES FOUND ON SITE SHALL BE PROTECTED AND SAVED UNLESS NOTED TO BE REMOVED OR ARE LOCATED IN AN AREA TO BE GRADED. NO VEHICLES OR EQUIPMENT ARE ALLOWED WITHIN THE DRIP LINE OF TREES TO BE PROTECTED. QUESTIONS REGARDING EXISTING PLANT MATERIAL SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO REMOVAL.
 - PRUNING AND REMOVAL OF BRANCHES ON EXISTING TREES SHALL BE DIRECTED IN THE FIELD BY OWNER OR LANDSCAPE ARCHITECT.
 - EQUIPMENT, PLANTS AND ALL OTHER MATERIALS TO BE STORED ON SITE WILL BE STORED OUTSIDE OF THE DRIPLINE OF TREES TO BE PROTECTED AND PLACED WHERE THEY WILL NOT CONFLICT W/ CONSTRUCTION OPERATIONS.
 - NEW PLANTING AREAS ARE TO BE TREATED WITH HERBICIDE (APPROVED BY STATE CHEMIST) TO KILL ALL EXISTING GROUND COVER. THERE SHALL BE A MINIMUM OF TWO (2) APPLICATIONS SEPARATED BY 10 DAYS. IF ALL EXISTING GROUND COVER VEGETATION IS NOT KILLED WITHIN 10 DAYS OF 2ND APPLICATION, A 3RD APPLICATION IS REQUIRED.
 - WHERE PROPOSED PLANTINGS ARE INDICATED IN EXISTING PAVING AREAS, CONTRACTOR SHALL EXCAVATE A MINIMUM OF 2'-0" BELOW PAVING SURFACE.
 - FINAL PLACEMENT OF PLANT MATERIALS, ETC., ARE SUBJECT TO APPROVAL BY OWNER AND LANDSCAPE ARCHITECT BEFORE PLANTING OPERATIONS ARE TO PROCEED. ALL TREE LOCATIONS SHALL BE MARKED WITH A WOOD STAKE OR FLAG INDICATING VARIETY AND SIZE OF TREE. ALL GROUND COVER AND PLANTING BED LINES SHALL BE MARKED W/ HIGHLY VISIBLE PAINT LINES W/ OCCASIONAL WOOD STAKES FOR REFERENCE. ALL STAKES SHALL BE REMOVED FOLLOWING PLANTING OPERATIONS. OWNER RESERVES THE RIGHT TO ADJUST PLANT LOCATIONS ON SITE.
 - ALL DISTURBED AREAS OUTSIDE THE LIMITS OF WORK SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION AT NO ADDITIONAL COST TO THE OWNER.
 - PRIOR TO FINAL PAYMENT, CONTRACTOR SHALL COORDINATE A FINAL INSPECTION WALK-THROUGH WITH OWNER AND LANDSCAPE ARCHITECT FOR OWNER ACCEPTANCE. THE LANDSCAPE ARCHITECT WILL PROVIDE A PUNCHLIST OF ANY DEFICIENCIES AND PROVIDE TO OWNER AND CONTRACTOR FOR REVIEW AND REMEDIATION.
- MAINTENANCE:
 - INCLUDE PRICING WITH THE BID FOR A 60-DAY MAINTENANCE PERIOD OF ALL LANDSCAPE PLANTINGS FOLLOWING COMPLETE INSTALLATION AND FINAL INSPECTION BY OWNER AND LANDSCAPE ARCHITECT. MAINTENANCE SHALL INCLUDE WATERING, WEEDING, CULTIVATING, MULCHING, MOWING, AND ALL OTHER NECESSARY OPERATIONS REQUIRED FOR PROPER ESTABLISHMENT OF LAWNS AND PLANTINGS.
- WARRANTY:
 - ALL LANDSCAPE PLANTINGS SHALL BE WARRANTED FOR A PERIOD OF ONE YEAR FOLLOWING 60-DAY MAINTENANCE PERIOD. AT THE END OF THIS PERIOD, PLANT MATERIAL TERMED DEAD OR UNSATISFACTORY (EXCEPT FOR DEFECTS RESULTING FROM ABUSE OR DAMAGE BY OTHERS, OR OTHER ACTS DETERMINED AS FORCE MAJEURE) BY OWNER AND LANDSCAPE ARCHITECT SHALL BE REPLACED AT NO ADDITIONAL CHARGE BY THE CONTRACTOR. THE REPLACEMENTS SHALL ALSO BE WARRANTED FOR 1 YEAR.

IRRIGATION NOTES:

- CONTRACTOR SHALL PROVIDE DESIGN/BUILD IRRIGATION SYSTEM PER THE IRRIGATION NOTES BELOW:
 - DESIGN GUIDELINES: CONTRACTOR TO VERIFY PRESSURE AND AVAILABLE WATER SERVICE SIZE
 - EMISSION (LAWNS): HUNTER I-40 SPRAY ROTARS (OR APPROVED EQUAL)
 - DRIP (BEDS): HUNTER HDL-CV (OR APPROVED EQUAL)
 - QUICK COUPLER: HUNTER QC-V - 3RC
 - CONTROLLER: HUNTER HCC (OR APPROVED EQUAL)
 - SENSOR: HUNTER SOLAR-SYNC & HC FLOW METER (OR APPROVED EQUAL)
 - PIPING: PVC OR APPROVED EQUAL
 - INSECT REPELLENT: AUTOMATED PRO FEEDER SYSTEM WITH 5-GALLON STORAGE FOR NATURE SHIELD
- CONTRACTOR SHALL PROVIDE A QUALIFIED IRRIGATION DESIGNER OR IRRIGATION CONSULTANT TO DESIGN THE SYSTEM FOR EFFICIENT AND UNIFORM DISTRIBUTION OF WATER. "QUALIFIED" MEANS CERTIFIED BY ONE THE FOLLOWING AGENCIES BELOW:
 - CERTIFIED IRRIGATION CONTRACTOR (CIC)
 - CERTIFIED LANDSCAPE IRRIGATION AUDITOR (CLIA)
 - CERTIFIED LANDSCAPE IRRIGATION MANAGER (CLIM)
 - CERTIFIED IRRIGATION DESIGNER (CID)
 - CERTIFIED WATER CONSERVATION MANAGER-LANDSCAPE (CWCM)
- SYSTEM DESIGN:
 - THE SYSTEM SHALL BE COMPRISED OF EITHER:
 - DRIP/MICRO-IRRIGATION COMPONENTS THAT ALLOW FOR HIGHER DISTRIBUTION UNIFORMITY AND LOWER EVAPORATION AND RUNOFF.
 - THE DESIGN AND LAYOUT OF THE EMISSION DEVICES PROVIDES FOR ZERO OVERSPRAY ACROSS OR ONTO A STREET, PUBLIC DRIVEWAY OR SIDEWALK, PARKING AREA, BUILDING, FENCE OR ADJOINING PROPERTY. OVERSPRAY MAY OCCUR DURING THE OPERATION OF THE IRRIGATION SYSTEM DUE TO THE ACTUAL WIND CONDITION THAT DIFFER FROM THE DESIGN CRITERIA.
- SYSTEM CONTROLLER:
 - THE SYSTEM SHOULD USE A CONTROLLER THAT HAS MULTI-PROGRAM CAPABILITY WITH AT LEAST FOUR START TIMES (FOR MULTIPLE REPEAT SOAK CYCLES) AND RUN TIME ADJUSTMENT IN ONE MINUTE INCREMENTS. THE CONTROLLER PROGRAMMING (SCHEDULING) SHOULD BE MANAGED TO RESPOND TO THE CHANGING NEED FOR WATER IN THE LANDSCAPE.
- DESIGN FEATURES:
 - FOLLOW ALL ORDINANCES RELATING TO IRRIGATION SYSTEMS INCLUDING THE INSTALLATION OF BACKFLOW DEVICES.
 - INSTALL A MASTER VALVE TO STOP UNSCHEDULED FLOW OF IRRIGATION WATER
 - A DESIGN THAT RESULTS IN UNIFORM AND EFFICIENT COVERAGE. SPRINKLER HEAD SPACING SHOULD BE A MINIMUM OF "HEAD-TO-HEAD" (MINIMUM 50% OF DIAMETER) UNLESS THE COVERAGE IS DESIGNED FOR WIND DE-RATING. WIND DE-RATING SHOULD BE BASED ON AVERAGE NIGHTTIME WIND SPEED.
 - A MINIMUM OF "HEAD-TO-HEAD" (MINIMUM OF 50% OF DIAMETER) UNLESS THE COVERAGE IS DESIGNED FOR WIND DE-RATING. WIND DE-RATING SHOULD BE BASED ON AVERAGE NIGHTTIME WIND SPEED.
 - HAVE SEPARATE STATIONS/ZONES (HYDROZONES) FOR AREAS WITH DISSIMILAR WATER OR SCHEDULING REQUIREMENTS
 - PROVIDE SENSOR TO SUSPEND IRRIGATION DURING WET WEATHER CONDITIONS.
 - PROVIDE FLOW METER FOR MONITORING FLOW CONDITIONS AND SAVING WATER.
 - PROVIDE OWNER WITH WALKTHROUGH FOR SYSTEM OPERATIONS, PRIOR TO FINAL ACCEPTANCE. INCLUDE PROCEDURES FOR CONTROLLER PROGRAMMING, MAINTENANCE AND WINTERIZATION.

PROJECT NAME:
PROPOSED DEVELOPMENT
2777 MANNHEIM
DES PLAINES, IL

OWNER NAME:
GW PROPERTIES
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| SUBMITTAL & REVISIONS | |
|-----------------------|----------------------------|
| 1 | 01.26.23 SCHEMATIC DESIGN |
| 2 | 02.02.23 REVISED SITE PLAN |
| 3 | 03.01.23 REVISED SITE PLAN |
| 4 | 03.20.23 REVISED SITE PLAN |
| 5 | 04.10.23 REVISED SITE PLAN |

STAMP:
LANDSCAPE ARCHITECT
JAMES J. POWERS
08-0000
OFFICE OF LANDSCAPE ARCHITECTURE

TITLE:
LANDSCAPE NOTES & DETAILS

SHEET:
L200

DRAWN BY: JIF
CHECK BY: JRR
PROJECT #: 23-003





Restaurant Support Office
6800 Bishop Road, Plano, TX 75024
Tel: 972-769-3100 Fax: 972-769-3101

STORE:
RAISING CANE'S RESTAURANT
2777 MANHEIM RD
DES PLAINES, IL 60018
PROTOTYPE: P6-V-AV
SCHEME: B
RESTAURANT #RC1009



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ENGINEER INFORMATION:

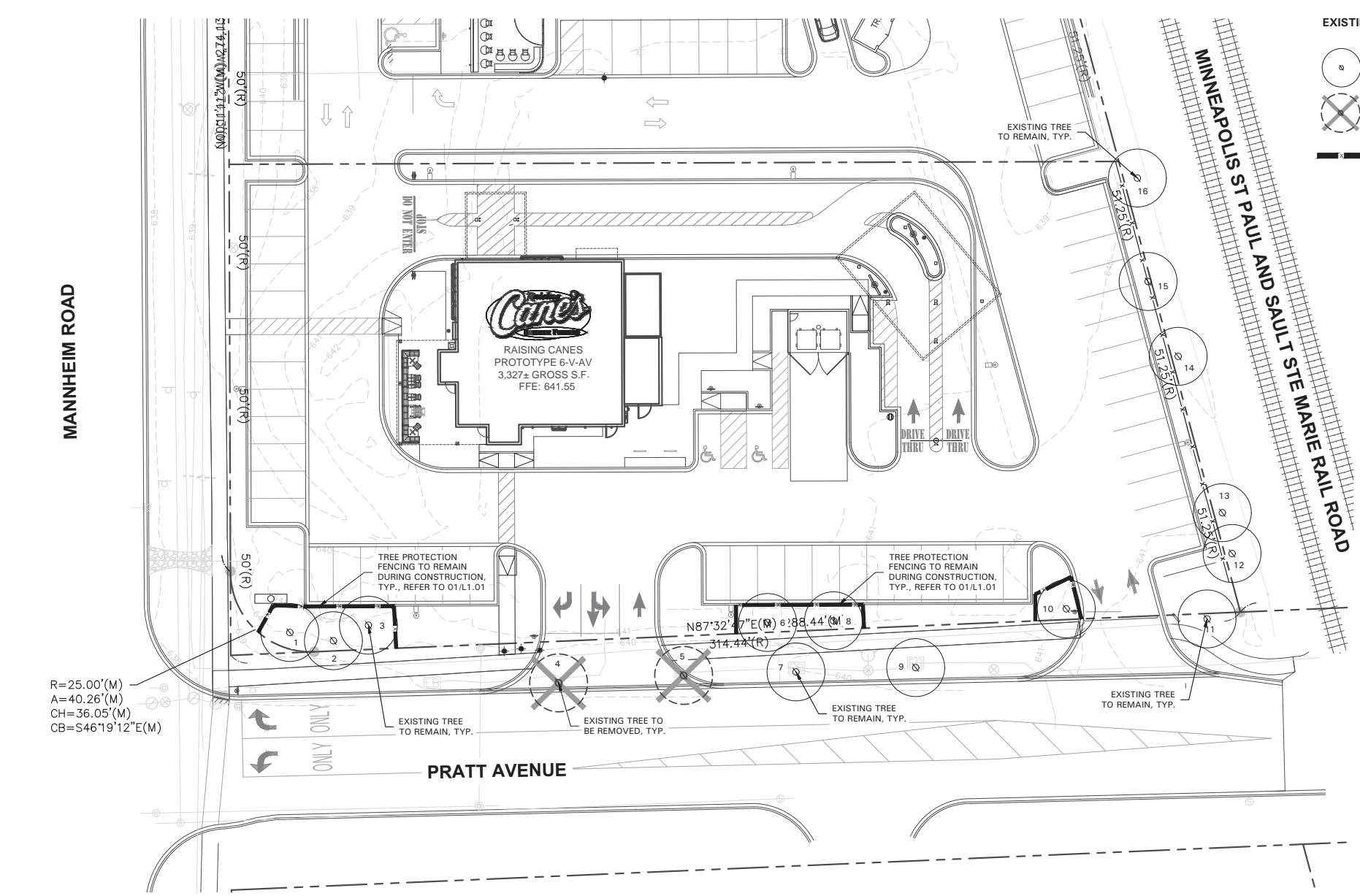


| TREE SURVEY FIELD DATA | | | | |
|------------------------|---------------|-----------------------|---------------|---------|
| No. | Dia. (inches) | Species (common name) | Status | Remarks |
| 1 | 12 | TREE | TO REMAIN | |
| 2 | 9 | TREE | TO REMAIN | |
| 3 | 12 | TREE | TO REMAIN | |
| 4 | 17 | TREE | TO BE REMOVED | |
| 5 | 17 | TREE | TO BE REMOVED | |
| 6 | 12 | TREE | TO REMAIN | |
| 7 | 17 | TREE | TO REMAIN | |
| 8 | 12 | TREE | TO REMAIN | |
| 9 | 17 | TREE | TO REMAIN | |
| 10 | 17 | TREE | TO REMAIN | |
| 11 | 17 | TREE | TO REMAIN | |
| 12 | 16 | TREE | TO REMAIN | |
| 13 | 18 | TREE | TO REMAIN | |
| 14 | 22 | TREE | TO REMAIN | |
| 15 | 19 | TREE | TO REMAIN | |
| 16 | 11 | TREE | TO REMAIN | |

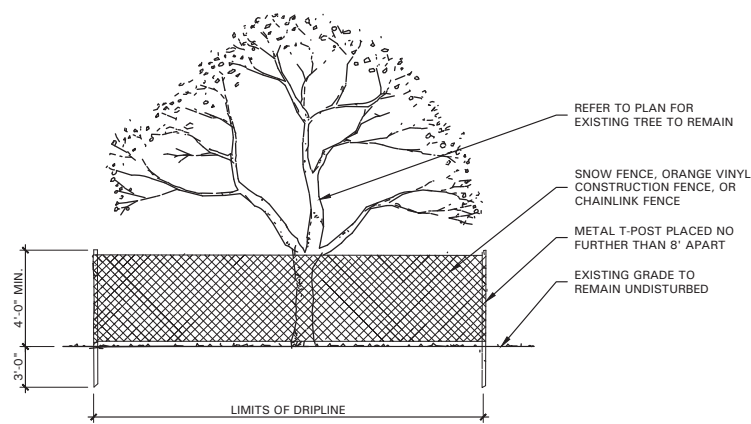
| | |
|----------------------------------|-----|
| Total Caliper Inches on Site | 245 |
| Total Caliper Inches Removed | 34 |
| Total Mitigation Inches Required | 0 |
| Total Mitigation Inches Provided | 0 |

TREE PRESERVATION NOTES

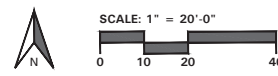
- EXISTING TREES TO REMAIN SHALL BE PROTECTED DURING CONSTRUCTION FROM TREE STRUCTURE DAMAGE AND COMPACTION OF SOIL UNDER AND AROUND DRIP LINE (CANOPY) OF TREE.
- IF ANY ROOT STRUCTURE IS DAMAGED DURING ADJACENT EXCAVATION / CONSTRUCTION, NOTIFY OWNER'S AUTHORIZED REPRESENTATIVE IMMEDIATELY. IT IS RECOMMENDED THAT A LICENSED ARBORIST BE SECURED FOR THE TREATMENT OF ANY POSSIBLE TREE WOUNDS.
- NO DISTURBANCE OF THE SOIL GREATER THAN 4" SHALL BE LOCATED CLOSER TO THE TREE TRUNK THAN 1/2 THE DISTANCE OF THE DRIP LINE TO THE TREE TRUNK. A MINIMUM OF 75% OF THE DRIP LINE AND ROOT ZONE SHALL BE PRESERVED AT NATURAL GRADE.
- ANY FINE GRADING DONE WITHIN THE CRITICAL ROOT ZONES OF THE PROTECTED TREES MUST BE DONE WITH LIGHT MACHINERY SUCH AS A BOBCAT OR LIGHT TRACTOR. NO EARTH MOVING EQUIPMENT WITH TRACKS IS ALLOWED WITHIN THE CRITICAL ROOT ZONE OF THE TREES.
- NO MATERIALS INTENDED FOR USE IN CONSTRUCTION OR WASTE MATERIALS ACCUMULATED DUE TO EXCAVATION OR DEMOLITION SHALL BE PLACED WITHIN THE LIMITS OF THE DRIP LINE OF ANY TREE.
- NO EQUIPMENT MAY BE CLEANED OR TOXIC SOLUTIONS, OR OTHER LIQUID CHEMICALS, SHALL BE DEPOSITED WITHIN THE LIMITS OF THE DRIP LINE OF A TREE, INCLUDING BUT NOT LIMITED TO: PAINT, OIL, SOLVENTS, ASPHALT, CONCRETE, MORTAR, PRIMERS, ETC.
- NO SIGNS, WIRES OR OTHER ATTACHMENTS, OTHER THAN THOSE OF A PROTECTIVE NATURE, SHALL BE ATTACHED TO ANY TREE.
- NO VEHICULAR / CONSTRUCTION EQUIPMENT TRAFFIC OR PARKING IS ALLOWED WITHIN THE LIMITS OF THE DRIP LINE OF TREES.
- BORING OF UTILITIES MAY BE PERMITTED UNDER PROTECTED TREES IN CERTAIN CIRCUMSTANCES. THE MINIMUM LENGTH OF THE BORE SHALL BE THE WIDTH OF THE TREE'S CANOPY AND SHALL BE A MINIMUM DEPTH OF FORTY-EIGHT (48") INCHES.
- IRRIGATION TRENCHING WHICH MUST BE DONE WITHIN THE CRITICAL ROOT ZONE OF A TREE SHALL BE DUG BY HAND AND ENTER THE AREA IN A RADIAL MANNER.
- ALL TREES TO BE REMOVED FROM THE SITE SHALL BE FLAGGED BY THE CONTRACTOR WITH BRIGHT RED VINYL TAPE (3" WIDTH) WRAPPED AROUND THE MAIN TRUNK AT A HEIGHT OF FOUR (4') FEET ABOVE GRADE. FLAGGING SHALL BE APPROVED BY OWNER'S AUTHORIZED REPRESENTATIVE PRIOR TO ANY TREE REMOVAL. CONTRACTOR SHALL CONTACT OWNER'S AUTHORIZED REPRESENTATIVE WITH 72 HOURS NOTICE TO SCHEDULE ON-SITE MEETING.
- ALL TREES TO REMAIN, AS NOTED ON DRAWINGS, SHALL HAVE PROTECTIVE FENCING LOCATED AT THE TREE'S DRIP LINE. THE PROTECTIVE FENCING MAY BE COMPRISED OF SNOW FENCING, ORANGE VINYL CONSTRUCTION FENCING, CHAIN LINK FENCE OR OTHER SIMILAR FENCING WITH A FOUR (4') FOOT APPROXIMATE HEIGHT. THE PROTECTIVE FENCING SHALL BE LOCATED AS INDICATED ON THE TREE PROTECTION DETAIL.
- WHEN A LOW HANGING LIMB IS BROKEN DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE OWNER'S AUTHORIZED REPRESENTATIVE IMMEDIATELY. UNDER NO CIRCUMSTANCE SHALL THE CONTRACTOR PRUNE ANY PORTION OF THE DAMAGED TREE WITHOUT THE PRIOR APPROVAL BY THE OWNER'S AUTHORIZED REPRESENTATIVE.



R=25.00'(M)
A=40.26'(M)
CH=36.05'(M)
CB=S46°19'12"E(M)



01 TREE PROTECTIVE FENCING
NOT TO SCALE



4245 North Central Expressway
Suite 501
Dallas, Texas 75205
214.865.7192 office

| SHEET REVISIONS | | |
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TREE PRESERVATION PLAN
ISSUE DATE: 04/14/2023
JOB NO. 22297
L1.01
SHEET NO.



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ENGINEER INFORMATION:



LANDSCAPE NOTES

- CONTRACTOR SHALL VERIFY ALL EXISTING AND PROPOSED SITE ELEMENTS AND NOTIFY LANDSCAPE ARCHITECT OF ANY DISCREPANCIES. SURVEY DATA OF EXISTING CONDITIONS WAS SUPPLIED BY OTHERS.
- CONTRACTOR SHALL LOCATE ALL EXISTING UNDERGROUND UTILITIES AND NOTIFY LANDSCAPE ARCHITECT OF ANY CONFLICTS. CONTRACTOR SHALL EXERCISE CAUTION WHEN WORKING IN THE VICINITY OF UNDERGROUND UTILITIES.
- CONTRACTOR SHALL PROVIDE A MINIMUM 2% SLOPE AWAY FROM ALL STRUCTURES.
- CONTRACTOR SHALL FINE GRADE AREAS TO ACHIEVE FINAL CONTOURS AS INDICATED. LEAVE AREAS TO RECEIVE TOPSOIL 3" BELOW FINAL FINISHED GRADE IN PLANTING AREAS AND 1" BELOW FINAL FINISHED GRADE IN LAWN AREAS.
- ALL PLANTING BEDS AND LAWN AREAS SHALL BE SEPARATED BY STEEL EDGING. NO STEEL EDGING SHALL BE INSTALLED ADJACENT TO BUILDINGS, WALKS, OR CURBS. CUT STEEL EDGING AT 45 DEGREE ANGLE WHERE IT INTERSECTS WALKS AND CURBS.
- TOP OF MULCH SHALL BE 1/2" MINIMUM BELOW THE TOP OF WALKS AND CURBS.
- ALL LAWN AREAS SHALL BE SOLID SOD KENTUCKY BLUEGRASS FESCUE MIX, UNLESS OTHERWISE NOTED ON THE DRAWINGS.
- ALL REQUIRED LANDSCAPE AREAS SHALL BE PROVIDED WITH AN AUTOMATIC UNDERGROUND IRRIGATION SYSTEM WITH RAIN AND FREEZE SENSORS AND EVAPOTRANSPIRATION (ET) WEATHER-BASED CONTROLLERS AND SAID IRRIGATION SYSTEM SHALL BE DESIGNED BY A QUALIFIED PROFESSIONAL AND INSTALLED BY A LICENSED IRRIGATOR.
- CONTRACTOR SHALL PROVIDE BID PROPOSAL LISTING UNIT PRICES FOR ALL MATERIAL PROVIDED.
- CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED LANDSCAPE AND IRRIGATION PERMITS.

MAINTENANCE NOTES

- THE OWNER, TENANT AND THEIR AGENT, IF ANY, SHALL BE JOINTLY AND SEVERALLY RESPONSIBLE FOR THE MAINTENANCE OF ALL LANDSCAPE.
- ALL LANDSCAPE SHALL BE MAINTAINED IN A NEAT AND ORDERLY MANNER AT ALL TIMES. THIS SHALL INCLUDE MOWING, EDGING, PRUNING, FERTILIZING, WATERING, WEEDING AND OTHER SUCH ACTIVITIES COMMON TO LANDSCAPE MAINTENANCE.
- ALL LANDSCAPE AREAS SHALL BE KEPT FREE OF TRASH, LITTER, WEEDS AND OTHER SUCH MATERIAL OR PLANTS NOT PART OF THIS PLAN.
- ALL PLANT MATERIAL SHALL BE MAINTAINED IN A HEALTHY AND GROWING CONDITION AS IS APPROPRIATE FOR THE SEASON OF THE YEAR.
- ALL PLANT MATERIAL WHICH DIES SHALL BE REPLACED WITH PLANT MATERIAL OF EQUAL OR BETTER VALUE.
- CONTRACTOR SHALL PROVIDE SEPARATE BID PROPOSAL FOR ONE YEAR'S MAINTENANCE TO BEGIN AFTER FINAL ACCEPTANCE.

LANDSCAPE TABULATIONS

THE CITY OF DES PLAINES, ILLINOIS

PARKING LOT LANDSCAPING

- Not less than 5% of the interior of a parking lot shall be devoted to landscaping.
- One (1) shade tree shall be provided for every 100 s.f. of landscaping area.
- A minimum of 50% of every interior parking lot landscaping area shall be planted with groundcover.

Parking Lot Area: 7,421 s.f.
Required: 371 s.f. (5%)
Provided: 790 s.f. (10%)
(4) trees, 3" cal. (4) trees, 3" cal.
50% groundcover 100% groundcover

PERIMETER PARKING LOT LANDSCAPING

- One (1) tree for every 40 l.f. of frontage.
- One (1) shrub for every 3 l.f. of frontage.

Manheim Road: 126 l.f.
Required: (3) trees
Provided: (3) trees
(42) shrubs (80) shrubs

Pratt Avenue: 308 l.f.
Required: (8) trees
Provided: (9) existing trees
(102) shrubs (140) shrubs

FOUNDATION LANDSCAPING

- 35% of the site to be landscape area.

Foundation Area: 100 l.f.
Required: 35 l.f. (35%)
Provided: 60 l.f. (60%)

GENERAL LAWN NOTES

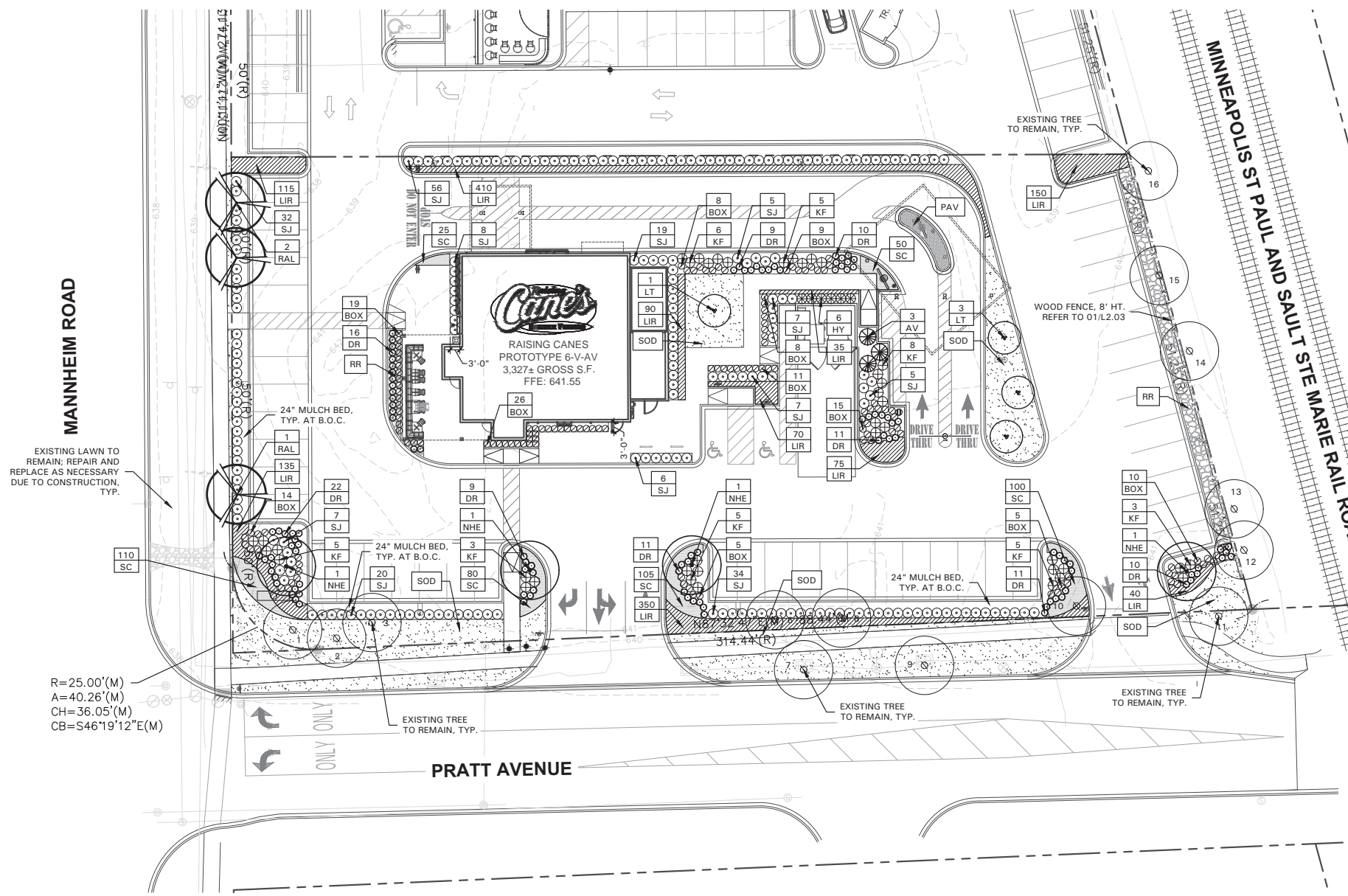
- CONTRACTOR SHALL COORDINATE OPERATIONS AND AVAILABILITY OF EXISTING TOPSOIL WITH ON-SITE CONSTRUCTION MANAGER.
- CONTRACTOR SHALL LEAVE LAWN AREAS 1" BELOW FINAL FINISHED GRADE PRIOR TO TOPSOIL INSTALLATION.
- CONTRACTOR SHALL FINE GRADE AREAS TO ACHIEVE FINAL CONTOURS AS INDICATED ON CIVIL PLANS. ADJUST CONTOURS TO ACHIEVE POSITIVE DRAINAGE AWAY FROM BUILDINGS. PROVIDE UNIFORM ROUNDING AT TOP AND BOTTOM OF SLOPES AND OTHER BREAKS IN GRADE. CORRECT IRREGULARITIES AND AREAS WHERE WATER MAY STAND.
- ALL LAWN AREAS SHALL BE FINE GRADED, IRRIGATION TRENCHES COMPLETELY SETTLED AND FINISH GRADE APPROVED BY THE OWNER'S CONSTRUCTION MANAGER OR LANDSCAPE ARCHITECT PRIOR TO LAWN INSTALLATION.
- CONTRACTOR SHALL REMOVE ALL ROCKS 3/4" DIAMETER AND LARGER, DIRT CLODS, STICKS, CONCRETE SPOILS, ETC. PRIOR TO PLACING TOPSOIL AND LAWN INSTALLATION.
- CONTRACTOR SHALL MAINTAIN ALL LAWN AREAS UNTIL FINAL ACCEPTANCE. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO: MOWING, WATERING, WEEDING, CULTIVATING, CLEANING AND REPLACING DEAD OR BARE AREAS TO KEEP PLANTS IN A VIGOROUS, HEALTHY CONDITION.
- CONTRACTOR SHALL GUARANTEE ESTABLISHMENT OF ACCEPTABLE TURF AREA AND SHALL PROVIDE REPLACEMENT FROM LOCAL SUPPLY IF NECESSARY.

SOLID SOD NOTES

- PLANT SOD BY HAND TO COVER INDICATED AREAS COMPLETELY. ENSURE EDGES OF SOD ARE TOUCHING. TOP DRESS JOINTS BY HAND WITH TOPSOIL TO FILL VOIDS.
- ROLL GRASS AREAS TO ACHIEVE A SMOOTH, EVEN SURFACE, FREE FROM UNNATURAL UNDULATIONS.
- WATER SOD THOROUGHLY AS SOD OPERATION PROGRESSES.

CONCRETE PAVER NOTES

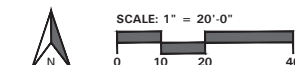
- CONTRACTOR SHALL ADHERE TO THE MANUFACTURER'S INSTALLATION GUIDELINES, SPECIFICATIONS, AND ANY OTHER REQUIREMENTS OUTLINED BY THE MANUFACTURER FOR ALL PAVER INSTALLATION.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN A COPY OF THE MANUFACTURER'S SPECIFICATIONS PRIOR TO COMMENCING ANY WORK.
- CONCRETE PAVERS SHALL BE:
 - BELGARD® LEGACY SERIES
 - MODEL: HOLLAND STONE
 - PATTERN: HERRINGBONE
 - COLOR: CHARCOAL
- CONTRACTOR SHALL SUBMIT A STANDARD COLOR SAMPLE BOARD TO THE OWNER'S AUTHORIZED REPRESENTATIVE FOR FINAL COLOR SELECTION PRIOR TO PLACING ORDER.
- THE FINAL COLOR SELECTION SHALL BE MADE BY THE OWNER'S AUTHORIZED REPRESENTATIVE ON-SITE.
- CONCRETE PAVERS AVAILABLE FROM:
 - WWW.BELGARD.COM
- THE CONTRACTOR SHALL CONSTRUCT A SAMPLE PANEL 4'-0" BY 4'-0" ON-SITE AT NO EXPENSE TO THE OWNER, FOR APPROVAL BY THE OWNER'S AUTHORIZED REPRESENTATIVE PRIOR TO COMMENCING WORK.
- THE OWNER'S AUTHORIZED REPRESENTATIVE RESERVES THE RIGHT TO REJECT ANY AND ALL WORK EXECUTED BY THE CONTRACTOR WHICH DOES NOT MEET HIS/HER EXPECTATIONS AND THE MANUFACTURER'S SPECIFICATIONS.
- THE CONTRACTOR SHALL MAKE ANY MODIFICATIONS REQUIRED BY THE OWNER'S AUTHORIZED REPRESENTATIVE AT NO EXPENSE TO THE OWNER.



PLANT LIST

| SYMBOL | BOTANICAL NAME | COMMON NAME | QTY. | SIZE | REMARKS |
|---------------------------|--|----------------------------------|------|---------|---|
| TREES | | | | | |
| LT | <i>Syringa reticulata</i> 'Ivory Silk' | Ivory Silk Japanese Lilac Tree | 4 | 2" cal. | container grown, 8' ht., 4' spread min. |
| NHE | <i>Ulmus davidiana</i> var. <i>japonica</i> | New Horizon Elm | 4 | 3" cal. | container grown, 12' ht., 4' spread, 4' branching ht., matching |
| RAL | <i>Tilia americana</i> 'Redmond' | Redmond American Linden | 3 | 3" cal. | B&B, 12' ht., 4' spread, 4' branching ht., matching |
| SHRUBS/GROUNDCOVER | | | | | |
| AV | <i>Thuja occidentalis</i> 'Emerald Green' | Emerald Green Arborvitae | 3 | 8' ht. | container grown, full to base, 36" spread |
| BOX | <i>Buxus</i> x 'Winter Gem' | Boxwood 'Winter Gem' | 130 | 7 gal. | container full, 20" spread, 24" o.c. |
| DR | <i>Rosa</i> 'Red Drift' | Red Drift Rose | 109 | 5 gal. | container full, 24" spread, 20" ht., 24" o.c. |
| HY | <i>Taxus x media</i> 'Hicksii' | Hicks' Yew | 8 | 7 gal. | container full |
| KF | <i>Calamagrostis</i> x <i>scutiflora</i> 'Karl Foerster' | Karl Foerster Feather Reed Grass | 40 | 5 gal. | container full, 24" ht., 36" o.c. |
| LIR | <i>Liriope muscari</i> 'Big Blue' | Liriope 'Big Blue' | 1470 | 4" pots | container full top of container, 12" o.c. |
| SC | | Seasonal Color | 470 | 4" pots | container full, 12" o.c., selection by Owner |
| SJ | <i>Juniperus chinensis</i> 'Sea Green' | Sea Green Juniper | 208 | 5 gal. | container full, 24" spread, 24" o.c. |
| MISCELLANEOUS | | | | | |
| PAV | | Concrete Pavers | | | refer to 03/L1.02 |
| RR | | Native River Rock | | | 2" - 4" dia., 4" depth rock mulch, typ. at planting adjacent to patio |
| SOD | <i>Poa pratensis</i> x <i>Festuca arundinacea</i> | Kentucky Bluegrass Fescue Mix | | | solid sod, refer to Solid Sod Notes |

NOTE: ALL TREES SHALL HAVE STRAIGHT TRUNKS AND BE MATCHING WITHIN VARIETIES.
PLANT LIST IS AN AID TO BIDDERS ONLY. CONTRACTOR SHALL VERIFY ALL QUANTITIES ON PLAN.
ALL HEIGHTS AND SPREADS ARE MINIMUMS. ALL PLANT MATERIAL SHALL MEET OR EXCEED REMARKS AS INDICATED.



BELLE FIRMA
4245 North Central Expy
Suite 501
Dallas, Texas 75205
214.865.7192 office

SHEET REVISIONS

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LANDSCAPE PLAN
ISSUE DATE: 04/14/2023
JOB NO. 22297
L2.01
SHEET NO.

SECTION 32 9300 - LANDSCAPE

PART 1 - GENERAL

1.1 REFERENCED DOCUMENTS

- A. Refer to Landscape Plans, notes, details, bidding requirements, special provisions, and schedules for additional requirements.

1.2 DESCRIPTION OF WORK

- A. Work included: Furnish all supervision, labor, materials, services, equipment and appliances required to complete the work covered in conjunction with the landscaping covered in these specifications and landscaping plans, including:
 1. Planting (trees, shrubs and grasses)
 2. Bed preparation and fertilization
 3. Notification of sources
 4. Water and maintenance until final acceptance
 5. Guarantee

1.3 REFERENCE STANDARDS

- A. American Standard for Nursery Stock published by American Association of Nurserymen: April 14, 2014 Edition; by American National Standards Institute, Inc. (Z60.1) - plant material
- B. American Joint Committee on Horticultural Nomenclature: 1942 Edition of Standardized Plant Names.
- C. Illinois Association of Nurserymen, Grades and Standards
- D. Hortis Third, 1976 - Cornell University

1.4 NOTIFICATION OF SOURCES AND SUBMITTALS

- A. Samples: Provide representative quantities of sandy loam soil, mulch, bed mix material, gravel, crushed stone, steel edging and tree stakes. Samples shall be approved by Owner's Authorized Representative before use on the project.

1.5 JOB CONDITIONS

- A. General Contractor to complete the following punch list: Prior to Landscape Contractor initiating any portion of landscape installation, General Contractor shall leave planting bed areas three (3") inches below final finish grade of sidewalks, drives and curbs as shown on the drawings. All lawn areas to receive solid sod shall be left one (1") inch below the final finish grade of sidewalks, drives and curbs. All construction debris shall be removed prior to Landscape Contractor beginning any work.
- B. Storage of materials and equipment at the job site will be at the risk of the Landscape Contractor. The Owner cannot be held responsible for theft or damage.

1.6 MAINTENANCE AND GUARANTEE

- A. Maintenance:
 1. The Landscape Contractor shall be held responsible for the maintenance of all work from the time of planting until final acceptance by the Owner. No trees, shrubs, groundcover or grass will be accepted unless they show healthy growth and satisfactory foliage conditions.
 2. Maintenance shall include watering of trees and plants, cultivation, weeding spraying, edging, pruning of trees, mowing of grass, cleaning up and all other work necessary of maintenance.
 3. A written notice requesting final inspection and acceptance should be submitted to the Owner at least seven (7) days prior to completion. An on-site inspection by the Owner's Authorized Representative will be completed prior to written acceptance.
- B. Guarantee:

- 1. Trees, shrubs and groundcover shall be guaranteed for a twelve (12) month period after final acceptance. The Contractor shall replace all dead materials as soon as weather permits and upon notification of the Owner. Plants, including trees, which have partially died so that shape, size, or symmetry have been damaged, shall be considered subject to replacement. In such cases, the opinion of the Owner shall be final.

- a. Plants used for replacement shall be of the same size and kind as those originally planted and shall be planted as originally specified. All work, including materials, labor and equipment used in replacements, shall carry a twelve (12) month guarantee. Any damage, including ruts in lawn or bed areas, incurred as a result of making replacements shall be immediately repaired.

- b. At the direction of the Owner, plants may be replaced at the start of the next year's planting season. In such cases, dead plants shall be removed from the premises immediately.

- c. When plant replacements are made, plants, soil mix, fertilizer and mulch are to be utilized as originally specified and re-inspected for full compliance with the contract requirements. All replacements are to be included under "Work" of this section.

- 2. The Owner agrees that for the guarantee to be effective, he will water plants at least twice a week during dry periods and cultivate beds once a month after final acceptance.

- 3. The above guarantee shall not apply where plants die after acceptance because of injury from storms, hail, freeze, insects, diseases, injury by humans, machines or theft.

- 4. Acceptance for all landscape work shall be given after final inspection by the Owner provided the job is in a complete, undamaged condition and there is a stand of grass in all lawn areas. At that time, the Owner will assume maintenance on the accepted work.

- C. Repairs: Any necessary repairs under the Guarantee must be made within ten (10) days after receiving notice, weather permitting. In the event the Landscape Contractor does not make repairs accordingly, the Owner, without further notice to Contractor, may provide materials and men to make such repairs at the expense to the Landscape Contractor.

1.7 QUALITY ASSURANCE

- A. General: Comply with applicable federal, state, county and local regulations governing landscape materials and work.

- B. Personnel: Employ only experienced personnel who are familiar with the required work. Provide full time supervision by a qualified foreman acceptable to Landscape Architect.

- C. Selection of Plant Material:

- 1. Make contact with suppliers immediately upon obtaining notice of contract acceptance to select and book materials. Develop a program of maintenance (pruning and fertilization) which will ensure the purchased materials will meet and / or exceed project specifications.

- 2. Substitutions: Do not make plant material substitutions. If the specified landscape material is not obtainable, submit proof of non-availability to Landscape Architect, together with proposal for use of equivalent material. At the time bids are submitted, the Contractor is assumed to have located the materials necessary to complete the job as specified.

- 3. Landscape Architect will provide a key identifying each tree location on site. Written verification will be required to document material selection, source and delivery schedules to site.

- 4. Measurements: Measure trees with branches and trunks or canes in their normal position. Do not prune to obtain required sizes. Take caliper measurements six inches above ground for trees up to and including 4" caliper size, and twelve inches above ground for larger sizes. Measure main body of all plant material of height and spread dimensions,

do not measure from branch or root tip-to-tip.

- 5. Owner's Authorized Representative shall inspect all plant material with requirements for genus, species, cultivar / variety size and quality.

- 6. Owner's Authorized Representative retains the right to further inspect all plant material upon arrival to the site and during installation for size and condition of root balls and root systems, limbs, branching habit, insects, injuries and latent defects.

- 7. Owner's Authorized Representative may reject unsatisfactory or defective material at any time during the process work. Remove rejected materials immediately from the site and replace with acceptable material at no additional cost to the Owner. Plants damaged in transit or at job site shall be rejected.

1.8 PRODUCT DELIVERY, STORAGE AND HANDLING

A. Preparation:

- 1. Balled and Burlapped (B&B) Plants: Dig and prepare shipment in a manner that will not damage roots, branches, shape and future development.

- 2. Container Grown Plants: Deliver plants in rigid container to hold ball shape and protect root mass.

B. Delivery:

- 1. Deliver packaged materials in sealed containers showing weight, analysis and name of manufacturer. Protect materials from deterioration during delivery and while stored on site.

- 2. Deliver only plant materials that can be planted in one day unless adequate storage and watering facilities are available on job site.

- 3. Protect root balls by heeling in with sawdust or other approved moisture retaining material if not planted within 24 hours of delivery.

- 4. Protect plants during delivery to prevent damage to root balls or desiccation of leaves. Keep plants moist at all times. Cover all materials during transport.

- 5. Notify Owner's Authorized Representative of delivery schedule 72 hours in advance job site.

- 6. Remove rejected plant material immediately from job site.

- 7. To avoid damage or stress, do not lift, move, adjust to plumb, or otherwise manipulate plants by trunk or stems.

PART 2 - PRODUCTS

2.1 PLANTS

- A. General: Well-formed No. 1 grade or better nursery grown stock. Listed plant heights are from tops of root balls to nominal tops of plants. Plant spread refers to nominal outer width of the plant, not to the outer leaf tips. Plants will be individually approved by the Owner's Authorized Representative and his decision as to their acceptability shall be final.

- B. Quantities: The drawings and specifications are complementary. Anything called for on one and not the other is as binding as if shown and called for on both. The plant schedule is an aid to bidders only. Confirm all quantities on plan.

- C. Quality and size: Plant materials shall conform to the size given on the plan, and shall be healthy, symmetrical, well-shaped, full branched and well rooted. The plants shall be free from injurious insects, diseases, injuries to the bark or roots, broken branches, objectionable disfigurements, insect eggs and larvae, and are to be of specimen quality.

- D. Approval: All plants which are found unsuitable in growth, or are in any unhealthy, badly shaped or undersized condition will be rejected by the Owner's Authorized Representative either before or after planting and shall be removed at the expense of the Landscape Contractor and replaced with acceptable plant as

specified at no additional cost to the Owner.

- E. Trees shall be healthy, full-branched, well-shaped, and shall meet the minimum trunk and diameter requirements of the plant schedule. Balls shall be firm, neat, slightly tapered and well wrapped in burlap. Any tree loose in the ball or with a broken root ball at time of planting will be rejected. Balls shall be ten (10") inches in diameter for each one (1") inch of trunk diameter, measured six (6") inches above ball. (Nomenclature confirms to the customary nursery usage. For clarification, the term "multi-trunk" defines a plant having three (3) or more trunks of nearly equal diameter.)

- F. Pruning: All pruning of trees and shrubs, as directed by the Landscape Architect prior to final acceptance, shall be executed by the Landscape Contractor at no additional cost to the Owner.

2.2 SOIL PREPARATION MATERIALS

A. Sandy Loam:

- 1. Friable, fertile, dark, loamy soil, free of clay lumps, subsoil, stones and other extraneous material and reasonably free of weeds and foreign grasses. Loam containing Dallasgrass or Nutgrass shall be rejected.

- 2. Physical properties as follows:
 - a. Clay - between 7-27 percent
 - b. Silt - between 15-25 percent
 - c. Sand - less than 52 percent

- 3. Organic matter shall be 3%-10% of total dry weight.

- 4. If requested, Landscape Contractor shall provide a certified soil analysis conducted by an approved soil testing laboratory verifying that sandy loam meets the above requirements.

- B. Organic Material: Compost with a mixture of 80% vegetative matter and 20% animal waste. Ingredients should be a mix of course and fine textured material.

- C. Sharp Sand: Sharp sand must be free of seeds, soil particles and weeds.

- D. Mulch: Double Shredded Hardwood Mulch, partially decomposed, dark brown.

- E. Organic Fertilizer: FertiLid, Sustane, or Green Sense or equal as recommended for required applications. Fertilizer shall be delivered to the site in original unopened containers, each bearing the manufacturer's guaranteed statement of analysis.

- F. Commercial Fertilizer: 10-20-10 or similar analysis. Nitrogen source to be a minimum 50% slow release organic Nitrogen (SCU or UF) with a minimum 8% sulfur and 4% iron, plus micronutrients.

- G. Peat: Commercial sphagnum peat moss or partially decomposed shredded pine bark or other approved organic material.

2.3 MISCELLANEOUS MATERIALS

- A. Steel Edging: All steel edging shall be 3/16" thick x 4" deep x 16' long with 6 stakes per section, painted black at the factory as manufactured by The J.D. Russell Company and under its trade name DURAEDE Heavy Duty Steel.

- B. Staking Material for Shade Trees: refer to details.

- C. Gravel: Washed native pea gravel, graded 1 inch to 1-1/2 inch.

- D. Filter Fabric: "Mirafi Mirascape" by Mirafi Construction Products or approved equal.

- E. River Rock: Native river rock, 2" - 4" dia.

- F. Decomposed Granite: Base material shall consist of a natural material mix of granite aggregate not to exceed 1 1/2" diameter in size and shall be composed of various stages of decomposed earth base.

PART 3 - EXECUTION

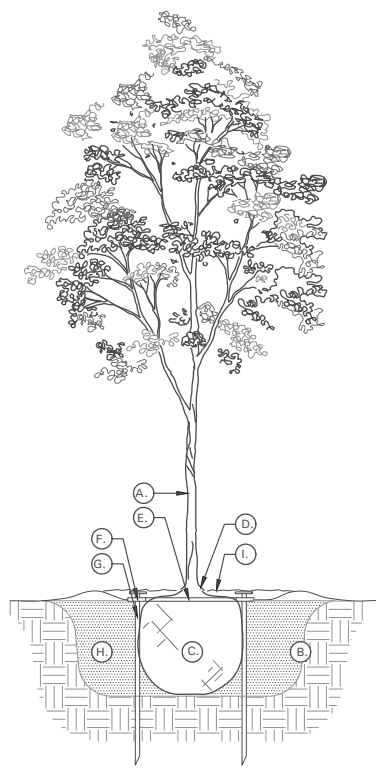
- H. BACKFILL: USE EXISTING NATIVE SOIL (no amendments) WATER THOROUGHLY TO ELIMINATE AIR POCKETS.

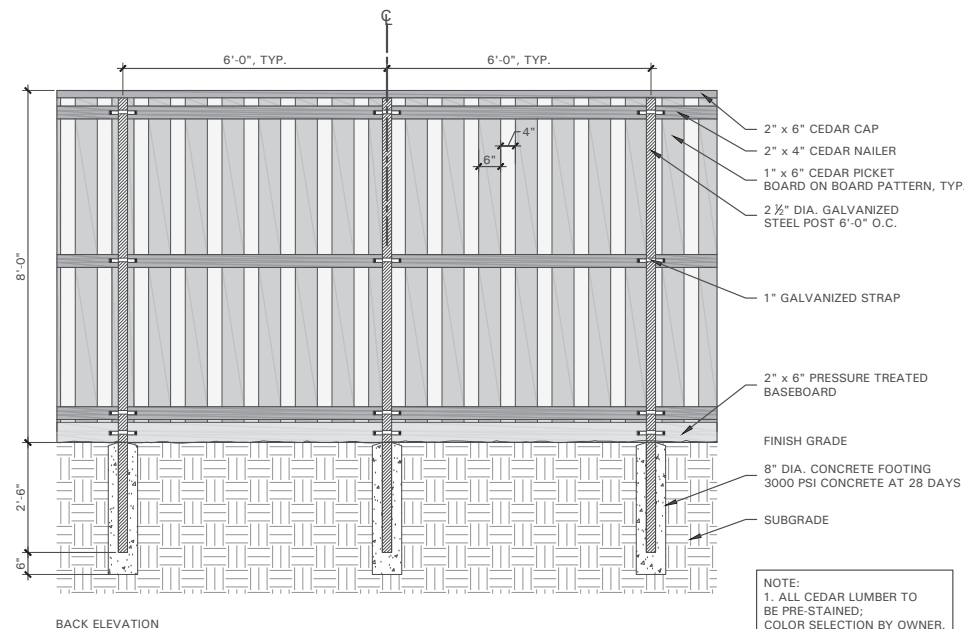
- I. MULCH: DOUBLE SHREDDED HARDWOOD MULCH 2 INCH SETTLED THICKNESS, WITH 2" HT. WATERING RING; ENSURE THAT ROOT FLARE IS EXPOSED. BELOW GROUND STAKE SHOULD NOT BE VISIBLE.

- J. TREE STAKES: TREE STAKE SOLUTIONS 'SAFETY STAKE' BELOW GROUND MODEL AVAILABLE FROM: Tree Stake Solutions ATTN: Jeff Talley (903) 676-6143 jeff@treestakesolutions.com www.treestakesolutions.com

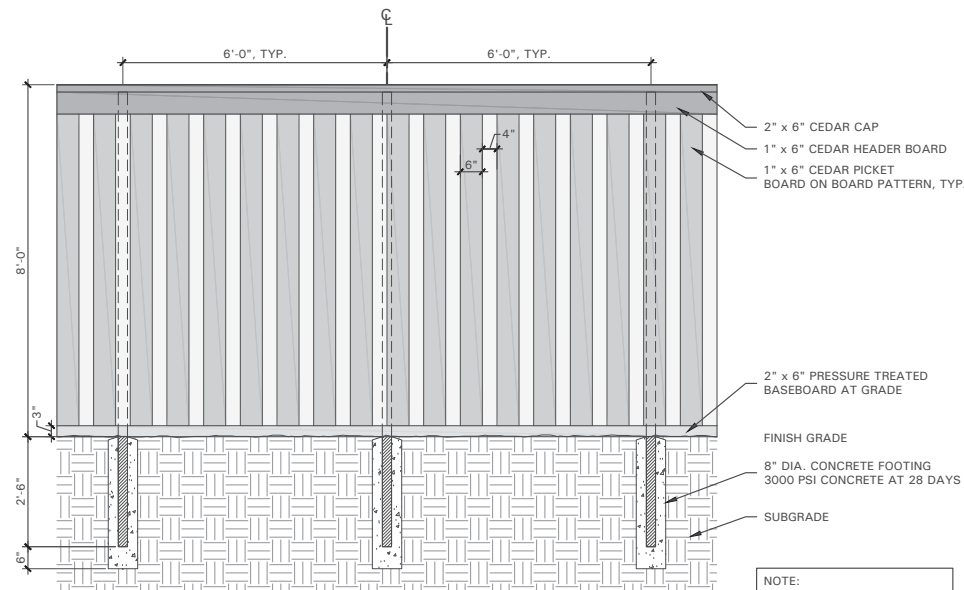
- OR APPROVED EQUAL. TREES SHALL BE STAKED BELOW GROUND WHERE NECESSARY; ABOVE GROUND STAKING IS EXPRESSLY PROHIBITED.

- K. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN A COPY OF THE MANUFACTURER'S SPECIFICATIONS PRIOR TO INSTALLATION OF TREE STAKES. CONTRACTOR SHALL ADHERE TO MANUFACTURER'S INSTALLATION GUIDELINES, SPECIFICATIONS, AND OTHER REQUIREMENTS FOR TREE STAKE INSTALLATION.

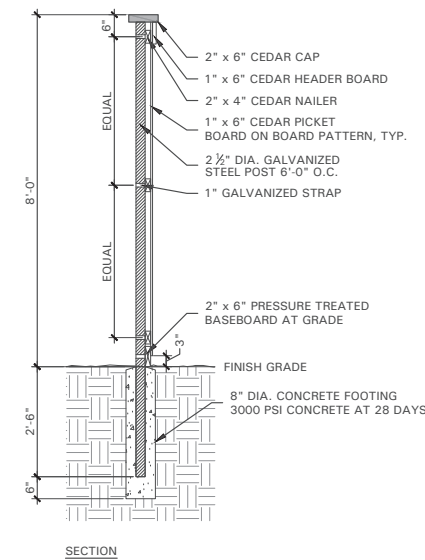




NOTE:
 1. ALL CEDAR LUMBER TO BE PRE-STAINED; COLOR SELECTION BY OWNER.
 2. ALL HARDWARE SHALL BE GALVANIZED.



NOTE:
 1. ALL CEDAR LUMBER TO BE PRE-STAINED; COLOR SELECTION BY OWNER.
 2. ALL HARDWARE SHALL BE GALVANIZED.



01 BOARD ON BOARD WOOD FENCING DETAIL
 SCALE: 1/2" = 1'-0"

WOOD FENCE NOTES

- CONTRACTOR SHALL VERIFY THE LOCATION OF ALL PROPOSED AND EXISTING SITE ELEMENTS AND NOTIFY THE LANDSCAPE ARCHITECT OF ANY DISCREPANCIES.
- EXISTING UTILITIES ARE TAKEN FROM AVAILABLE RECORDS. LOCATIONS SHOWN, IF SHOWN AT ALL, ARE FROM DRAWINGS PROVIDED BY OTHERS AND MAY NOT ACCURATELY REFLECT THE SIZE AND LOCATION OF EACH UTILITY. CONTRACTOR SHALL ASSUME RESPONSIBILITY FOR ACTUAL FIELD LOCATION AND PROTECTION OF EXISTING FACILITIES WHETHER SHOWN OR NOT. CONTRACTOR SHALL ASSUME RESPONSIBILITY FOR REPAIRS TO EXISTING FACILITIES, WEATHER SHOWN OR NOT, DAMAGES BY CONTRACTOR'S ACTIVITIES.
- EXISTING TREES AND OTHER PLANT MATERIALS SHALL BE PROTECTED DURING FABRICATION OF FENCE. NO TREE OR PLANT STRUCTURE DAMAGE OR SOIL COMPACTION UNDER TREES AND PLANTS IS ALLOWED. NO TOXIC SOLUTIONS OR OTHER LIQUID CHEMICALS SHALL BE POURED, MIXED, FOR DISPOSED OF NEAR TREES OR PLANTS INCLUDING, BUT NOT LIMITED TO, PRIMERS, PAINTS, THINNERS, FUELS, ETC.
- CONTRACTOR SHALL LAYOUT AND STAKE LOCATION OF FENCE FOR OWNER'S AUTHORIZED REPRESENTATIVE APPROVAL PRIOR TO CONSTRUCTION.
- CONTRACTOR SHALL USE CEDAR LUMBER FOR ALL PICKETS, NAILERS, AND CAP MEMBERS.
- KEEP FENCE MATERIAL DRY DURING DELIVERY AND STORAGE. PROTECT AGAINST EXPOSURE TO WEATHER AND CONTACT WITH DAMP OR WET SURFACES.
- DISCARD UNITS OF MATERIAL WITH DEFECTS WHICH MIGHT IMPAIR THE QUALITY OF WORK AND UNITS WHICH ARE TOO SMALL TO FABRICATE THE WORK WITH MINIMUM JOINTS FOR THE OPTIMUM JOINT ARRANGEMENT. ALL WOOD MEMBERS SHALL BE ONE (1) SINGLE PIECE BETWEEN POSTS, INCLUDING PICKETS, CAPS, AND NAILERS.
- SET CARPENTRY WORK ACCURATELY TO REQUIRED LEVELS AND LINES, WITH MEMBERS PLUMB, LEVEL, STRAIGHT, FLAT, AND ACCURATELY CUT, FITTED, AND DRILLED.
- SECURELY ATTACH CARPENTRY WORK BY BASES BY ANCHORING AND FASTENING. USE STAINLESS STEEL OR GALVANIZED COMMON WOOD SCREWS, UNLESS NOTED OTHERWISE ON PLANS. SELECT FASTENERS OR SIZE THAT WILL NOT PENETRATE MEMBERS WHERE OPPOSITE SIDE WILL BE EXPOSED TO VIEW OR WILL RECEIVE FINISH MATERIAL. MAKE TIGHT CONNECTIONS BETWEEN MEMBERS. INSTALL FASTENERS WITHOUT SPLITTING WOOD, PRE-DRILLING AS REQUIRED. MATERIAL WHICH HAS BEEN MARRIED BY HAMMERING OR DISFIGURED BY ANY OTHER MEANS WILL BE REJECTED.
- MITER ALL CORNERS WHERE APPLICABLE. ALL FLUSH MITERS AND INTERSECTIONS TO BE WELDED AND GROUND SMOOTH.
- CONTRACTOR SHALL PROVIDE STAINLESS STEEL OR GALVANIZED FASTENERS, UNLESS NOTED OTHERWISE ON PLANS. SELECT FASTENERS FOR THE TYPE, GRADE, AND CLASS REQUIRED FOR THE INSTALLATION OF ALL WOOD ITEMS. FASTENERS INCLUDE, BUT NOT LIMITED TO, ALL HANGERS, SLEEVES, RODS, BARS, SCREWS, BOLTS, NUTS, ANCHORS, AND BRACKETS.
- CONTRACTOR SHALL BE REQUIRED TO CONSTRUCT A SAMPLE PANEL ON SITE, 8'-0" HT. BY 8'-0" LONG, FOR APPROVAL BY THE OWNER'S AUTHORIZED REPRESENTATIVE PRIOR TO COMMENCING WORK.
- UPON COMPLETION AND PRIOR TO FINAL ACCEPTANCE, REMOVE DEBRIS AND TRASH. CLEAN UP SPILLS IMMEDIATELY USING APPROVED METHODS.



Restaurant Support Office
 6800 Bishop Road, Plano, TX 75024
 Tel: 972-769-3100 Fax: 972-769-3101

STORE:
RAISING CANE'S RESTAURANT
 2777 MANHEIM RD
 DES PLAINES, IL 60018
 PROTOTYPE: P6-V-AV
 SCHEME: B
 RESTAURANT #RC1009



17710 Detroit Avenue
 Lakewood, Ohio 44107
 Phone (216) 521-5134 Fax (216) 521-4824
 www.adaarchitects.com

THIS DOCUMENT CONTAINS INFORMATION PROPRIETARY TO ADA ARCHITECTS, INC. UNAUTHORIZED USE OF THESE DOCUMENTS IS EXPRESSLY PROHIBITED UNLESS AGREED UPON IN WRITING.

ENGINEER INFORMATION:



| SHEET REVISIONS | | |
|-----------------|------|------|
| # | DATE | TYPE |
| 1 | | |
| 2 | | |
| 3 | | |
| 4 | | |
| 5 | | |
| 6 | | |
| 7 | | |
| 8 | | |
| 9 | | |
| 10 | | |

LANDSCAPE SPECIFICATIONS AND DETAILS
 ISSUE DATE: 04/14/2023
 JOB NO. 22297
L2.03
 SHEET NO.

BELLE FIRMA
 4245 North Central Expy
 Suite 501
 Dallas, Texas 75205
 214.865.7192 office

Date 4/19/2023

Attn: Bryan Rosenblum
GW Properties
2211 N Elston Ave
Suite 400
Chicago, IL 60614

Address: 2777 Mannheim Rd, Des Plaines 60018

Dear Bryan,

This letter will serve as Nicor Gas' intention to provide service to the above, potential project. Nicor Gas will install up to 200' of gas main per new customer (each meter) and 60' of service line per new customer (each meter) at no cost. If your project exceeds these footages, Nicor will determine the cost based on an economic evaluation of the project.

Please complete the attached New Service Agreement and submit the following to initiate the installation process; Plat of Subdivision, Over-all Site Utility Plan with gas meter locations marked (commercial buildings only), Gas loads and delivery pressure for each unit along with Contact information.

Nicor Gas requires curbs/pavement to be completed prior to gas main installation. Nicor Gas also requires sleeves to be installed at paved intersections within new subdivisions and on Commercial service installations. When applicable and to establish a defined/proper running line, easements will need to be appropriately staked. All permits (State, County, and Village) will require approval prior to gas main and/or service installation.

As the site contact for this project, please advise the general contractor to communicate with me, Zack Jarling at start up in order to coordinate the gas service/main installation timetable. I can be reached at 224-239-3341 or by email at zjarling@southernco.com

If you have any additional questions, please let me know

Sincerely,

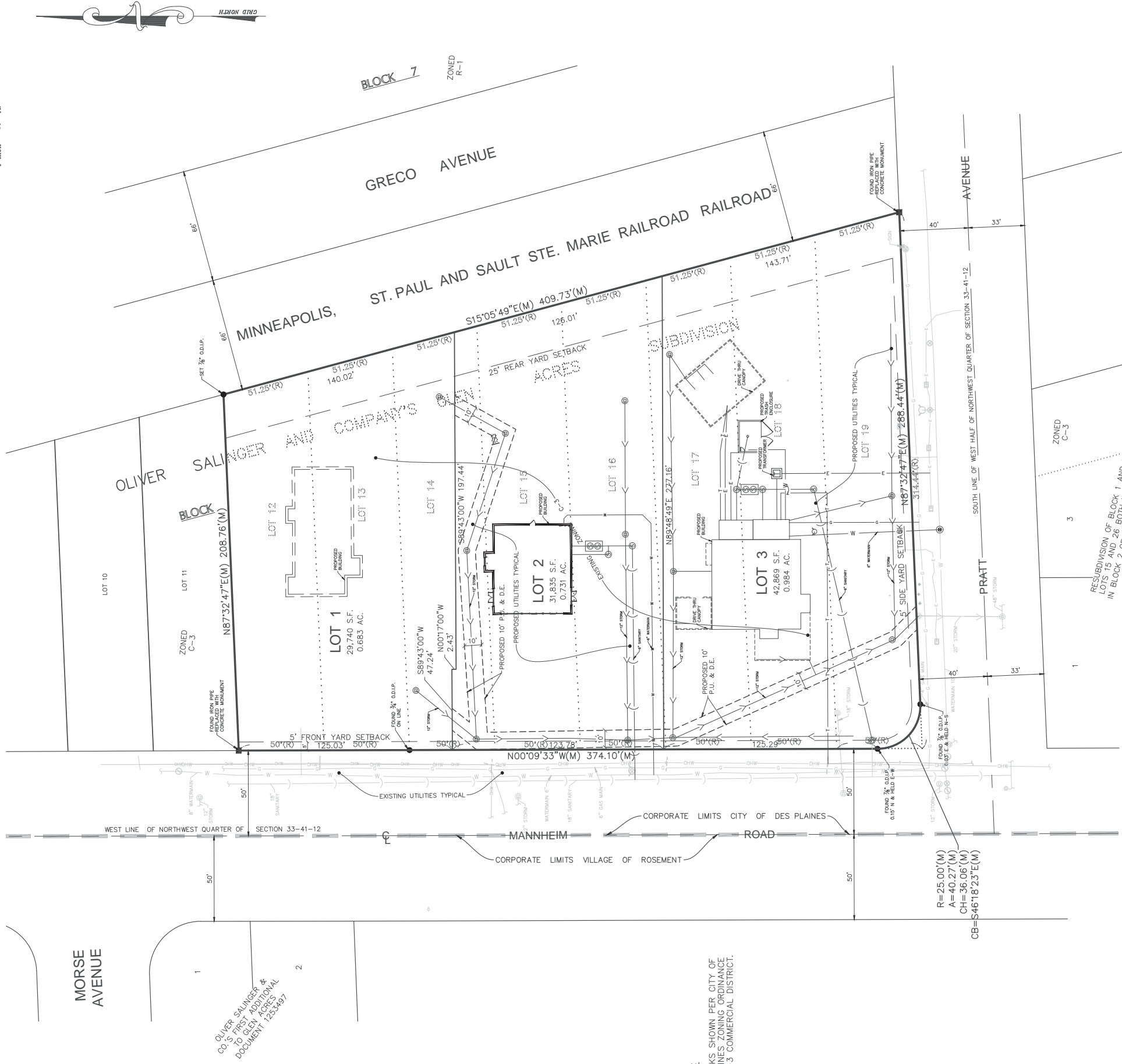
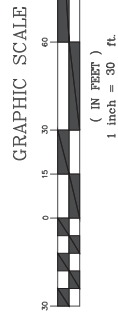
Zack Jarling

New Business Construction Consultant
Customer Development, Nicor Gas

TENTATIVE PLAT OF SUBDIVISION 2777 MANNHEIM ROAD SUBDIVISION

P.I.N. 09-33-108-012-0000
P.I.N. 09-33-108-013-0000
P.I.N. 09-33-108-014-0000
P.I.N. 09-33-108-022-0000
P.I.N. 09-33-108-023-0000

PART OF THE WEST 1/2 OF THE NORTHWEST
1/4 OF SECTION 33, TOWNSHIP 41 NORTH,
RANGE 12, EAST OF THE THIRD PRINCIPAL
MERIDIAN, IN COOK COUNTY, ILLINOIS.



LEGEND
 • SET 7/8" O.D.I.P.
 ■ UNLESS OTHERWISE NOTED
 + SET CROSS IN CONCRETE UNLESS OTHERWISE NOTED

UTILITY LEGEND
 PROPOSED UTILITIES:
 (S) STORM STRUCTURE
 (E) SANITARY MANHOLE
 (E) ELECTRIC MANHOLE
 (U) UTILITY POLE
 (L) LIGHT POLE
 (H) HAND HOLE
 (V) VALVE VAULT
 (F) FIRE HYDRANT
 (B) VALVE BOX
 (T) TELEPHONE MANHOLE
 (S) STORM SEWER
 (W) WATER MAIN
 (G) GAS MAIN
 (O) OVERHEAD WIRES
 (C) COMMUNICATION LINE

EXISTING UTILITIES
 (S) STORM STRUCTURE
 (E) SANITARY MANHOLE
 (E) ELECTRIC MANHOLE
 (U) UTILITY POLE
 (L) LIGHT POLE
 (H) HAND HOLE
 (V) VALVE VAULT
 (F) FIRE HYDRANT
 (B) VALVE BOX
 (T) TELEPHONE MANHOLE
 (S) STORM SEWER
 (W) WATER MAIN
 (G) GAS MAIN
 (O) OVERHEAD WIRES
 (C) COMMUNICATION LINE

LINE LEGEND
 — SUBDIVISION BOUNDARY LINE
 — ADJACENT LAND PARCEL LINE
 — LOT LINE
 - - - CASEMENT LINE
 - - - PUBLIC UTILITY EASEMENT
 - - - BUILDING SETBACK LINE
 - - - SECTION LINE

ABBREVIATIONS
 O.D.I.P. = OUTSIDE DIAMETER IRON PIPE
 S = SOUTH
 E = EAST
 W = WEST

(R) = RECORD BEARING OR DISTANCE
 (M) = MEASURED BEARING OR DISTANCE
 (C) = CALCULATED BEARING OR DISTANCE
 (P) = PLANNED BEARING OR DISTANCE
 B.S.L. = BUILDING SETBACK LINE
 U.E. = UTILITY EASEMENT
 D.E. = DRAINAGE EASEMENT
 P.U.E. = PUBLIC UTILITY EASEMENT
 P.O.B. = POINT OF BEGINNING
 P.U. & D.E. = PUBLIC UTILITY AND DRAINAGE EASEMENT

LEGAL DESCRIPTION OF SUBDIVISION

LOTS 12, 13, 14, 15, 16, 17, 18, AND 19 IN BLOCK 8 IN OLIVER SALINGER AND COMPANY'S GLEN ACRES SUBDIVISION IN THE WEST 1/2 OF THE NORTHWEST 1/4 OF SECTION 33, TOWNSHIP 41 NORTH, RANGE 12, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE THEREOF RECORDED JUNE 3, 1939 AS DOCUMENT 12335452, IN COOK COUNTY, ILLINOIS.

| AREA SUMMARY | |
|-------------------|---|
| GROSS | 104,444 SQUARE FEET OR 2.398 ACRES |
| R.O.W. DEDICATION | 0 SQUARE FEET OR 0 ACRES |
| NET AREA | 104,444 SQUARE FEET OR 2.398 ACRES (TO HEAVY LINES) (BASED ON MEASURED VALUES) |

SURVEYOR'S STATEMENT

THE ABOVE PLAT WAS PREPARED UNDER MY DIRECT SUPERVISION FROM EXISTING RECORDS, MAPS AND PLATS.
 COMPASS SURVEYING LTD
 PROFESSIONAL DESIGN FIRM
 LAND SURVEYOR CORPORATION NO. 184-002778
 LICENSE EXPIRES 4/30/2025

BY: SCOTT KREBS DATE: 3/10/2023

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3509
 LICENSE EXPIRES 11/30/2024

COMPASS SURVEYING LTD
 ALTA SURVEYS • TOPOGRAPHY • CONSTRUCTION STAKING
 2631 GINGER WOODS PARKWAY, STE. 100
 AUSTIN, TEXAS 78745
 PHONE: (610) 820-9100 FAX: (610) 830-7600 EMAIL: ADMIN@COMPASSSURVEYING.COM

PROJECT
 2777 MANNHEIM ROAD SUBDIVISION
 DES PLAINES, IL

CLIENT/ENGINEER
Kimley-Horn
 © 2020 KIMLEY-HORN AND ASSOCIATES, INC.
 4201 WINFIELD ROAD, SUITE 800,
 PRINCETON, IL 61855
 PHONE: 618-467-5550
 WWW.KIMLEY-HORN.COM

| NO. | DATE | BY |
|-----|---------|-----|
| 1 | 4/10/23 | TMR |
| 2 | 4/20/23 | TMR |
| 3 | 4/27/23 | TMR |

| DATE | PC N/A | IPC N/A | DRAWN BY | MRA | CHECKED BY | SK | BOOK | N/A | PG | N/A | OWNER / DEVELOPER |
|-----------|--------|---------|----------|-----|------------|----|------|-----|----|-----|---|
| 3/10/2023 | | | | | | | | | | | GW PROPERTIES 2311 N. ELSTON AVENUE, SUITE #400 CHICAGO, IL |

J:\PROJECTS\2023\PROJECTS\23.0015\23.0015 TENTPOSUBD.DWG



**PUBLIC WORKS AND
ENGINEERING DEPARTMENT**

1420 Miner Street
Des Plaines, IL 60016
P: 847.391.5390
desplaines.org

MEMORANDUM

Date: April 20, 2023
To: John Carlisle, Director of Community and Economic Development
From: John La Berg, P.E., Civil Engineer
Cc: Jon Duddles, P.E., Assistant Director of Public Works and Engineering
Subject: 2777 Mannheim Rd. Proposed 3 lot subdivision for zoning board review

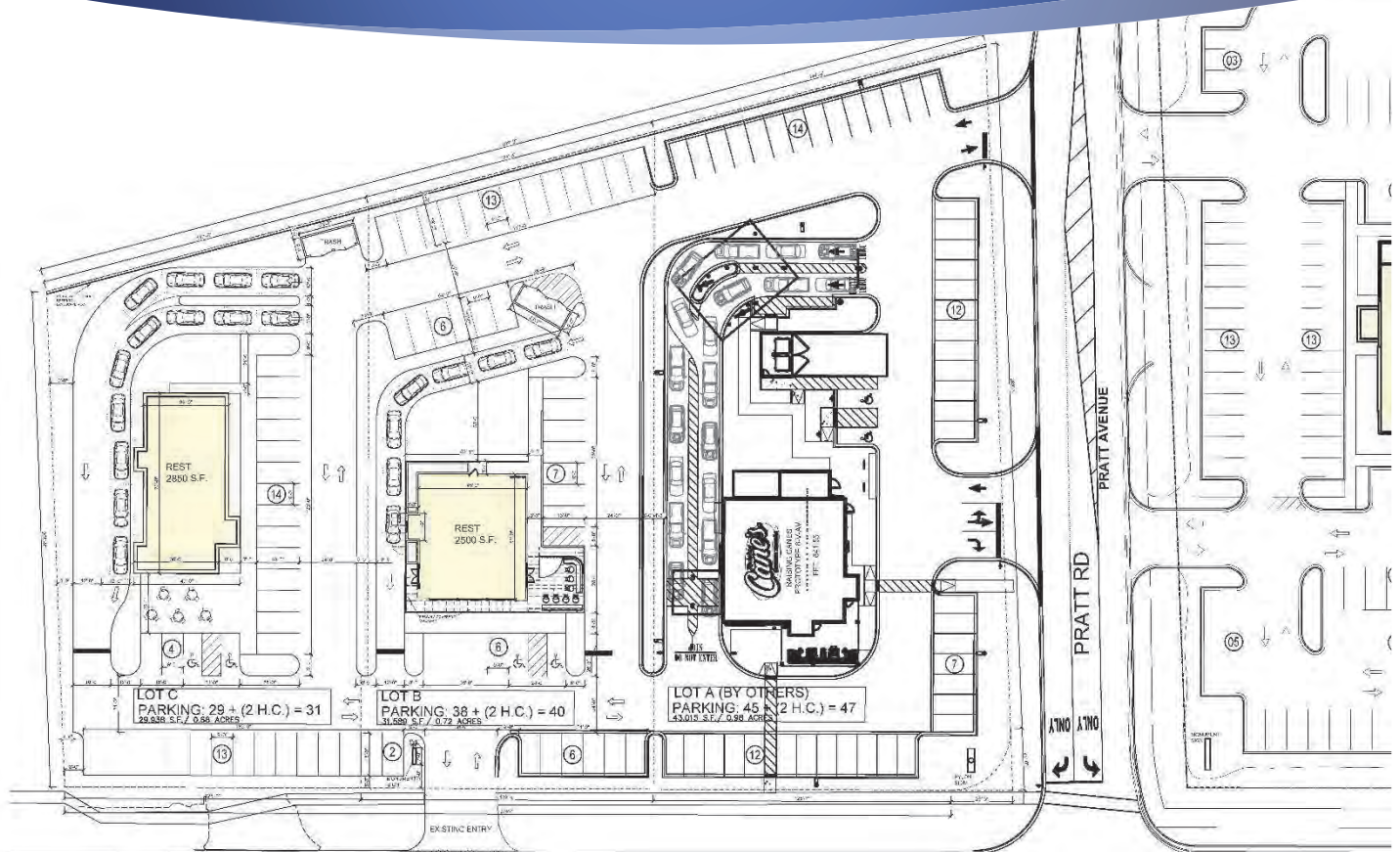
Public Works and Engineering has reviewed the plans for the above and have the following general comments:

- We are concerned that the traffic study did not address the traffic from All-State Arena who do have several events that occur late morning and during the peak traffic hour of 5pm during the week.
- Parking for events at the All-State arena should not be allowed to occur at the above proposed location during any All-State arena events, since the parking that is proposed is for the restaurant patrons only.
- The developer will be responsible for removing and replacing the existing water main along the west side of the development, and street widening and resurfacing on Pratt Av.
- IDOT is requiring a turn lane to be added in Mannheim Rd., and the city will work with the Village of Rosemont and the developer on this requirement.

JL/jl

Traffic Impact Study 2777 Mannheim Retail Development

Des Plaines, Illinois



Prepared For:



April 7, 2023

1. Introduction

A traffic impact study was conducted for the proposed 2777 Mannheim retail development to be located in the northeast quadrant of the intersection of Mannheim Road with Pratt Avenue in Des Plaines, Illinois. The intersection of Mannheim Road with Pratt Avenue is unsignalized. The plans call for three buildings consisting of the following:

- Building A – An approximately 2,850 square-foot restaurant with a drive-through lane.
- Building B – An approximately 2,500 square-foot restaurant with a drive-through lane.
- Building C – An approximate 2,820 square-foot Raising Cane’s restaurant with a drive-through lane.

The site is proposed to be served by one full-movement access drive off Mannheim Road and one full-movement access drive off Pratt Avenue.

The sections of this report present the following:

- Existing roadway conditions including vehicle, pedestrian, and bicycle traffic volumes for the weekday morning and weekday evening peak hours
- A detailed description of the proposed development
- Vehicle trip generation for the proposed development
- Directional distribution of development-generated traffic
- Regional growth in traffic and background development traffic for Year 2028 no-build conditions
- Future transportation conditions including access to and from the development

Traffic capacity analyses were conducted for the weekday morning and weekday evening peak hours for the following three conditions:

1. Base Conditions – Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes in the surrounding area adjusted to reflect normal conditions.
2. No-Build Conditions – Analyzes the capacity of the existing roadway system using the base traffic volumes increased by an ambient growth factor (growth not attributable to and particular development) and any background developments in the area.
3. Year 2028 Total Projected Conditions – The total projected traffic volumes include the base traffic volumes increased by an ambient growth factor, traffic from planned background developments in the immediate area, and the traffic estimated to be generated by the proposed subject development.

The purpose of this study was to examine existing traffic conditions to establish a base condition, assess the impact that the proposed development would have on traffic conditions in the area, and determine the roadway and traffic control improvements needed to mitigate this development’s impact based on Year 2028 projected traffic conditions.

2. Existing Conditions

Existing traffic and roadway conditions were documented based on field visits and traffic counts conducted by KLOA, Inc. The following provides a detailed description of the physical characteristics of the roadways including geometry and traffic control, adjacent land uses, and peak hour traffic flows along area roadways.

Site Location

The development site is located in the northeast quadrant of the unsignalized intersection of Mannheim Road with Pratt Avenue in Des Plaines, Illinois and is generally bordered by Pratt Avenue to the south, Royal Touch Car Wash to the north, Canadian National Railway (CN) railroad tracks to the east, and Mannheim Road to the west. The site is currently occupied by Café La Cave restaurant and banquets and its parking lot. There are two curb cuts along Mannheim Road and one on Pratt Avenue providing access to the site. **Figure 1** shows the location of the site in relation to the area roadway system. **Figure 2** shows an aerial view of the site.

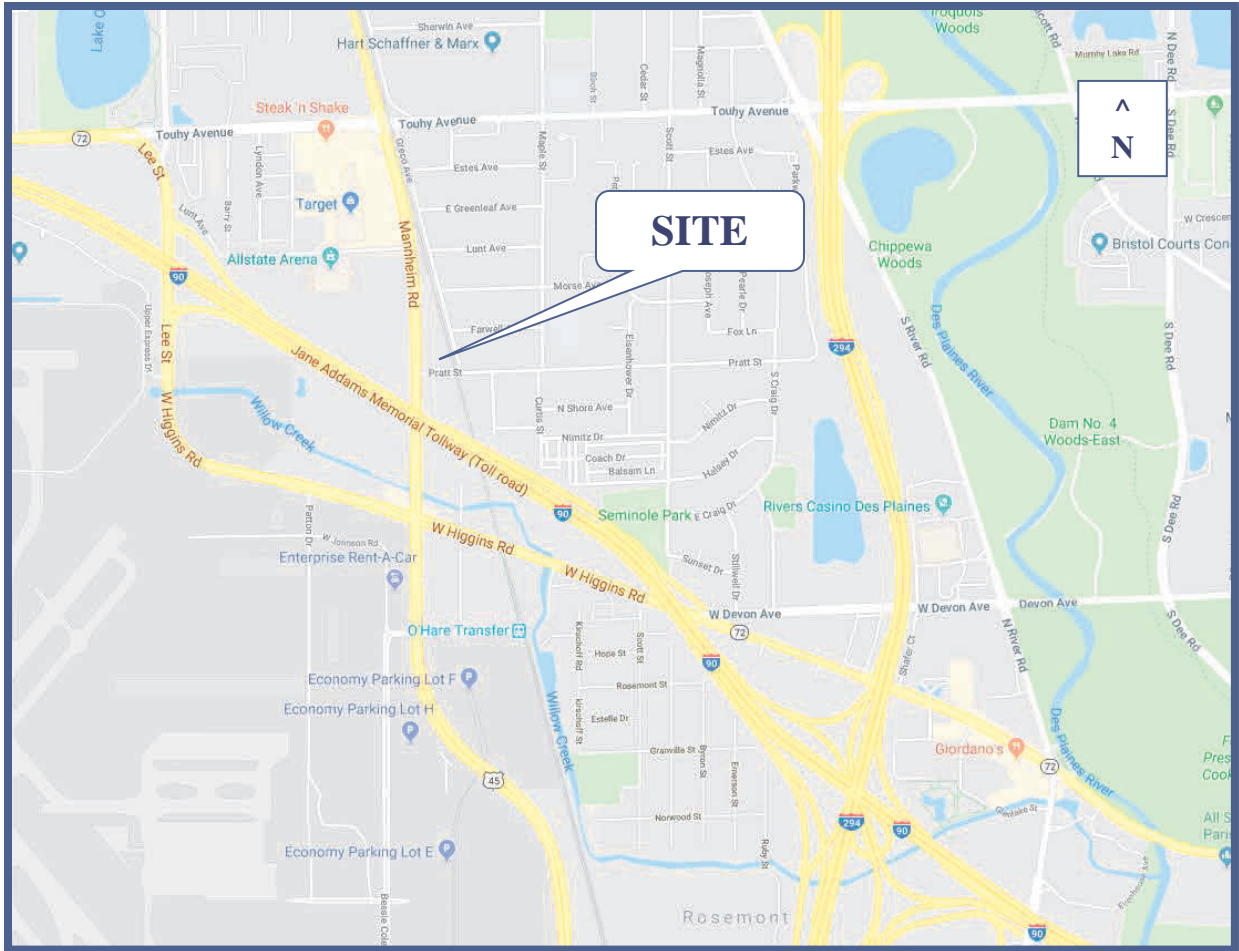
Surrounding Land Uses

Land uses surrounding the site include Royal Touch Car Wash and a retail strip containing Starbucks and Potbelly to the north, residential to the east, and Hyatt Place hotel and Allstate Arena to the west of Mannheim Road. To the south is a currently vacant land parcel that is proposed for a retail development. The Rosemont Marketplace shopping center anchored by Target is located in the northwest quadrant of the signalized intersection of Mannheim Road with Lunt Avenue.

Existing Roadway System Characteristics

The characteristics of the existing roadways that surround the proposed development are illustrated in **Figure 3** and described below.

Mannheim Road (US Routes 12 and 45) is a north-south other principal arterial roadway that generally provides two through lanes in each direction. At its signalized intersection with Lunt Avenue, an exclusive left-turn lane and two through lanes are provided on the northbound approach and a through lane and a combined through/right-turn lane is provided on the southbound approach along with a high-visibility crosswalk. No exclusive turn lanes are provided on Mannheim Road at its unsignalized intersection with Pratt Avenue or any of the access drives included in the study area. A sidewalk with a grass easement is provided on the east side of the roadway. A sidewalk located immediately adjacent to the roadway is provided on the west side of the roadway beginning at the Main Access Drive serving the site/Hyatt Hotel parking and extending north. The roadway is designated as a Strategic Regional Arterial (SRA) and is under the jurisdiction of the Illinois Department of Transportation (IDOT). Mannheim Road has a posted speed limit of 40 mph and carries an Annual Average Daily Traffic (AADT) volume of 24,700 vehicles (IDOT 2019).



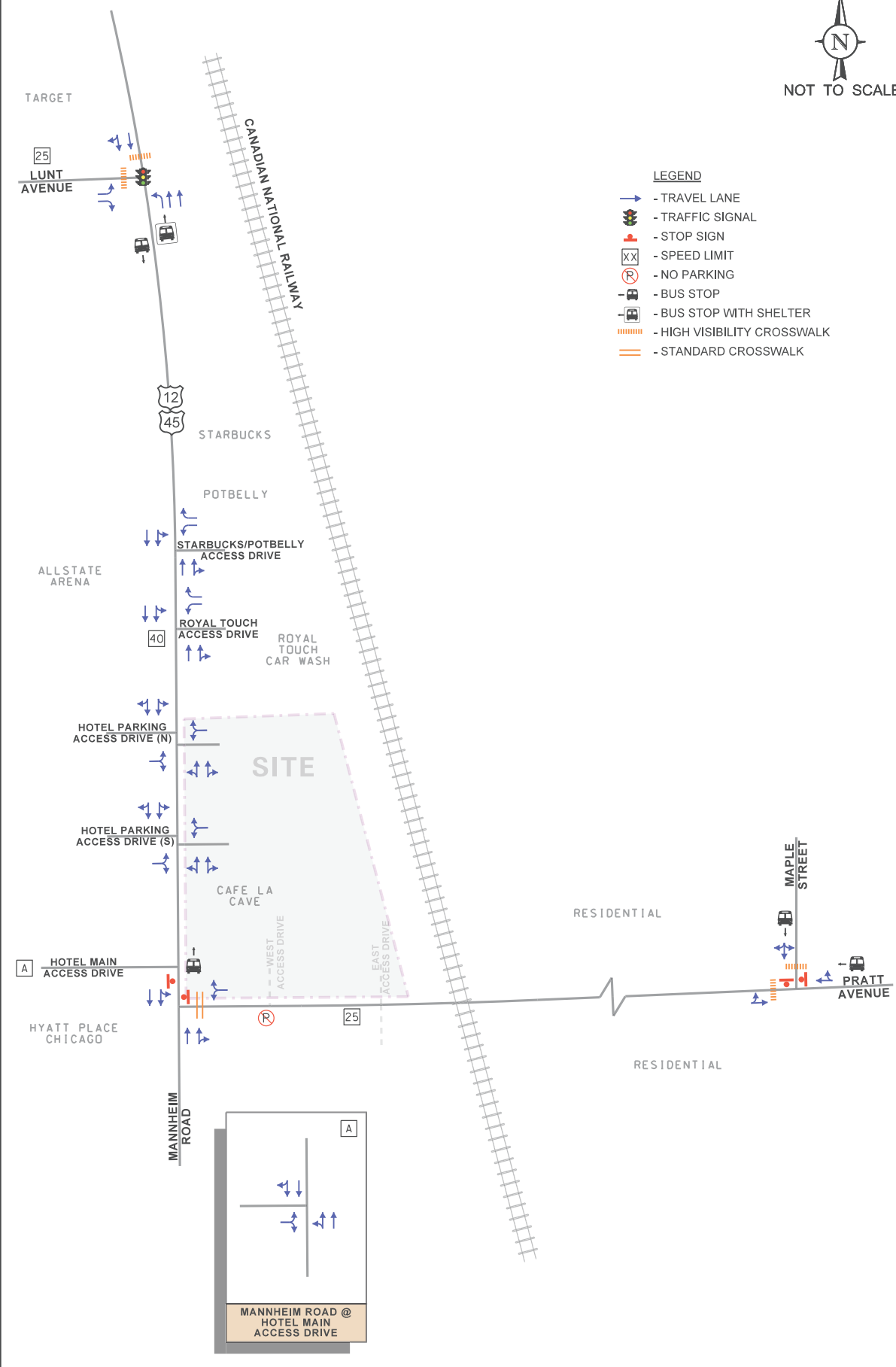
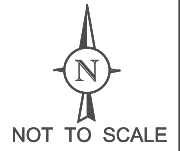
Site Location

Figure 1



Aerial View of Site

Figure 2



- LEGEND**
- TRAVEL LANE
 - TRAFFIC SIGNAL
 - STOP SIGN
 - SPEED LIMIT
 - NO PARKING
 - BUS STOP
 - BUS STOP WITH SHELTER
 - HIGH VISIBILITY CROSSWALK
 - STANDARD CROSSWALK

2777 MANNHEIM ROAD
DES PLAINES, ILLINOIS

EXISTING ROADWAY CHARACTERISTICS

KLOA
Kenig, Lindgren, O'Hara, Aboona, Inc.
Job No: 22-243 Figure: 3

Pratt Avenue is an east-west local roadway that is under stop sign control at its three-way intersection with Mannheim Road (US Routes 12 and 45) providing one inbound lane and one outbound lane allowing both left- and right-turn exiting movements. The westbound approach provides a standard-style crosswalk. Pratt Avenue is under all-way stop sign control at its intersection with Maple Street to the east of Mannheim Road and provides a high-visibility crosswalk in the middle of the offset intersection. There is an at-grade railroad crossing between Mannheim Road and Maple Street. A sidewalk with a grass easement is provided on both sides of the roadway. Pratt Avenue carries an AADT volume of 1,950 vehicles (IDOT 2018). The posted speed limit is 25 mph and parking is restricted on both sides of the roadway. Pratt Avenue is under the jurisdiction of the City of Des Plaines.

Maple Street is a north-south local roadway that provides one lane in each direction and T-intersect Pratt Avenue as an offset intersection with Curtis Street to the south. The southbound approach provides a high-visibility crosswalk. A sidewalk with a grass easement is provided on both sides of the roadway. On-street parking is permitted on both sides of the roadway and the posted speed limit is 25 mph. Maple Street is under the jurisdiction of the City of Des Plaines.

Lunt Avenue is an east-west roadway that provides one lane in each direction and is under the jurisdiction of the Village of Rosemont. At its signalized intersection with Mannheim Road, Lunt Avenue provides an exclusive left-turn lane and an exclusive right-turn lane. Lunt Avenue provides a high-visibility crosswalk on the eastbound approach. A sidewalk located immediately adjacent to the roadway is provided on both sides of the roadway. Parking is restricted on both sides of the roadway and the posted speed limit is 25 mph.

Existing Traffic Volumes

Vehicle, pedestrian, and bicycle traffic counts were conducted on Tuesday, August 9, 2022 during the morning (7:00 to 9:00 A.M.) and the evening (4:00 to 6:00 P.M.) peak periods at the following intersections:

- Mannheim Road with Pratt Avenue
- Mannheim Road with Hotel Main Access Drive
- Mannheim Road with Café La Cave Access Drive/Hotel Parking South Access Drive
- Mannheim Road with Royal Touch Carwash Access Drive
- Mannheim Road with Potbelly/Starbucks Access Drive

From the count data, it was determined that the weekday morning peak hour generally occurs between 8:00 and 9:00 A.M. and the weekday evening peak hour generally occurs between 4:30 and 5:30 P.M. These two respective peak hours will be used for the traffic capacity analyses and are presented later in this report. Pedestrian and bicycle activity was reported to be very low at the study intersections. A copy of the traffic count summary sheets is included in the Appendix.

Furthermore, traffic counts previously conducted in 2018 were utilized for the following two intersections:

- Mannheim Road with Lunt Avenue
- Maple Street with Pratt Avenue

These counts were adjusted to represent 2022 with an annual growth rate (to be discussed later) over four years.

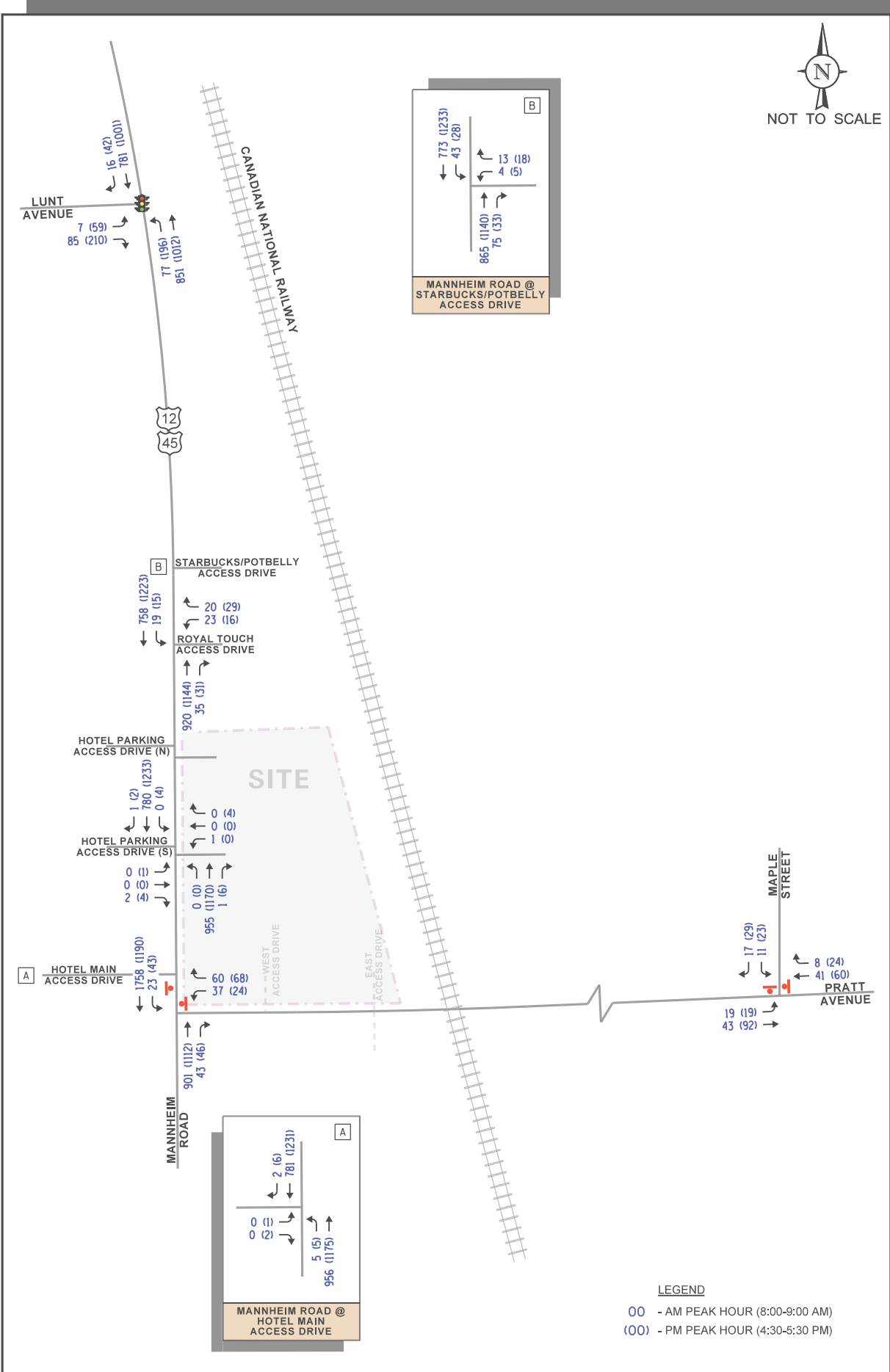
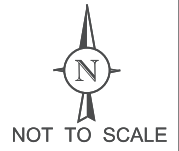
The traffic counts conducted in 2022 were compared with 2018 counts in the area adjusted to 2022 and it was determined that an increase of 20 percent on the through volumes along Mannheim Road was necessary to reflect normal traffic conditions.

While no events were taking place at Allstate Arena when the traffic counts were conducted, the traffic volumes represent typical traffic conditions, especially given that these events primarily occur late in the evenings on weekday and on weekends during which the traffic that will be generated by the proposed development will be lower. As such, these events will not have an adverse impact on the traffic operations of the proposed development

The base Year 2022 peak hour vehicle traffic volumes are shown in **Figure 4**.

Railroad Crossing

As noted, there is an at-grade, gated railroad crossing on Pratt Avenue approximately 350 feet east of Mannheim Road. Based on information provided by the Illinois Commerce Commission (ICC) which is included in the Appendix of this report, this railroad line carries both passenger trains and freight trains with 38 average daily trains, a daily average of 22 of which are passenger trains. Based on observations, the gates were lowered for passing trains during the morning peak hour three times for an average of 55 seconds with an average eastbound queue of one vehicle. The gates were lowered for passing trains during the evening peak hour three times for an average of 60 seconds with an average eastbound queue of one vehicle. It is important to note that anecdotal observations have noted that freight trains sometimes stop in the vicinity of Pratt Avenue, resulting in the gates being lowered for an extended period of time. Vehicles east of the tracks can use the various roadways in the area, including Maple Street, as a detour. Vehicles west of the tracks must make a U-turn on Pratt Avenue and return to Mannheim Road.



LEGEND
 00 - AM PEAK HOUR (8:00-9:00 AM)
 (00) - PM PEAK HOUR (4:30-5:30 PM)

2777 MANNHEIM ROAD
 DES PLAINES, ILLINOIS

YEAR 2022 BASE TRAFFIC VOLUMES



Job No: 22-243 Figure: 4

Crash Data Analysis

KLOA, Inc. obtained crash data from IDOT for the most recent past five years available (2017 to 2021) for the intersections of Mannheim Road (US Route 12 and 45) with Lunt Avenue, Allstate Arena/Royal Touch Car Wash Access Drive, and Pratt Avenue and Pratt Avenue with Maple Street. A review of the crash data indicated that no fatalities were reported at any of the intersections¹. No crashes were reported at the intersections of Mannheim Road with Allstate Arena/Royal Touch Car Wash Access Drive or Pratt Avenue with Maple Street during the surveyed period. **Tables 1** and **2** summarize the crash data.

Table 1

MANNHEIM ROAD WITH LUNT AVENUE – CRASH SUMMARY

| Year | Type of Crash Frequency | | | | | | | Total |
|----------------|-------------------------|----------------|------------|------------|----------------|------------|----------------|------------|
| | Angle | Head On | Object | Rear End | Sideswipe | Turning | Other | |
| 2017 | 0 | 1 | 0 | 0 | 0 | 1 | 0 | 2 |
| 2018 | 0 | 0 | 0 | 1 | 0 | 3 | 0 | 4 |
| 2019 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 2 |
| 2020 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 1 |
| 2021 | <u>0</u> | <u>0</u> | <u>0</u> | <u>3</u> | <u>1</u> | <u>1</u> | <u>0</u> | <u>5</u> |
| Total | 0 | 1 | 0 | 6 | 1 | 5 | 1 | 14 |
| Average | 0.0 | <1.0 | 0.0 | 1.2 | <1.0 | 1.0 | <1.0 | 2.8 |

Table 2

MANNHEIM ROAD WITH PRATT AVENUE – CRASH SUMMARY

| Year | Type of Crash Frequency | | | | | | | Total |
|----------------|-------------------------|------------|------------|----------------|------------|------------|------------|------------|
| | Angle | Head On | Object | Rear End | Sideswipe | Turning | Other | |
| 2017 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 |
| 2018 | 0 | 0 | 0 | 2 | 0 | 2 | 0 | 4 |
| 2019 | 0 | 0 | 0 | 1 | 0 | 2 | 0 | 3 |
| 2020 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 4 |
| 2021 | <u>0</u> | <u>0</u> | <u>0</u> | <u>1</u> | <u>0</u> | <u>4</u> | <u>0</u> | <u>5</u> |
| Total | 0 | 0 | 0 | 4 | 0 | 13 | 0 | 17 |
| Average | 0.0 | 0.0 | 0.0 | <1.0 | 0.0 | 2.6 | 0.0 | 3.4 |

¹ IDOT DISCLAIMER: The motor vehicle crash data referenced herein was provided by the Illinois Department of Transportation. Any conclusions drawn from analysis of the aforementioned data are the sole responsibility of the data recipient(s). Additionally, for coding years 2015 to present, the Bureau of Data Collection uses the exact latitude/longitude supplied by the investigating law enforcement agency to locate crashes. Therefore, location data may vary in previous years since data prior to 2015 was physically located by bureau personnel. The author is responsible for any data analyses and conclusions drawn.

3. Traffic Characteristics of the Proposed Development

To evaluate the impact of the subject development on the area roadway system, it was necessary to quantify the number of vehicle trips the overall site will generate during the weekday morning and weekday evening peak hours and then determine the directions from which the proposed traffic will approach and depart the site, given the proposed access points that will serve the overall development.

Proposed Site and Development Plan

The site is located in the northeast quadrant of the unsignalized intersection of Mannheim Road with Pratt Avenue in Des Plaines, Illinois. The plans call for three buildings consisting of the following:

- Building A – An approximately 2,850 square-foot restaurant with a drive-through lane.
- Building B – An approximately 2,500 square-foot restaurant with a drive-through lane.
- Building C – An approximate 2,820 square-foot Raising Canes restaurant with a drive-through lane.

The site is proposed to be served by one full access drive off Mannheim Road and two full access drives off Pratt Avenue, one of which is existing and currently serves the site. The parking lot serving the buildings is proposed to provide 118 parking spaces, including four accessible spaces. A copy of the site plan is included in the Appendix.

Vehicle Access

The three access points proposed to serve the development are described below:

Existing Access Drive with Mannheim Road. This full access drive is located off Mannheim Road approximately 265 feet north of Pratt Avenue at the far north end of the site. This access drive is opposite the Hotel Parking South Access Drive. The access drive provides one inbound lane and one outbound lane under stop sign control.

Proposed West Access Drive with Pratt Avenue. This full access drive is proposed to be located off Pratt Avenue approximately 165 feet east of Mannheim Road. The access drive is proposed to provide one inbound lane and two outbound lanes under stop sign control. The two outbound lanes will be striped to provide one left-turn lane and one shared through/right-turn lane. This access drive will align with the proposed full access drive that will serve the planned retail development to the south.

Proposed East Access Drive with Pratt Avenue. Located approximately 330 feet east of Mannheim Road, this access drive is proposed to provide one inbound lane and one outbound lane under stop sign control. This access drive would align with a proposed full access drive to the proposed development to the south.

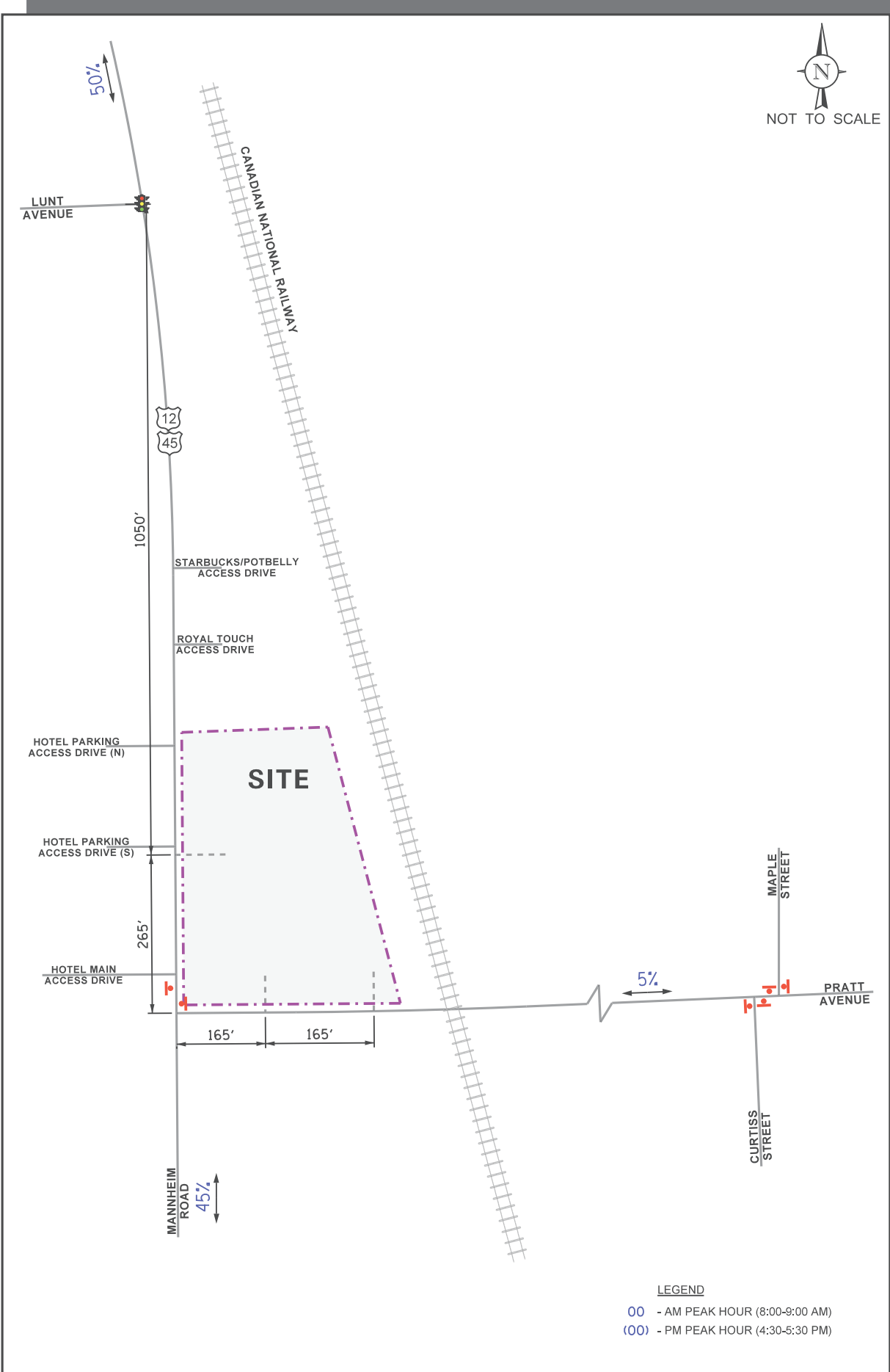
Directional Distribution of Development Traffic

The directional distribution of how traffic will approach and depart the site was estimated based on the general travel patterns through the study area derived from the peak hour traffic volumes and previous studies conducted in the area. **Figure 5** shows the directional distribution established for this development. Further, Figure 5 shows the distance, in feet, between the existing and proposed intersections analyzed in this study.

Development Traffic Generation

The estimates of vehicle traffic to be generated by the proposed development are based upon the proposed land use types and sizes. The volume of traffic generated for the proposed development was estimated using data published in the Institute of Transportation Engineers' (ITE) *Trip Generation Manual*, 11th Edition. It should be noted that the Raising Cane's restaurant does not serve breakfast and therefore does not generate traffic during the weekday morning peak hour. Further, a pass-by vehicle trip reduction of 50 percent was applied to the restaurant uses. Pass-by vehicle trips are those vehicles already on the adjacent roadway (i.e. Mannheim Road) en route to another destination (i.e. work to home or vice versa) and are not considered new vehicle trips to the roadway system.

Table 3 tabulates the vehicle trips anticipated for this development for the weekday morning and weekday evening peak hours as well as the daily (two-way) traffic volumes.



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DES PLAINES, ILLINOIS

DIRECTIONAL DISTRIBUTION



Table 3
ESTIMATED DEVELOPMENT-GENERATED PEAK HOUR TRAFFIC VOLUMES

| ITE Land-Use Code | Type/Size | Weekday Morning Peak Hour | | | Weekday Evening Peak Hour | | | Daily Two-Way Traffic | | |
|---------------------------------|---|---------------------------|------------|-------------|---------------------------|------------|-------------|-----------------------|--------------|---------------|
| | | In | Out | Total | In | Out | Total | In | Out | Total |
| 934 | Fast Food Restaurant with Drive-Through (2,820 s.f.) ¹ | - | - | - | 48 | 45 | 93 | 659 | 659 | 1,318 |
| 934 | Fast Food Restaurant with Drive-Through (2,500 s.f.) | 57 | 55 | 112 | 43 | 40 | 83 | 585 | 585 | 1,170 |
| 934 | Fast Food Restaurant with Drive-Through (2,850 s.f.) | <u>65</u> | <u>62</u> | <u>127</u> | <u>49</u> | <u>45</u> | <u>94</u> | <u>666</u> | <u>666</u> | <u>1,332</u> |
| Total Vehicle Trips: | | 122 | 117 | 239 | 140 | 130 | 270 | 1,910 | 1,910 | 3,820 |
| <i>Pass-By Trips (50%):</i> | | <i>-59</i> | <i>-59</i> | <i>-118</i> | <i>-65</i> | <i>-65</i> | <i>-130</i> | <i>-955</i> | <i>-955</i> | <i>-1,910</i> |
| Total New Vehicle Trips: | | 63 | 58 | 121 | 75 | 65 | 140 | 955 | 955 | 1,910 |

1: Not open for breakfast



Development Traffic Assignment

The peak hour traffic volumes projected to be generated by the proposed development (Table 3) were assigned to the area roadways based on the directional distribution analysis (Figure 5).

Figure 6 shows the assignment of the new vehicle trips. **Figure 7** shows the assignment of the pass-by vehicle trips.

Year 2028 Background (No-Build) Traffic Conditions

To account for the increase in existing traffic related to regional growth in the area (i.e. not attributable to any particular planned development) for Year 2028 conditions, the base traffic volumes were increased by a total of three percent. This percentage increase is based on AADT projections provided by the Chicago Metropolitan Agency for Planning (CMAP) in a letter dated August 23, 2021. A copy of the CMAP letter is included in the Appendix.

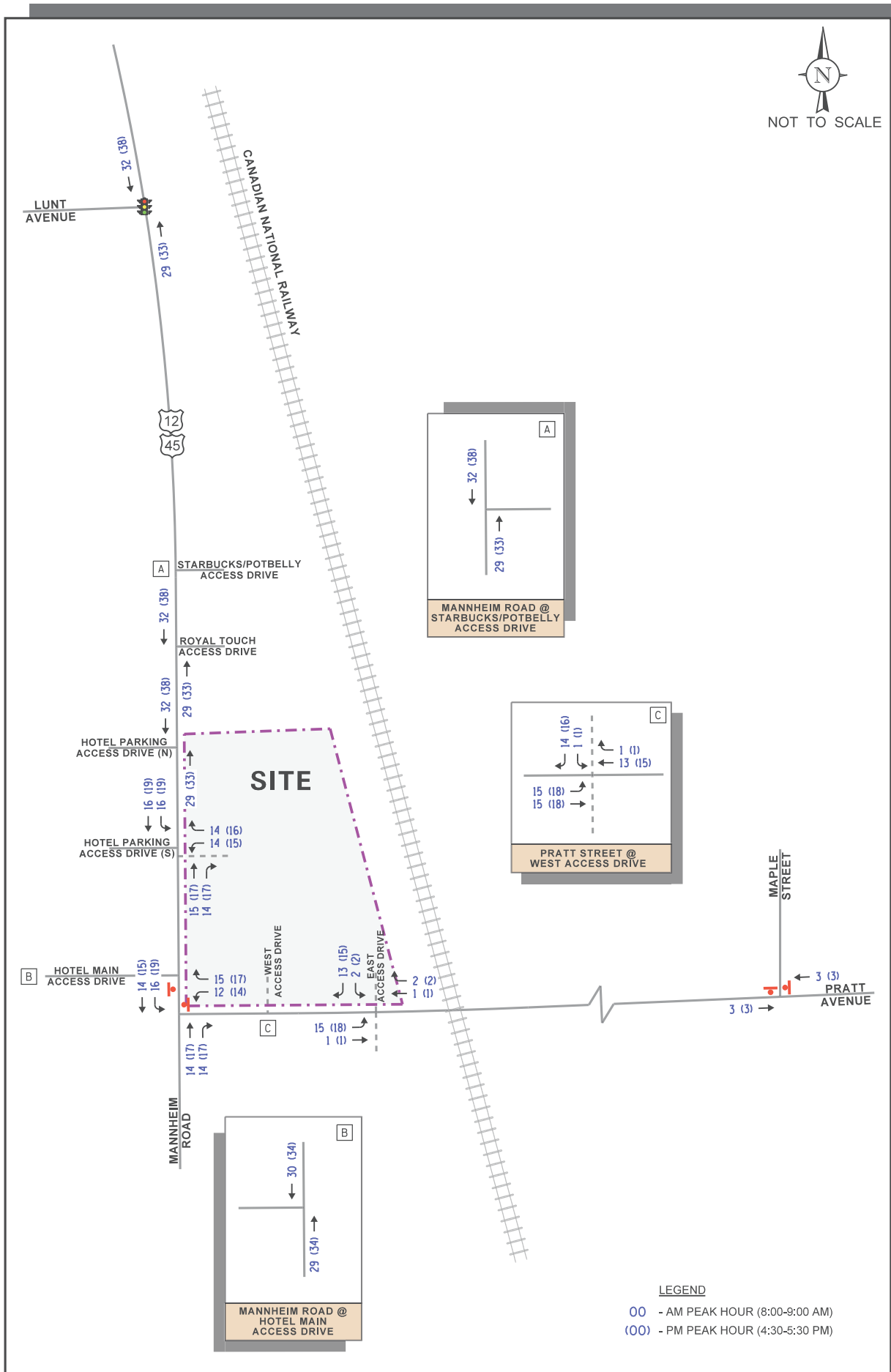
Also included in the background traffic is the traffic estimated to be generated by the planned retail development to be located in the southeast corner of Mannheim Road with Pratt Avenue.

Figure 8 shows the Year 2028 no-build traffic volumes, which do not include the traffic estimated to be generated by the subject development.

Year 2028 Total Projected Traffic Conditions

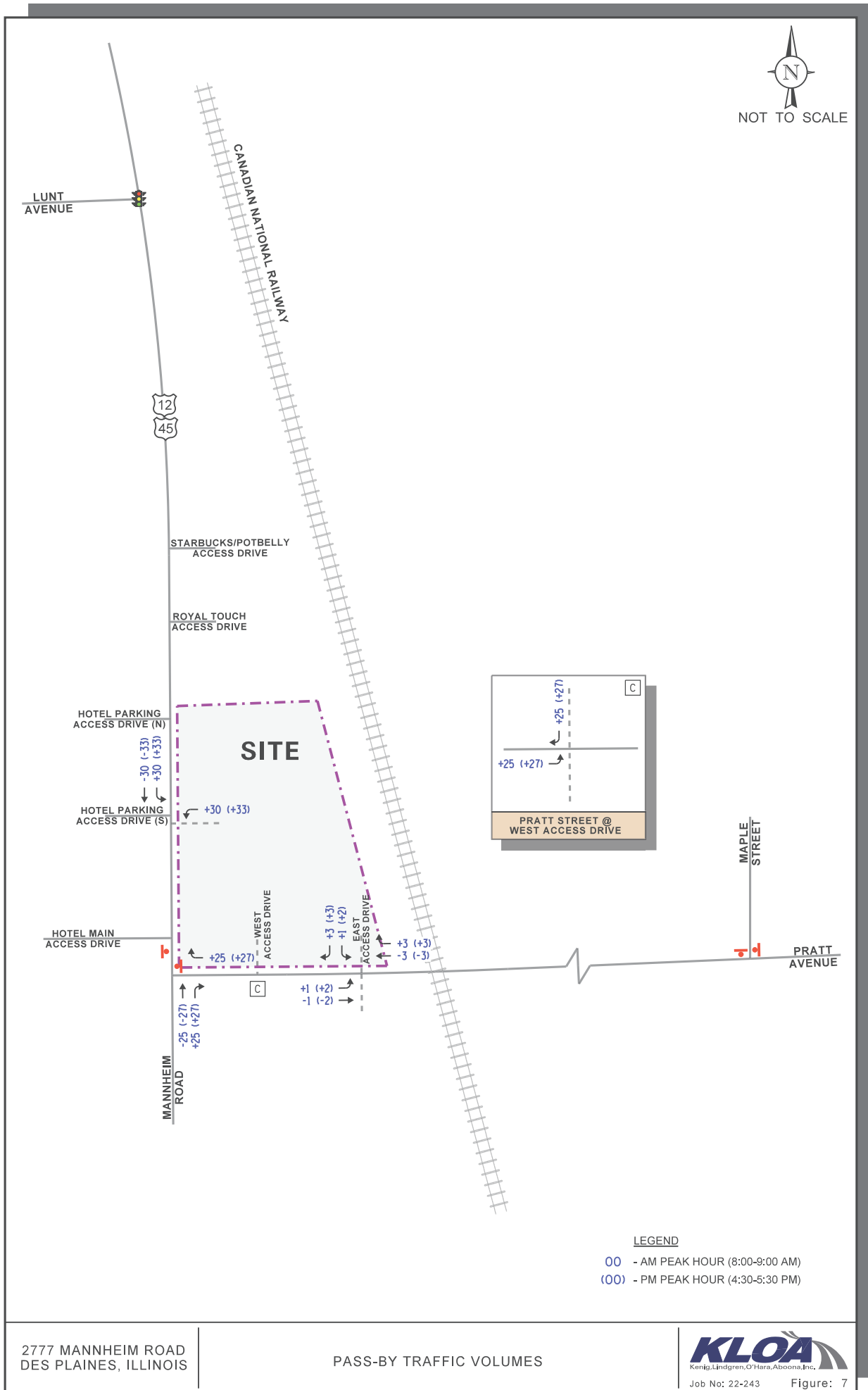
The Year 2028 total projected traffic volumes include the Year 2028 no-build traffic volumes (Figure 8) plus the traffic estimated to be generated by the proposed subject development (Figures 6 and 7).

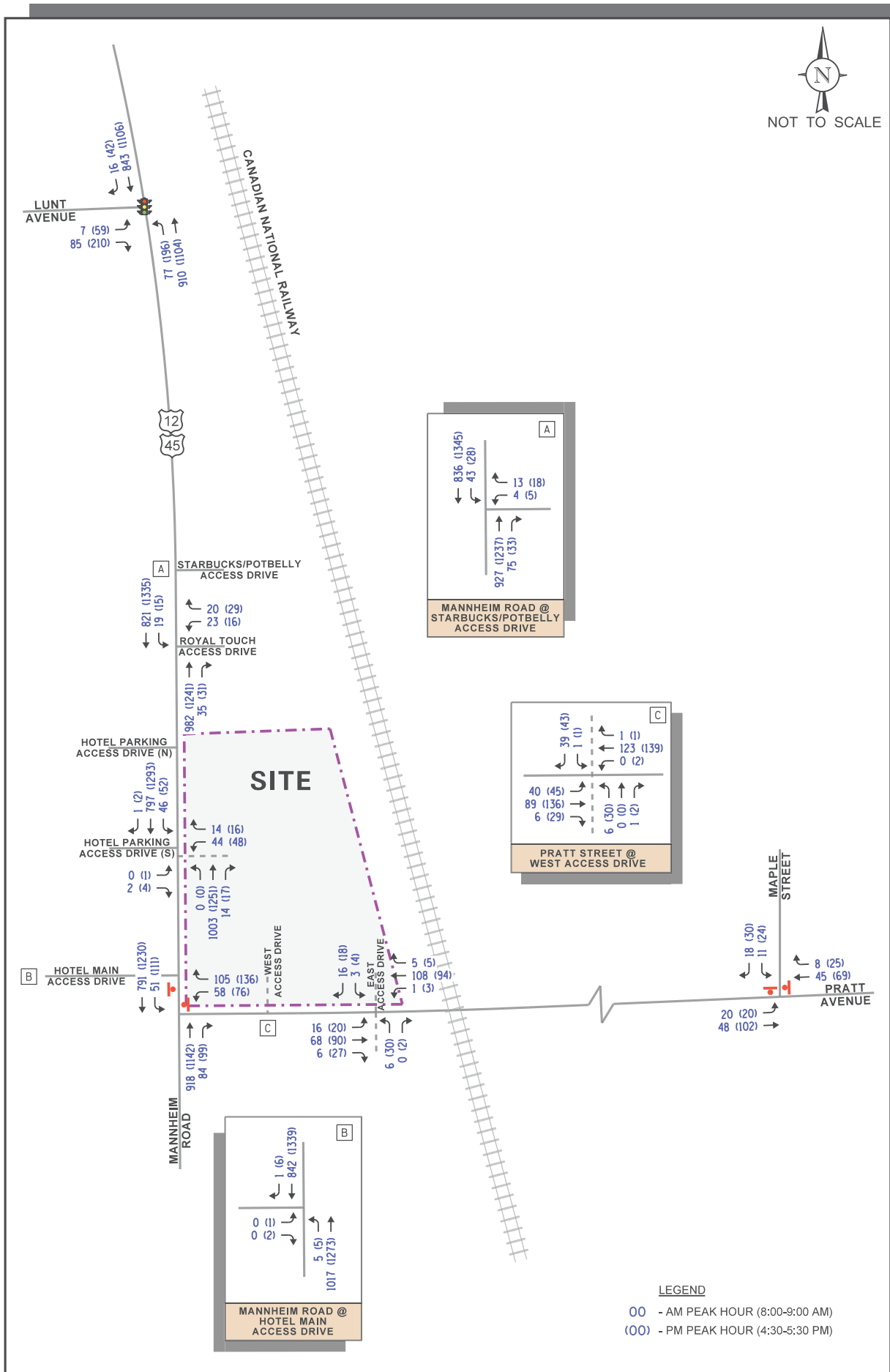
Figure 9 shows the Year 2028 total projected traffic volumes.



2777 MANNHEIM ROAD
DES PLAINES, ILLINOIS

SITE-GENERATED TRAFFIC VOLUMES
NEW TRIPS





2777 MANNHEIM ROAD
DES PLAINES, ILLINOIS

YEAR 2028 TOTAL TRAFFIC VOLUMES

4. Traffic Analysis and Recommendations

Capacity analyses were performed for the key intersections included in the study area to determine the ability of the existing roadway system to accommodate existing and future traffic demands. Analyses were performed for the weekday morning and weekday evening peak hours for the base (Year 2022), no-build (Year 2028), and total projected future (Year 2028) conditions.

The traffic analyses were performed using the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM)*, 6th Edition and using Synchro/SimTraffic 11 analysis software.

The signalized intersection of Mannheim Road with Lunt Avenue was analyzed using existing signal cycle lengths (110 seconds weekday morning and evening) offsets, and phasings to determine the average overall vehicle delay, volume-to-capacity ratios, and levels of service.

The analyses for the unsignalized intersections determine the average control delay to vehicles at an intersection. Control delay is the elapsed time from a vehicle joining the queue at a stop sign (includes the time required to decelerate to a stop) until its departure from the stop sign and resumption of free flow speed. The methodology analyzes each intersection approach controlled by a stop sign and considers traffic volumes on all approaches and lane characteristics.

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter from A to F based on the average control delay experienced by vehicles passing through the intersection. The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for signalized intersections and unsignalized intersections are included in the Appendix of this report.

A summary of the traffic analysis results showing the LOS and delay (measured in seconds) for the signalized intersection for the base (Year 2022), no-build (Year 2028), and future (Year 2028) conditions is shown in **Table 4**. The unsignalized intersections are presented in **Tables 5** through **7**. A copy of the capacity analysis reports is included in the Appendix. A discussion of each of the intersections follows.

Table 4
CAPACITY ANALYSIS RESULTS – MANNHEIM ROAD WITH LUNT AVENUE – SIGNALIZED

| Peak Hour | Eastbound | | Northbound | | | Southbound | Overall |
|--------------------------------|-----------------|------------------------|------------|----------|-----------|------------|---------|
| | L | R | L | T | T/R | | |
| Year 2022 Base Conditions | Weekday Morning | D 42.1 E 58.3 | A 3.2 | A 3.6 | A 7.1 | A 7.8 | |
| | Weekday Evening | D 47.5 E 71.8 | B 10.4 | A 6.9 | B 12.9 | B 16.6 | |
| Year 2028 No-Build Conditions | Weekday Morning | D 42.1 E 57.1 | A 3.2 | A 3.7 | A 7.2 | A 7.8 | |
| | Weekday Evening | D 47.5 E 71.8 | B 11.9 | A 7.1 | B 13.3 | B 16.6 | |
| Year 2028 Projected Conditions | Weekday Morning | D 42.1 E 57.1 | A 3.3 | A 3.8 | A 7.4 | A 7.8 | |
| | Weekday Evening | D 47.5 E 71.8 | B 12.8 | A 7.2 | B 13.6 | B 16.7 | |

Letter denotes Level of Service L – Left Turn R – Right Turn
Delay is measured in seconds. T – Through



Table 5
CAPACITY ANALYSIS RESULTS – UNSIGNALIZED INTERSECTIONS
YEAR 2022 BASE CONDITIONS

| Intersection | Weekday Morning Peak Hour | | Weekday Evening Peak Hour | |
|--|---------------------------|-------------------------------|---------------------------|-------|
| | LOS | Delay | LOS | Delay |
| Maple Street/Curtis Street with Pratt Avenue¹ | | | | |
| • Overall | A | 7.5 | A | 7.6 |
| • Eastbound Approach | A | 7.7 | A | 7.8 |
| • Westbound Approach | A | 7.3 | A | 7.4 |
| • Southbound Approach | A | 7.5 | A | 7.7 |
| Mannheim Road with Pratt Avenue² | | | | |
| • Westbound Approach | B | 12.5 | B | 14.3 |
| • Southbound Left Turn | A | 8.5 | A | 9.0 |
| Mannheim Road with Hotel Main Access Drive² | | | | |
| • Eastbound Approach | A | 0.0 | B | 12.8 |
| • Northbound Left Turn | A | 8.1 | A | 9.1 |
| Mannheim Road with Café La Cave Access Drive/Hotel Parking South Access Drive² | | | | |
| • Eastbound Approach | A | 9.7 | B | 11.9 |
| • Westbound Approach | B | 11.8 | B | 11.1 |
| • Northbound Left Turn | A | 0.0 | A | 0.0 |
| • Southbound Left Turn | A | 0.0 | A | 9.0 |
| Mannheim Road with Royal Touch Car Wash Access Drive² | | | | |
| • Westbound Approach | B | 11.7 | B | 13.8 |
| • Southbound Left Turn | A | 8.4 | A | 9.0 |
| Mannheim Road with Potbelly/Starbucks Access Drive² | | | | |
| • Westbound Approach | B | 10.6 | B | 12.7 |
| • Southbound Left Turn | A | 8.5 | A | 9.0 |
| LOS = Level of Service | | 1 – All-way stop sign-control | | |
| Delay is measured in seconds. | | 2 – Two-way stop sign-control | | |

Table 6
 CAPACITY ANALYSIS RESULTS – UNSIGNALIZED INTERSECTIONS
 YEAR 2028 NO-BUILD CONDITIONS

| Intersection | Weekday Morning Peak Hour | | Weekday Evening Peak Hour | |
|--|---------------------------|-------------------------------|---------------------------|-------|
| | LOS | Delay | LOS | Delay |
| Maple Street/Curtis Street with Pratt Avenue¹ | | | | |
| • Overall | A | 7.5 | A | 7.7 |
| • Eastbound Approach | A | 7.7 | A | 7.9 |
| • Westbound Approach | A | 7.3 | A | 7.5 |
| • Southbound Approach | A | 7.5 | A | 7.8 |
| Mannheim Road with Pratt Avenue² | | | | |
| • Westbound Approach | B | 13.2 | C | 22.7 |
| • Southbound Left Turn | A | 8.5 | A | 9.4 |
| Mannheim Road with Hotel Main Access Drive² | | | | |
| • Eastbound Approach | A | 0.0 | B | 13.3 |
| • Northbound Left Turn | A | 8.1 | A | 9.3 |
| Mannheim Road with Café La Cave Access Drive/Hotel Parking South Access Drive² | | | | |
| • Eastbound Approach | A | 9.7 | B | 12.2 |
| • Westbound Approach | B | 12.0 | B | 11.3 |
| • Northbound Left Turn | A | 0.0 | A | 0.0 |
| • Southbound Left Turn | A | 0.0 | A | 9.2 |
| Mannheim Road with Royal Touch Car Wash Access Drive² | | | | |
| • Westbound Approach | B | 11.9 | B | 14.9 |
| • Southbound Left Turn | A | 8.5 | A | 9.3 |
| Mannheim Road with Potbelly/Starbucks Access Drive² | | | | |
| • Westbound Approach | B | 10.8 | B | 13.7 |
| • Southbound Left Turn | A | 8.5 | A | 9.4 |
| LOS = Level of Service | | 1 – All-way stop sign-control | | |
| Delay is measured in seconds. | | 2 – Two-way stop sign-control | | |

Table 7
 CAPACITY ANALYSIS RESULTS – UNSIGNALIZED INTERSECTIONS
 YEAR 2028 FUTURE CONDITIONS

| Intersection | Weekday Morning Peak Hour | | Weekday Evening Peak Hour | |
|--|---------------------------|-------------------------------|---------------------------|-------|
| | LOS | Delay | LOS | Delay |
| Maple Street/Curtis Street with Pratt Avenue¹ | | | | |
| • Overall | A | 7.5 | A | 7.7 |
| • Eastbound Approach | A | 7.7 | A | 7.9 |
| • Westbound Approach | A | 7.3 | A | 7.5 |
| • Southbound Approach | A | 7.5 | A | 7.8 |
| Mannheim Road with Pratt Avenue² | | | | |
| • Westbound Approach | B | 14.3 | D | 29.3 |
| • Southbound Left Turn | A | 8.7 | A | 9.5 |
| Mannheim Road with Hotel Main Access Drive² | | | | |
| • Eastbound Approach | A | 0.0 | B | 13.6 |
| • Northbound Left Turn | A | 8.2 | A | 9.5 |
| Mannheim Road with Hotel Parking South Access Drive/Proposed Access Drive² | | | | |
| • Eastbound Approach | A | 9.7 | B | 12.5 |
| • Westbound Approach | B | 14.2 | C | 21.0 |
| • Northbound Left Turn | A | 0.0 | A | 0.0 |
| • Southbound Left Turn | A | 8.7 | A | 9.5 |
| Mannheim Road with Royal Touch Car Wash Access Drive² | | | | |
| • Westbound Approach | B | 12.2 | C | 15.0 |
| • Southbound Left Turn | A | 8.6 | A | 9.3 |
| Mannheim Road with Potbelly/Starbucks Access Drive² | | | | |
| • Westbound Approach | B | 10.9 | B | 14.0 |
| • Southbound Left Turn | A | 8.6 | A | 9.4 |
| LOS = Level of Service | | 1 – All-way stop sign-control | | |
| Delay is measured in seconds. | | 2 – Two-way stop sign-control | | |

Table 7 - CONTINUED
 CAPACITY ANALYSIS RESULTS – UNSIGNALIZED INTERSECTIONS
 YEAR 2028 FUTURE CONDITIONS

| Intersection | Weekday Morning Peak Hour | | Weekday Evening Peak Hour | |
|---|---------------------------|--|---------------------------|-------|
| | LOS | Delay | LOS | Delay |
| Pratt Avenue with Proposed East Access Drive² | | | | |
| • Northbound Approach | B | 10.1 | B | 10.6 |
| • Southbound Approach | A | 9.1 | A | 9.2 |
| • Eastbound Left Turn | A | 7.5 | A | 7.5 |
| • Westbound Left Turn | A | 7.4 | A | 7.5 |
| Pratt Avenue with Proposed West Access Drive² | | | | |
| • Northbound Approach | B | 10.6 | B | 12.6 |
| • Southbound Approach | A | 9.1 | A | 9.3 |
| • Eastbound Left Turn | A | 7.5 | A | 7.6 |
| • Westbound Left Turn | A | 0.0 | A | 7.6 |
| LOS = Level of Service Delay is measured in seconds. | | 1 – All-way stop sign-control 2 – Two-way stop sign-control | | |

Discussion and Recommendations

The following is an evaluation of the analyzed intersections based on the projected traffic volumes and the capacity analyses performed.

Mannheim Road with Lunt Avenue

This signalized intersection currently operates overall at Level of Service (LOS) A during the weekday morning peak hour and at LOS B during the weekday evening peak hour. The eastbound approach operates at LOS E during the weekday morning and weekday evening peak hours. Under Year 2028 no-build conditions, the intersection and approaches are projected to continue operating at the current levels of service with increases in delay of less than one second over Year 2022 base conditions. Under Year 2028 total projected conditions, the intersection and approaches are projected to continue operating at the current levels of service with increases in delay of less than one second over Year 2028 no-build conditions. The delay of LOS E on the eastbound approach is common and expected for a minor roadway such as Lunt Avenue that intersects a major arterial such as Mannheim Road. As such, no roadway or traffic control improvements are recommended at this signalized intersection in conjunction with the proposed development.

Mannheim Road with Pratt Avenue

The results of the capacity analysis indicate that the westbound approach from Pratt Avenue currently operates at LOS B during the weekday morning and evening peak hours. The southbound left turn currently operates at LOS A during both peak hours. Under existing conditions, Pratt Avenue T-intersects Mannheim Road from the east under stop sign control and provides one inbound lane and one outbound lane allowing left- and right-turn movements. Southbound vehicles on Mannheim Road desiring to turn left and travel eastbound on Pratt Avenue must do so from the inside through lane of traffic, since there is no center lane to allow for left-turn storage. There is approximately a six-foot wide painted median that separates the opposing through traffic flow that a vehicle can partially queue on; however, it is not wide enough to effectively remove the southbound left-turn vehicle from the southbound through traffic stream.

Under Year 2028 no-build conditions, the westbound approach is projected to continue operating at LOS B during the weekday morning peak hour and at LOS C during the weekday evening peak hour with increases in delay of approximately eight seconds or less over base conditions. The southbound left turn is projected to continue operating at LOS A during the peak hours.

Under Year 2028 total projected conditions, the westbound approach is projected to operate at LOS B during the weekday morning peak hour and at LOS D during the weekday evening peak hour with increases in delay of less than seven seconds over no-build conditions. The southbound left turn is projected to continue operating at LOS A during the peak hours.

Further, field observations during both the weekday morning and weekday evening peak hours show the following:

- Vehicle gaps in through traffic on Mannheim Road were observed throughout each peak hour. This is primarily due to the traffic signals at Lunt Avenue to the north and Higgins Road (IL Route 72) to the south, which effectively platoon the through traffic along Mannheim Road, thereby creating additional gaps in traffic for vehicles to exit from Pratt Avenue onto Mannheim Road.

A southbound left-turn lane on Mannheim Road is not recommended given the existing offset alignment of Pratt Avenue with the Hotel Main Access Drive to the north. Providing a southbound left-turn lane at Pratt Avenue will impact northbound left-turn movements at the Hotel Main Access Drive, which has an offset T-intersection approximately 80 feet to the north of Pratt Avenue.

Maple Street with Pratt Avenue

As noted, Maple Street T-intersects Pratt Avenue from the north, slightly offset to the east of Curtis Street, which T-intersects Pratt Avenue from the south. All four approaches are under stop sign control. Further, the intersection provides high-visibility crosswalks on both the north and west legs of the Maple Street/Pratt Avenue intersection. The capacity analyses show that this offset intersection will continue to operate at LOS A under no-build and total projected conditions during the weekday morning and evening peak hours. Further, the development is projected to generate a low volume of traffic through this intersection and within the neighborhood. As such, this intersection has sufficient reserve capacity to accommodate the projected volumes and no traffic control or roadway improvements are needed or recommended at this intersection.

Mannheim Road with Existing Access Drives

The results of the capacity analysis indicate that the eastbound and westbound approaches from existing access drives along Mannheim Road currently operate at LOS B or better during the weekday morning and evening peak hours. Northbound and southbound left turns into the drives currently operate at LOS A during the peak hours. These access drives currently serve Café La Cave, the Hyatt Hotel parking lot, Royal Touch Car Wash, and the retail strip containing Potbelly and Starbucks.

Under Year 2028 no-build and total projected conditions, the access drives along Mannheim Road are projected to operate at LOS C or better during the weekday morning and weekday evening peak hours. The northbound and southbound left turns into the drives are projected to continue operating at LOS A during the peak hours. As such, no roadway or traffic control improvements to the access drives are needed in conjunction with the proposed development at 2777 Mannheim.

Mannheim Road with Hotel Main Access Drive

The results of the capacity analysis indicate that the eastbound approach currently operates at LOS A during the weekday morning peak hour and LOS B during the weekday evening peak hour. The northbound left-turn movement operates at LOS A during both peak hours.

Under Year 2028 no-build and total projected conditions, the eastbound approach and the northbound left-turn movement are projected to continue operating at the same existing levels of service during both peak hours with increases in delay of less than one second. As such, the traffic estimated to be generated by the proposed development will have a limited impact on the operation of this intersection and no roadway improvements or traffic control adjustments are required.

Pratt Avenue with Proposed East Access Drive

The results of the capacity analysis indicate that the northbound and southbound approaches are projected to operate at LOS B or better during the weekday morning and weekday evening peak hours. The eastbound and westbound left turns are projected to operate at LOS A during both peak hours. As such, it is projected that this access drive will provide efficient and flexible access to the site and no additional recommendations are needed for this intersection.

Pratt Avenue with Proposed West Access Drive

The results of the capacity analysis indicate that the northbound and southbound approaches are projected to operate at LOS B or better during the weekday morning and weekday evening peak hours. The eastbound and westbound left turns are projected to operate at LOS A during both peak hours. As such, it is projected that this access drive will provide efficient and flexible access to the site and no additional recommendations are needed for this intersection.

Mannheim Road with Proposed Access Drive/Hotel Parking South Access Drive

The results of the capacity analysis indicate that the eastbound and westbound approaches are projected to operate at LOS C or better during the weekday morning and evening peak hours. The northbound and southbound left turns are projected to operate at LOS A during both peak hours. As such, it is projected that this access drive will provide efficient and flexible access to the site and no additional recommendations are needed for this intersection.

Circulation and Drive-Through Stacking

Per the site plan of the proposed development, the drive aisles within the parking lots serving the buildings will be two-way drive aisles. This will allow for vehicles to travel in either direction throughout the site. The main drive aisles will parallel Mannheim Road and Pratt Avenue and be lined with parking spaces. Additional north-south drive aisles will be located east of building A and east of Building C, as well as one east-west drive aisle at the rear of the site, north of all three buildings.

The drive-through system for the proposed building A will feature a two-lane configuration. Vehicles will enter the drive-through from the southeast corner of the building. Vehicles will travel around the east and north sides of the building to reach the pay-window/pick-up window located on the north side of the building and exit the drive-through lane from the northwest corner of the building. Based on a review of the site plan, there is adequate space for eighteen vehicles to queue without impacting vehicle circulation through the site.

The drive-through system for the proposed Building B will feature a one-lane configuration. Vehicles will enter the drive-through from the southeast corner of the building. Vehicles will travel around the east and north sides of the building to reach the pay-window/pick-up window located on the north side of the building and exit the drive-through lane from the northwest corner of the building. Based on a review of the site plan, there is adequate space for seven vehicles to queue without impacting vehicle circulation through the site.

The drive-through system for the proposed Raising Cane`s restaurant will feature a two-lane configuration. Vehicles will enter the drive-through from the southeast corner of the building at one of two order lanes. Vehicles will then proceed counterclockwise, merging after ordering, and proceeding single-file to the pay-window/pick-up window, located on the north side of the building. Based on a review of the site plan, there is adequate space for six vehicles to queue at the ordering boards without impacting vehicle circulation through the site, with space for an additional seven vehicles queueing from the pick-up window.

Appropriate wayfinding signs and striping should be provided within the site directing customers to and from the entrance of the pick-up lane. “Do Not Enter” signs should be placed at the exit of the drive-through lanes to deter opposing traffic from entering the drive-through system from the one-way exit direction. Additionally, the exiting movements from the drive-through lanes should be under stop sign control.

5. Conclusion

Based on the preceding analyses and recommendations, the following conclusions have been made:

- The proposed development-generated traffic will be consistent and compatible with traffic patterns and volumes in the area.
- The proposed restaurant land uses typically attract a significant amount of traffic from the existing traffic on the adjacent roadway network en route to another destination. As such, the net new traffic the subject development is proposed to generate will be reduced.
- The proposed access system to serve the development will help disperse the development-generated traffic onto the surrounding roadway network and provide alternatives for the site traffic to enter and exit the development.
- No traffic control or roadway improvements are recommended at the intersections analyzed.
- The two proposed full access drives off Pratt Avenue will be under stop sign control. No improvements on Pratt Avenue are needed or recommended.
- The proposed full access drive off Mannheim Road will provide one inbound lane and one outbound lane under stop sign control.
- The proposed site plan allows for efficient internal circulation and access to the three proposed access drives serving the development.
- Appropriate wayfinding signs and striping should be provided within the site directing customers to and from the entrance of the drive-through lanes.
- “Do Not Enter” signs should be placed at the exits of the drive-through lanes to deter opposing traffic from entering the drive-through lanes from the one-way exit direction.
- Exiting movements from the drive-through lanes should be under stop sign control.

MEMORANDUM

Date: April 21, 2023
To: Planning and Zoning Board (PZB)
From: Samantha Redman, Associate Planner *SR*
Cc: John T. Carlisle, AICP, Director of Community and Economic Development *JTC*
Subject: Conditional Use for Commercially Zoned Assembly Use and Private School at 820-848 Lee Street

Issue: The petitioner is requesting a conditional use permit (i) to operate a Commercially Zoned Assembly Use in the C-5 Zoning District, in accordance with a project narrative that has been revised since initial entitlement (2019); and (ii) to operate a private elementary and high school in the C-5 Zoning District.

PIN: 09-17-425-029-0000, 09-17-425-030-0000, 09-17-452-031-0000, 09-17-425-032-0000, 09-17-425-033-0000

Petitioner: Little Bulgarian School in Chicago, 832 Lee St., Des Plaines, IL, 60016

Owner: Little Bulgarian School in Chicago, 832 Lee St., Des Plaines, IL, 60016

Case Number: #23-013-CU

Ward Number: #2, Alderman Colt Moylan

Existing Zoning: C-5, General Commercial

Surrounding Zoning: North: C-5, Central Business
South: C-5, Central Business
East: R-4, Central Core Residential and C-5, Central Business
West: C-5, Central Business

Surrounding Land Uses: North: Office Building
South: Office Building
East: Townhomes and Religious Use
West: Condominiums

Street Classification: Lee Street is an arterial street.

Comprehensive Plan : The Comprehensive Plan illustrates this site as Institutional.

Property/Zoning History: This site is zoned C-5 and includes five parcels. 832 Lee Street includes the Little Bulgarian School building and the associated parking lot, which was completed in 2021. 842 and 848 Lee Street are two single-family houses that the Little Bulgarian School owns and operates as rentals. In 2019 Ordinance Z-12-19 (see attached) granted the subject property conditional use permits to operate as a commercially zoned assembly use and a private school for high school students and a major variation to the parking requirement. The parking variation allowed for the total required parking to be reduced from 73 to 63 spaces.

After Z-12-19 was approved and signed by the petitioner, the petitioner submitted a business registration application to the City. In accordance with the approved conditions of approval and the signed unconditional agreement of consent, the petitioner is limited to assembly uses related to:

- a.) Community services
- b.) Recreational and social activities
- c.) Private school and adult education lessons
- d.) Office uses directly related to the Little Bulgarian School Organization

The petitioner submitted a business registration application in July 2019 to operate their uses from this location. The City required the petitioner to sign an affidavit restricting uses to those related to the school. However, the Petitioner did not approve of the restrictions in the affidavit and thus refused to sign the document. During this business registration process, the property was inspected several times by the building, zoning and fire departments to determine if the site is compliant with applicable zoning, fire and building codes. The property passed all inspections on April 19, 2023. Because the affidavit was not signed, however, no business registration has been issued for the uses on this property.

The petitioner has been issued several violations for operating in this location without a business registration. The first violation was issued for operating in conflict with their conditional use in December 2021, when it was discovered that large events were held on the site without a business license allowing for the assembly use. A second violation was issued in March 2022 for operating without a business license and hosting events with liquor without proper City approvals. Three administrative hearings were held regarding this case. The City Attorney and staff met with the petitioner in February 2023 to discuss how to proceed.

Project Description: The petitioner has submitted this application to amend the conditional use to allow for assembly uses related and unrelated to the school to be held on their property. The petitioner and property owner, Little Bulgarian School (LBS), is requesting a conditional use for the following:

- 1.) Allow commercially zoned assembly uses at 832 Lee St., open to the public and not restricted to the school activities. These events may include athletic events, performances, fundraisers, cultural events, or other events. The petitioner requests that the school be able to rent or lend facilities to third parties that:

- a. Support and promote the school;
- b. Celebrate, promote, support, and educate about Bulgarian culture, arts, and history;
- c. Support and promote civic education, volunteerism, and community engagement;
- d. Support the activities of community residents and other community, educational, and cultural groups and organizations.

2.) Allow for a private school for students of all ages (elementary, high school, adult classes) to operate seven days a week.

Proposed Uses and Hours of Operation

The petitioner does not have any plans to alter the interior or exterior of any of the properties at 820-848 Lee Street. All proposed uses will be located inside the building.

The below table provides an outline of approximate days and times of programming in the building. As stated in the Petitioner’s Narrative and Response to Standards, the specific programming will vary depending on demand, available resources, seasonality, or other factors. Some activities may occur simultaneously on the site, either when the programming is complimentary (i.e. a preschool recital during the weekday preschool) or when there will not be a conflict with other uses of the facilities. Efforts will be taken by the petitioner to program in a way that does not overburden the facility. A condition of approval states the fire occupancy load cannot be exceeded at any time in the building, requiring the petitioner to ensure any activities are scheduled in a way that does not violate any fire codes.

| Use | Types of Activities | Hours of Operation | Spaces Utilized | Maximum # of Occupants |
|-----------------------------|---|--|---------------------------------------|---|
| Assembly uses ¹ | Athletic events, fundraisers, performances, cultural events, other events relevant to mission of school | Monday through Thursday, 5 p.m. to 10 p.m. Friday & Saturday, 11 a.m. to 3 p.m. or 5 p.m. to 11 p.m., Sunday, 11 a.m. to 3 p.m. or 5 p.m. to 10 p.m. | Gym, library | 254 in the gym (fire occupancy limit for gym, balcony, and stage combined) + 10 in library (for special events) |
| Private School ¹ | Weekend classes | Saturday and Sunday, 9 a.m. to 3 p.m. | Classrooms, craft space, library, gym | 200 students + 15 teachers / staff members |

| | | | | |
|--|--|---|---|---|
| | Weekday Preschool | M-F, 7 a.m. to 4 p.m. | Classrooms, craft space, library, gym | 60 students over three sessions (12- 20 students per session) + 2 staff members |
| | Weekday After School | M-F, 5:30 p.m. to 8 p.m. | Classrooms, craft space, library, gym | 60 students over three sessions (12- 20 students per session), two instructors + 90 adults and two instructors |
| | Weekday school ² | M-F, 7 a.m. to 4 p.m. | Classrooms, craft space, library, gym | 200 students + 15 teachers / staff members |
| Office | Organization related office and meeting activities | As needed, during operating hours of the school | Offices | Varies |
| Other Recreational Use | Gym use by local basketball group | Upon request, subject to availability | Gym | 234 in the gym (fire occupancy limit for gym and balcony combined) |
| Single family residences ³ | Residential rental | N/A | Houses | N/A |
| ¹ Requires conditional use ² Estimate from approximate weekend school enrollment. LBS is exploring this option and has not obtained necessary state licensing yet to operate this type of school. ³ Rental properties unassociated with school and assembly use activities. | | | | |

Drop-Off and Pick-up Operations

Most students participating in classes or programs will likely be dropped off and picked up by parents. The 2019 KLOA Traffic Study provided guidance on how to reduce conflicts on the site and improve traffic flow, stating that pick-ups and drop-offs should not occur in the front of the building along Lee St. Rather, students should be dropped off in the rear of the building, using the alley for access. Any staff members or older students parking at the site should be instructed to enter from the Lee Street entrance to the northmost parking lot.

Off-Street Parking

Pursuant to Section 12-9-7, commercially zoned assembly uses for community centers are required to provide one space for every 200 square feet of gross activity area. The proposed private school would require one space for each classroom, plus one space per 200 square feet of area devoted to offices, plus one space for every six students based on maximum enrollment. The definition of “floor area” in Section 12-13-3 allows certain spaces such as restrooms, mechanical rooms, hallways, and a percentage of storage areas. The table below

reflects the floor area of the building. Note the single-family residences are excluded from this calculation, as they each have their own parking areas that satisfy requirements and will not be using the LBS parking lot.

| Use | Floor Area | Required parking ² |
|---|---|--|
| Assembly uses community centers, banquet halls and membership organizations | 3678.5 square feet ¹ | 19 spaces |
| Private School | 13 classrooms Max enrollment: 200 students Offices: 309 square feet | 13 spaces + 2 spaces + 34 spaces |
| | Total | 67 spaces |

¹ Excludes floor area for mechanical room and a percentage of storage areas

² Spaces rounded up to next whole number

During the previous entitlement process in 2019, it was determined 73 spaces were required to meet the anticipated parking demand. Since 2019, the petitioner has achieved a better understanding of how building spaces will be used, and thus submitted a more detailed floor plan to city staff for review. The updated floor plan (including square footage of storage and mechanical areas) allows a greater portion of the building to be excluded from the parking calculation, and thus reducing the amount of necessary parking from 73 to 67 spaces. The parking variation from the original Z-12-19 that reduced the required parking from 73 to 63 spaces is still valid and applicable. *However, because the new conditional use request envisions more frequent and potentially larger events, and therefore potential peaks in parking demand, the PZB and/or City Council may find parking to be relevant in its consideration.*

In addition to the 63 spaces available for the property, a parking agreement allows the petitioner to use 28 parking spaces at 854 Lee Street (Immanuel Lutheran Church’s west parking lot) during the hours of 6:39PM to 11:30PM Monday through Friday and Saturday from 1:30PM to 11:59PM and Sunday from 1:30PM to 11:30PM (Refer to attachment). The parking agreement is active until August 31, 2025, with terms allowing for renewal after this date. The additional spaces would be able to accommodate any excess parking demand for either the assembly use or school during the noted hours.

Standards for Conditional Use

The following is a discussion of standards for zoning amendments from Section 12-3-4(E) of the Zoning Ordinance. Rationale for how the proposed amendments may or may not satisfy the standards is provided below and in the petitioner’s response to standards. The PZB may use this rationale toward its recommendation, or the Board may make up its own.

1. The proposed Conditional Use is in fact a Conditional Use established within the specific Zoning district involved:

Comment: Commercially zoned assembly use and private schools require a conditional use permit in the C-5 Zoning District.

PZB Additions or Modifications (if necessary): _____
_____.

2. The proposed Conditional Use is in accordance with the objectives of the City’s Comprehensive Plan:

Comment: The 2019 Comprehensive Plan illustrates this area to be used for institutional uses. Institutional uses include the proposed school and community organizations associated with this request.

PZB Additions or Modifications (if necessary): _____
_____.

3. The proposed Conditional Use is designed, constructed, operated and maintained to be harmonious and appropriate in appearance with the existing or intended character of the general vicinity:

Comment: No alterations to the building are proposed with this application, thus there will be no changes to appearance that would affect the character of the neighborhood.

PZB Additions or Modifications (if necessary): _____
_____.

4. The proposed Conditional Use is not hazardous or disturbing to existing neighboring uses:

Comment: All activities will occur inside the existing building and will be minimally disruptive to the neighborhood. Parking will be accommodated by the sixty-three spaces provided on site. A parking variation was granted in the previous conditional use process to allow a reduction from 73 to 63 spaces. Based on the petitioner’s narrative, it does not appear the new activities proposed will be greater in intensity than the previous uses approved by the 2019 ordinance. Staff does not anticipate any concerns with the proposed uses interfering with the parking equilibrium of the neighborhood.

The 2019 traffic study indicates the traffic generated by this use will not substantially be affected by the proposed uses in this area. For the school activities, the most intensive traffic generation will be during pick-up and drop-off. However, the traffic study does not take into account the proposed assembly uses – for which there are at least 14 events listed (see narrative) - but focuses solely on school uses. The petitioner is currently working with a traffic engineer to update the traffic study.

At the time of report writing, adequate information for staff to assess traffic impact based on the combination of uses is not available. A recommended condition of approval states an updated traffic study must be provided and reviewed by city staff prior to the case appearing before City Council, to provide an adequate understanding to decision-makers regarding the impact of this use to the neighborhood. *However, the Board may choose to ask the petitioner to answer questions or present evidence related to traffic before voting on a recommendation to the Council, regardless of the recommended conditions.*

PZB Additions or Modifications (if necessary): _____

5. The proposed Conditional Use is to be served adequately by essential public facilities and services, such as highways, streets, police and fire protection, drainage structures, refuse disposal, water and sewer, and schools; or, agencies responsible for establishing the Conditional Use shall provide adequately any such services:

Comment: The existing building has been adequately served by essential public facilities and services. Staff has no concerns that the proposed use will not be adequately served with essential public facilities and services in the future.

PZB Additions or Modifications (if necessary): _____

6. The proposed Conditional Use does not create excessive additional requirements at public expense for public facilities and services and will not be detrimental to the economic well-being of the entire community:

Comment: The proposed use would neither create a burden on public facilities, nor would it be a detriment to the economic well-being of the community.

PZB Additions or Modifications (if necessary): _____

7. The proposed Conditional Use does not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property, or the general welfare by reason of excessive production of traffic, noise, smoke fumes, glare or odors:

Comment: All activities are proposed to occur inside the building and will not involve any processes or activities that will be disruptive to the neighborhood. Any uses must be in compliance with the Environmental Performance Standards in Chapter 12 of the Zoning Ordinance. Noise level for any activities on the site will be regulated by Section 6-2-7 of the Police Regulations in the City’s municipal code. Refer to Standards 4 and 8 for discussion on traffic impacts.

PZB Additions or Modifications (if necessary): _____
_____.

8. The proposed Conditional Use provides vehicular access to the property designed so that it does not create an interference with traffic on surrounding public thoroughfares:

Comment: Vehicular access will continue to be provided through Lee Street, to the north parking lot, and the alley for pickups and drop-offs of students or parking in the rear of the building, as stated in the attached Petitioner’s Narrative and Responses to Standards. Particularly because the petitioner is seeking an entitlement for up to 200 daytime students, the use of the Lee Street curb may not be sufficient. The site plan does not include a designated off-street pick-up or drop-off area. At this time, adequate information to assess traffic impact based on the combination of uses is not available. The 2019 traffic study indicates the traffic generated by this use will not substantially be affected by this use in this area. However, the traffic study does not take into account the proposed assembly uses, but focuses solely on school uses. Staff comments on this standard are consistent with Standard No. 4.

PZB Additions or Modifications (if necessary): _____
_____.

9. The proposed Conditional Use does not result in the destruction, loss, or damage of natural, scenic, or historic features of major importance:

Comment: The subject property is within an existing building and thus would not result in the loss or damage of natural, scenic, or historic features. No new development is proposed for this site.

PZB Additions or Modifications (if necessary): _____
_____.

10. The proposed Conditional Use complies with all additional regulations in the Zoning Ordinance specific to the Conditional Use requested:

Comment: The proposed uses comply with all applicable requirements as stated in the Zoning Ordinance.

PZB Additions or Modifications (if necessary): _____
_____.

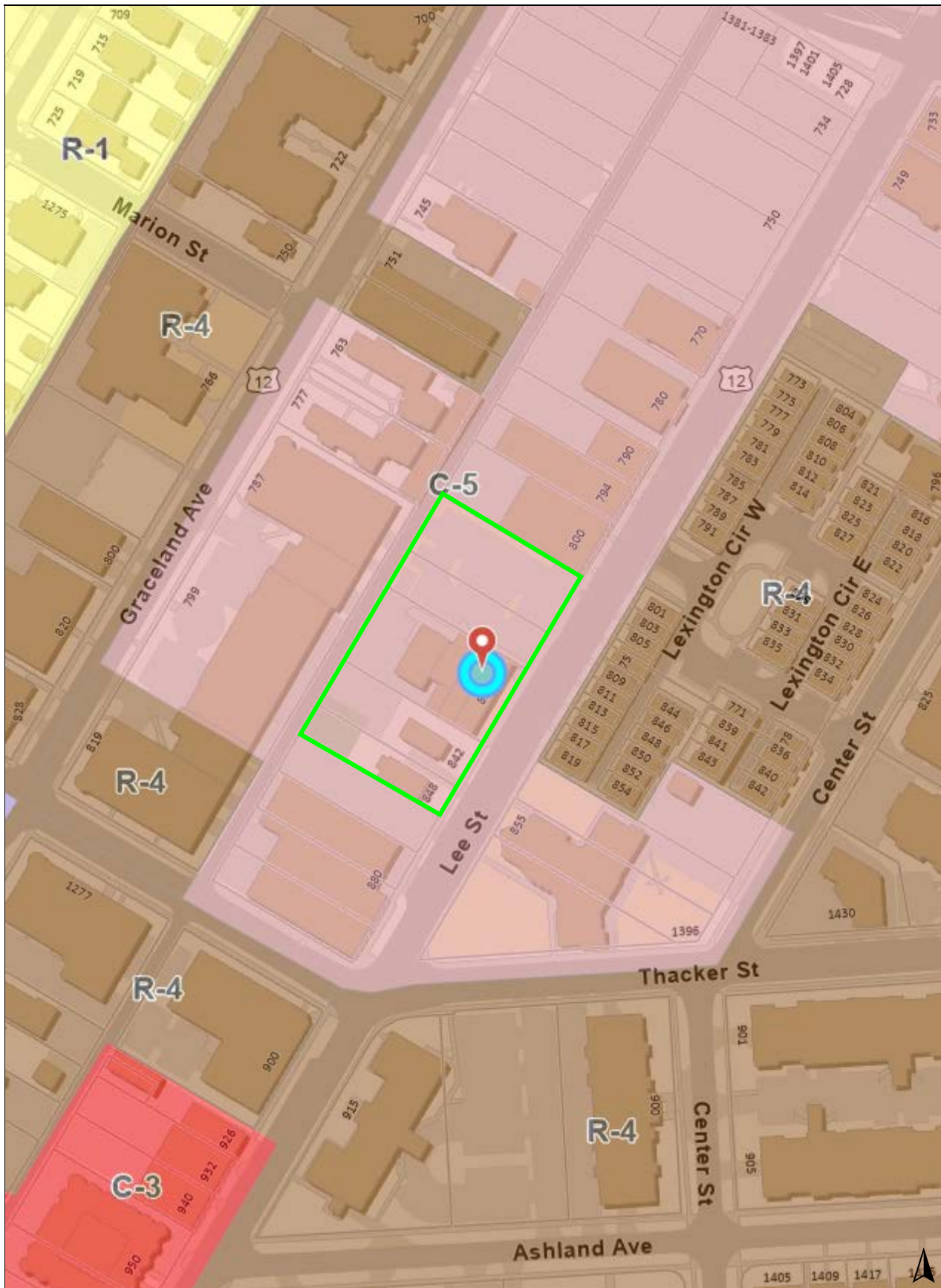
PZB Procedure and Recommended Conditions: Pursuant to Sections 12-3-4(E) of the Zoning Ordinance, the PZB may vote to *recommend* approval, approval with modifications, or disapproval of the conditional use. The City Council has final authority over both requests. However, should the PZB recommend approval of the conditional use, staff suggests the following conditions for the conditional use request.

Conditions of Approval:

1. The operation of the commercially zoned assembly and private school uses shall be located only within the School Building at 832 Lee Street. The Single-Family Homes shall not be used for commercially zoned assembly or private school uses.
2. Any expansion for any use shall require the Petitioner to obtain an amendment to the Conditional Use Permits.
3. The Subject Property shall only be used as a commercially zoned assembly use for uses related to the school or open to the public that meet any of the following goals of the School:
 - a. Support and promote the School;
 - b. Celebrate, promote, support, and educate about Bulgarian culture, arts, and history;
 - c. Support and promote civic education, volunteerism, and community engagement;
 - d. Support the activities of community residents and other community, educational, and cultural groups and organizations.
4. The maximum number of people in any space shall not exceed the maximum occupancy load prescribed by the Fire Department. Every room or space that is an assembly occupancy shall have the occupant load of that room or space posted in a conspicuous place, near the main exit.
5. No alcohol shall be served during any event unless approved by the City of Des Plaines, as required by the Fire Department. Any food service preparation for any member shall come from a commercial grade kitchen.
6. The Petitioner shall maintain the Parking Lease Agreement as long as the Subject Property is used for commercially zoned assembly use and a private school. Any amendment to the Parking Lease Agreement shall be approved by the Director of Community and Economic Development.
7. An updated traffic study addendum detailing the traffic impacts of the proposed assembly uses must be submitted for review by city staff prior to the case appearing before City Council.

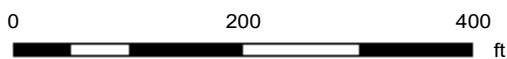
Attachments:

- Attachment 1: Location Map
- Attachment 2: Site and Context Photos
- Attachment 3: Project Narrative and Responses to Standards
- Attachment 4: 2019 Traffic Study Prepared by KLOA
- Attachment 5: Parking Agreement for 854 Lee Street
- Attachment 6: Site Plan
- Attachment 7: Floor Plan
- Attachment 8: Original 2019 Ordinance – Z-12-19



Legend

- Subject Site
- Zoning
- C-3: General Comm
- C-5: Central Busines
- M-2: General Manufacturing
- R-1: Single Family Residential
- R-4: Central Core Residential



Print Date: 3/31/2023

Notes

Disclaimer: The GIS Consortium and MGP Inc. are not liable for any use, misuse, modification or disclosure of any map provided under applicable law. This map is for general information purposes only. Although the information is believed to be generally accurate, errors may exist and the user should independently confirm for accuracy. The map does not constitute a regulatory determination and is not a base for engineering design. A Registered Land Surveyor should be consulted to determine precise location boundaries on the ground.



820-848 Lee St – Public Notice Sign



View of 832 Lee St - Little Bulgarian Center Building and parking lot



View of rear of building where pick-up/drop-off will occur. Photo taken from alley facing southeast.



854 Lee Parking Lot - Vehicles and storage related to construction activities at 880 Lee St, anticipated to end by May 2023

Little Bulgarian School **Project Narrative**

April 2023

I. Executive Summary

Little Bulgarian School in Chicago (“**LBS**”) is the owner of the property generally located at 820-840 Lee Street in Des Plaines, Illinois (“**Subject Property**”). The Subject Property is improved with an existing school building (the “**School Building**”), an accessory surface parking lot, and two single-family residences.

LBS operates a cultural center in the School Building at the Subject Property known as the Little Bulgarian School (the “**School**”). The School is intended to (a) provide weekend classes and school activities to children of Bulgarian descent; (b) celebrate, promote, support, and educate about Bulgarian culture, arts, and history; (c) support and promote civic education, volunteerism, and community engagement; and (d) support the activities of other community, educational, and cultural groups and organizations.

LBS uses the School Building for “private school” and “commercially zoned assembly” uses, as those uses are defined in the Zoning Code of Des Plaines (“**Zoning Code**”) pursuant to Conditional Use Permits granted by the City of Des Plaines under Ordinance Z-12-19 (collectively, the “**CUPs**”). This Project Narrative sets forth details of the School’s/LBS’s private school and assembly operations under the CUPs.

II. Organizational History

LBS began operation in the school year 2005/2006 with total of 6 students. The organization was created mainly for Bulgarian children who wish to learn the Bulgarian language, history and culture. In 2010, LBS was recognized as a 501 (c) (3) non-profit organization by the US authorities allowing it grow even faster. As of 2018, LBS provides education services to more than 600 children in 2 facilities in Elk Grove Village and 1 in Mount Prospect. LBS is officially registered with the Bulgarian Ministry of Education and is entitled to issue certificates of graduation from grade 1st to 12th.

III. Intended Uses and Programming

Without limitation by other sections of this project narrative, the School intends to engage in the following types of uses at the Subject Property:

- 1) Classes and other school activities administered by the School.
- 2) Rehearsals, recitals, performances, and recreational and athletic activities administered by the School and involving its students, faculty, families, and/or alumni.
- 3) School fundraisers and Bulgarian cultural events open to people other than students, faculty, families, and alumni of the School, including events which involve the sale of tickets, service of catered food and alcoholic beverages (subject to obtaining a liquor license), playing of recorded music, and/or live performances by Bulgarian performers (including performers who are not students, faculty, families, or alumni of the School).
- 4) Traditional Bulgarian craft shows open to people other than students, faculty, families, and alumni of the School.
- 5) Historical, holiday, community, civic, and educational events and activities, such as election day events and guest speakers from Bulgaria, that are open to people other than students, faculty, families, and alumni of the School.
- 6) Recreational and athletic activities that are open to people other than students, faculty, families, and alumni of the School, such as pickup basketball games and tournaments.
- 7) Use of School classrooms, athletic facilities, and assembly spaces by third-party community, educational, and cultural groups and organizations, such as musical, dance, and theater programs, other local schools, and community residents and organizations, for classes, lessons, rehearsals, performances, recitals, and recreational and athletic activities.

*The School may rent or lend its facilities and assembly spaces to third parties when consistent with one or more of the above listed uses. The School does not intend to rent out its facilities and assembly spaces to third parties for any special events, performances, or shows that do not either: (a) support and promote the School; (b) celebrate, promote, support, and educate about Bulgarian culture, arts, and history; (c) support and promote civic education, volunteerism, and community engagement; or (d) support the activities of community residents and other community, educational, and cultural groups and organizations, such as musical, dance, and theater programs, other local schools, and community residents and organizations.

Programming Details

Without limitation of the Intended Uses described above, below are categories and descriptions of programming LBS intends to offer.

The below table summarizes the approximate days and times during which each programming category may occur and the primary areas of the School Building that each category may generally utilize. The table below is not a schedule of programming. On many days certain categories of programming may not occur or, if they occur, may occur during only a subset of the hours indicated in the table. The specific schedule of programming offered by LBS at any time will depend upon demand, available resources, seasonality, and other factors. On many days and during many times, numerous spaces within the School Building may not be utilized.

| Programming Category | Spaces Utilized | Days / Times |
|---|---------------------------------------|---|
| Weekend Classes | Classrooms, craft space, library, gym | Sat-Sun / 9am-1pm or 11am-3pm |
| Weekday Preschool | Classrooms, craft space, library, gym | Mon-Fri / 7am -4pm |
| Weekday After School Classes | Classrooms, craft space, library, gym | Mon-Fri / 5:30pm-8pm |
| Weekday Full-Time School (potential future use) | Classrooms, craft space, library, gym | Mon-Fri / 7am -4pm |
| Special Events (limited frequency) | Gym, library | Mon – Thur / 5pm – 10pm Fri - Sat / 11am – 3pm or 5pm – 11pm Sun / 11am – 3pm or 5pm – 10pm |
| Recreational/gym use by local basketball group | Gym | Per request, subject to availability. |

Each programming category will involve incidental use of the facility’s parking areas, and may involve incidental use of certain utility, locker/storage, common space, office, and administrative areas.

Programs will occasionally occur at the Subject Property simultaneously when programs are complimentary (e.g., a preschool recital during the weekday preschool program) or will not overburden the facility (e.g., an event in the gym at the same time as a small class in the library). Programming will generally be scheduled and sequenced to minimize the overlap of peak drop-off and pick-up activities for different programs.

A. Weekend Classes

LBS plans to offer weekend classes for up to 200 kids, depending on demand (currently, approximately 150 children attend weekend classes). On the weekend the classes will have up to

2 shifts – 9 am to 1 pm and 11 am to 3 pm, depending on demand and the parents' preference. For the weekend classes LBS plans to have up to 15 teachers / staff members on site, depending on the actual number of classes and students. Students will be dropped off and picked up by their parents at the back of the building as per the recommendations of the traffic study.

B. Weekday Preschool and After School/Evening Classes

In addition, in order to financially support the facility, LBS offers a weekday preschool program for younger children, after school programs, and weekday evening classes for adults.

The after school programs include math and science tutoring, folklore dance classes, and other child centered class activities. These activities are composed of 12-20 children per class session, with one instructor and the possibility of one helper. The after school classes range from 5:30PM to 8:00PM on weekdays, with up to approximately 2 to 3 classes offered at any given time, depending on demand. Students will be dropped off by their parents in the back of the building and picked up in the same manner.

For adults LBS offers weekday evening classes for citizenship and ESL, as well as folklore dances. Each class may have up to 2 instructors and approximately 15-30 adults attending, with up to approximately 3 classes offered at any given time, depending on demand.

C. Weekday School

In the future, LBS plans to offer a full-time weekday-based school program in the School Building. LBS would obtain any and all additional governmental licenses and approvals necessary in order to establish a full-time school.

D. Special Events

LBS plans to organize small gatherings / concerts / events to commemorate the holidays, normally with performances by the children from the School. At these events LBS estimates about 100 families attending. The events will be happening in the gym and may include, without limitation, events around the following dates/holidays:

1. First day of school – approximately 1st week of September
2. Bulgarian Unification Day – September 6th
3. Bulgarian Independence Day - September 22nd
4. Halloween
5. Day of the Bulgarian Revival Leaders - November 1st
6. Thanksgiving
7. Christmas
8. New Year's Eve

9. National Day (Day of Liberation) - March 3rd
10. International Women's Day - March 8th
11. Easter (Catholic & Orthodox)
12. St George's Day (Day of Bulgarian Army) – May 6th
13. St Cyril & Methodius Day (Day of Culture & Literacy) - May 24
14. International Children's Day – June 1st

E. Use of Gym

Currently the gym has been used by a local basketball team for many years. LBS will continue that relationship.

F. Office Space

Currently, LBS has no central location/office and this is preventing parents, teachers, foreign dignitaries, community leaders, and art performers to gather easily and exchange information and knowledge. LBS is relying on the Elk Grove Library for meetings, Elk Grove High School, Christus Victor Lutheran Church and Christian Life College classes, as well as the private residential homes of many of our parents and board members. LBS is committed of finding a permanent home to provide even better service to its students, faculty and the local community. LBS may use office, classroom, and meeting space at the Subject Property for these purposes.

G. Single Family Residences

LBS rents the two single-family residences at the Subject Property for residential use and has no plans to use them for any other uses.

The main development objectives of the facility are:

- Establish a main hub for all current activities offered by LBS in the Mount Prospect facility including Bulgarian weekend classes and all other after school activities.
- Positively influence the Des Plaines and nearby suburbs Slavic community, consisting of Macedonian, Serbian, Polish, Russian, Ukrainian and many other nationalities, by offering adults classes and activities.
- Community outreach center for all Slavic and other nationalities in the Des Plaines and nearby suburbs.
- Open 2 new FTE positions to manage the building in Des Plaines, attract new residents.
- Maintains high level of integrity and full transparency to the community.

Additional programming at the Subject Property may include the following services and programs:

- Children's Library and Book Share center.
- Food Pantry center.

- General Clothing provision program.
- Citizenship classes.
- Adult weekday evening language classes.
- Folklore and dance classes for kids and adults.
- Math and science tutoring.
- Pre- and afterschool programs.

IV. Parking and Pick-Up/Drop-Off Activities

A. Off-Street Parking

Pursuant to Ordinance Z-12-19, the City Council previously: (1) determined that 73 off-street parking spaces must be provided at the Subject Property under Section 12-9-7 of the Zoning Ordinance; and (2) approved a variation reducing the off-street parking requirement from 73 spaces to 63 spaces (the "**Variation**"). To calculate the 73 off-street parking spaces required by Section 12-9-7 of the Zoning Ordinance, the City applied the parking standards for a "commercially zoned assembly use – banquet hall" and a "high school," which are set forth in the table below. Importantly, the City applied the parking standard for a high school, which is more demanding than the standard for other types of schools, even though LBS does not currently operate any type of a full-time weekday school on the Subject Property. Fewer spaces would have been required under Section 12-9-7 of the Zoning Ordinance if the City had applied the parking standards for other types of schools. In any event, the Variation reduced the applicable requirement from 73 spaces to 63 spaces.

The City Council's previous parking analysis under Section 12-9-7 and Ordinance Z-12-19 remains valid. LBS continues to propose a mixture of assembly uses and school uses on the Subject Property. Pursuant to Section 12-9-7, the same parking standards apply and the required number off-street parking spaces, as varied by Ordinance Z-12-19, should not change. The accessory parking lot on the Subject Property currently has 63 parking spaces, which satisfies the requirements of Ordinance Z-12-19.

| Zoning Code Parking Category | Zoning Code Parking Standard | Calculation (Application to LBS) |
|---|--|---|
| Commercially Zoned Assembly Use – Banquet Halls | 1 space for every 200 SF of gross activity area | 5,185 SF of gross activity area [x] 1 space per 200 SF = 25.9 spaces |
| High School | 1 space for each classroom | 13 classrooms [x] 1 space per classroom = 13 spaces |

| | | |
|-------------|--|--|
| High School | 1 space per 200 SF of office space | 309 SF of office space [x] 1 space per 200 SF = 1.5 spaces |
| High School | 1 space per 6 students based on maximum enrollment | 200 students [x] 1 space per 6 students = 33.3 spaces |

B. Pick-Up and Drop-Off

Pursuant to the Des Plaines Zoning Map, Section 12-13-3 of the Zoning Code, and Ordinance Z-12-19, the School is located in the C-5 Central Business District and its uses are categorized as “private school” and “commercially-zoned assembly.” Pursuant to Sections 12-9-7 and 12-9-9 of the Zoning Code, dedicated loading or pick-up/drop-off spaces are not required for a private school or commercially-zoned assembly use in the C-5 District. LBS has and will conduct pick-up, drop-off, and loading activities at the rear of the building adjacent to the parking lot in order to prevent any such activities from negatively impacting traffic on surrounding rights-of-way.

C. Parking Lot License Agreement and Activity at Immanuel Lutheran Church

Pursuant to the Parking Lot License Agreement between the Little Bulgarian School (“LBS”) and Immanuel Lutheran Church (“Parking Agreement”), LBS has the exclusive right to use the parking lot located at 854 Lee Street (the “Parking Lot”) at the following times: Mondays-Fridays from 6:39 p.m. until 11:30 p.m., Saturday from 1:30 p.m. until 11:59 p.m., and Sundays from 1:30 p.m. until 11:30 p.m. LBS and Immanuel Lutheran have extended the term of the Parking Agreement through at least August 31, 2025. The Parking Lot contains 28 parking spaces.

Pedestrians can safely travel between the LBS building and the Parking Lot by using the existing sidewalk, without crossing any public rights-of-way. The Parking Lot is located on the same side of Lee Street as the LBS building.

The most intensive activities at Immanuel Lutheran Church—it’s worship services—generally take place on Sunday mornings. Even during those hours, many visitors to Immanuel Lutheran Church use the parking lot at the rear of the church building (855 Lee Street), rather than the Parking Lot.

By contrast, Sunday mornings are one of the least active periods for the LBS. Certain relatively small classes may take place at the School on Sunday mornings, but those classes will not create material parking demand. The programs generating the greatest parking demand on the Subject Property will primarily take place during weekdays and evenings. LBS has found that the 63 spaces within the accessory parking lot on the Subject Property is sufficient to accommodate parking demand for the vast majority of its programs. LBS does not anticipate that to change. Infrequently, the 28 additional spaces in the Parking Lot may be used to accommodate greater parking demand related to a special event held in the late afternoon or evening.

V. Mission:

LBS is an educational and cultural center, the preferred choice in learning Bulgarian language, tradition and history, while also building children's tolerance and compassion for other cultures and languages. LBS also provides quality afterschool services for working families of all socio-economic levels in a nurturing environment to the local community.

VI. Vision:

LBS will strive to remain the best educational center to preserve the Bulgarian national and spiritual identity for future generations by inspiring passion for learning the Bulgarian language and keeping Bulgarian tradition alive.

Little Bulgarian School history of unique children teaching technics, via games, songs, drama and priority given to conversational speech, has proven to be the right growth strategy. The training relies exclusively on the most modern methods of language teaching, where students experience the joy of learning. The organization also performs social functions as it is often used as meeting place and community resource for the children as well as parents.

VII. Management Team:

Little Bulgarian School in Chicago is registered as a non-profit organization under state and federal authorities and it is managed by 5 board members with combined professional experience in the finance, legal and most importantly educational area of more than 135 combined years.

VIII. Market Position and Future Growth:

According to the 2000 Census there were 63K Bulgarians living in the State of Illinois and in the 2010 Census this number increase to 105K, or 70% higher in just 10 years. It is currently estimated that more than 150K Bulgarians permanently live in State of Illinois, with very high concentration in the Chicago Northwest suburbs like Des Plaines, many of which now started a family.

Based on the Bulgarian Ministry of Education for the 2018/2019 school year, there are total of 12 registered Bulgarian schools in Chicagoland with total of 1142 children enrolled. Little Bulgarian school represented total of 597 children, or a market share of 52%.

Given the current trend, it is expected that more and more young Bulgarians will continue to relocate to the Northwest suburbs from other states or Bulgaria, as Chicago metro is now the biggest Bulgarian community establishment in North America. Bulgarian families with young children will prefer to move in specifically to Des Plaines as there are many established Bulgarian daycares, restaurants, for example Balkanika, Mehanata, and Nick's Grill, two Bulgarian churches (St. Sophia Bulgarian Orthodox Church, New Life Evangelical Bulgarian Church), as well as many

ethnic grocery stores for example Malincho and Serdika. In addition, there are many organized year-round Bulgarian picnics, concerts and social events. All this shows that the need for Bulgarian schools in the area will continue to grow significantly, as well as the need for social-sport events and adult classes.

Little Bulgarian School is well positioned for strong growth in the future, and a permanent home and cultural hub will further fuel this growth to provide outstanding services to the local community, while also help families and children transition into the American society. Little Bulgarian School has proven thru out the years that it has positive cultural and development impact to the community and it is expected to do so even more in the future.

Conditional Use Permit Approval Standards

Applicant: Little Bulgarian School in Chicago

Subject Property: 820-852 Lee Street, Des Plaines, IL

Application: Amendment to Conditional Use Permits for Commercially Zoned Assembly Use and Private School

Per Section 12-3-4.E of the Des Plaines Zoning Ordinance

1. Standard: The proposed conditional use is in fact a conditional use established within the specific zoning district involved.

Response: Commercially Zoned Assembly Use and Private School are designated as Conditional Uses in the C-5 Central Business District (“**C-5 District**”) pursuant to Section 17-7-3(K) of the Des Plaines Zoning Ordinance.

The School operates commercially-zoned assembly and private school uses in accordance with the two Conditional Use Permits (collectively, the “**CUPs**”) granted pursuant to Des Plaines Ordinance Z-12-19. The proposed amendment to the CUPs (“**Proposed Amendment**”) will update and clarify the scope of the School’s assembly and school activities at the Subject Property.

2. Standard: The proposed conditional use is in accordance with the objectives of the city's comprehensive plan and this title.

Response: The Future Land Use Map included as Figure 2.1 of the Des Plaines Comprehensive Plan designates the Subject Property as “Institutional.” Institutional uses are defined in Chapter 2 of the Comprehensive Plan to include uses “that provide services to Des Plaines residents and the surrounding area...including schools, libraries, [and] community organizations.” The Applicant currently operates a cultural center within the school building at the Subject Property (“**School Building**”) known as the Little Bulgarian School (the “**School**”). The School provides educational, cultural, and recreational programs that promote Bulgarian heritage, enhance the diversity and character of the area, and serve the people in its community. These activities are consistent with the "Institutional" designation of the Subject Property under the Comprehensive Plan.

The School's activities are also consistent with purpose of the City's Zoning Ordinance. The School is located in the C-5 District which, pursuant to Section 2-7-3 of the Zoning Ordinance, is “intended to permit a broad range of commercial development necessary to serve the citizens of Des Plaines and neighboring areas.” The School provides programs and services that are intended to benefit residents of Des Plaines and neighboring areas.

3. Standard: The proposed conditional use is designed, constructed, operated, and maintained so as to be harmonious and appropriate in appearance with the existing or intended character of the general vicinity.

Response: The School operates within and maintains the existing historic School Building , which contributes positively to the physical/design characteristics of the area. No physical change to the School Building is proposed.

The School operates educational, cultural, and community enrichment programs in a manner similar to the operations of the Immanuel Lutheran School that operated at the site prior to the establishment of the School.

Furthermore, the operation of the School is harmonious with other uses in the surrounding area, which includes a mix of office, religious/institutional, and multi-unit and residential uses. Specifically, the School’s assembly operations are similar to those of the Immanuel Lutheran Church, which operates a religious assembly directly across the street from the Subject Property.

4. Standard: The proposed conditional use is not hazardous or disturbing to existing neighboring uses.

Response: The School’s conditional uses are not hazardous or disturbing to, but rather, are consistent and compatible with existing neighboring uses.

The School is located along the active commercial Lee Street corridor and less than a mile from downtown Des Plaines. The School is immediately surrounded by a mix of office, religious/institutional, commercial, and multi-unit residential uses which are similar in intensity and impact. Specifically, the School’s assembly and school programs produce activity similar in intensity and schedule to the programming offered at neighboring Plato Academy, GL Hills Funeral Home, and Immanuel Lutheran Church.

The Subject Property includes 63 on-site accessory parking spaces. The School also has access to 28 supplemental off-street parking spaces at 854 Lee Street pursuant to a Parking License Agreement with Immanuel Lutheran Church (“*Parking Agreement*”), for a total aggregate amount of 91 parking spaces.

Pursuant to Ordinance Z-12-19, the City Council previously: (a) determined that 73 off-street parking spaces must be provided at the Subject Property under Section 12-9-7 of the Zoning Ordinance; and (b) approved a variation reducing the off-street parking requirement from 73 spaces to 63 spaces (the "**Variation**"). To calculate the 73 off-street parking spaces required by Section 12-9-7 of the Zoning Ordinance, the City applied the parking standards for a "commercially zoned assembly use – banquet hall" and a "high school," which are set forth in the table below. Importantly, the City applied the parking standard for a high school, which is more demanding than the standard for other types of schools, even though LBS does not currently operate any type of a full-time weekday school on the Subject Property. Fewer spaces would have been required under Section 12-9-7 of the Zoning Ordinance if the City had applied the parking standards for other types of schools. In any event, the Variation reduced the applicable requirement from 73 spaces to 63 spaces.

While the Proposed Amendment updates and clarifies the scope of the School’s assembly and school uses, the City Council's previous parking analysis under Section 12-9-7 and Ordinance Z-12-19 remains valid. LBS continues to propose a mixture of assembly uses and school uses on the Subject Property. Pursuant to Section 12-9-7, the same parking standards apply and the required number off-street parking spaces, as varied by Ordinance Z-12-19, should not change. The accessory parking lot on the Subject Property currently has 63 parking spaces, which satisfies the requirements of Ordinance Z-12-19.

Functionally, the 91 spaces available to the School have proved more than sufficient to satisfy the School's parking needs throughout its time operating the Subject Property. LBS does not anticipate that to change.

| Zoning Code Parking Category | Zoning Code Parking Standard | Calculation (Application to LBS) |
|---|--|---|
| Commercially Zoned Assembly Use – Banquet Halls | 1 space for every 200 SF of gross activity area | 5,185 SF of gross activity area [x] 1 space per 200 SF = 25.9 spaces |
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| High School | 1 space per 6 students based on maximum enrollment | 200 students [x] 1 space per 6 students = 33.3 spaces |

- 5. Standard: The proposed conditional use is to be served adequately by essential public facilities and services such as highways, streets, police and fire protection, drainage structures, refuse disposal, water and sewer, and schools; or the persons or agencies responsible for the establishment of the proposed conditional use shall provide adequately any such services.**

Response: In connection with the City's prior review and approval of the CUPs, the City’s Public Works, Engineering, Fire Prevention, Building, and Zoning Departments previously determined that the School Building is adequately served by essential public facilities . The Proposed Amendment will not materially change demand at the Subject Property for any public facilities or services.

- 6. Standard: The proposed conditional use does not create excessive additional requirements at public expense for public facilities and services and not be detrimental to the economic welfare of the community.**

Response: The Proposed Amendment does not involve new construction or improvements or material expansion of the activities taking place on the Subject Property. The Proposed Amendment therefore will not materially change demand at the Subject Property for any public facilities or services, materially increase any public expense for public facilities and services, or impose any burdens on the City that would be detrimental to the economic welfare of the community.

- 7. Standard: The proposed conditional use does not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property, or the general welfare by reason of excessive production of traffic, noise, smoke, fumes, glare or odors.**

Response: The Proposed Amendment does not involve any construction or renovation activities at the Subject Property or any other activities that would involve harmful noises, materials, equipment, smoke, fumes, glare, or odors.

All of the proposed assembly activities will continue to take place inside of the School Building and all will continue to be managed by the School's professional staff in a manner designed to prevent excessive noise or traffic. The School's current and proposed activities are disbursed throughout the week and the vast majority of its most active programs occur during non-peak hours (weekend days and week nights). The School's programming will generally be scheduled and sequenced to minimize the overlap of peak drop-off and pick-up activities for different programs.

Traffic impacts are mitigated both by the Subject Property's ample parking and its access to transit. The Subject Property includes 63 off-street parking spaces which are further supplemented by 28 off-site spaces under the Parking Agreement (91 spaces in total). While the Proposed Amendment updates and clarifies the scope of the School's assembly and school uses, the updated and clarified uses do not alter any of the factors that determine the School's parking requirement under Section 12-9-7 of the Zoning Code and Ordinance Z-12-19. The 91 spaces have been sufficient to satisfy the School's parking needs throughout its time operating the Subject Property. The School does not anticipate this will change.

In addition, the Subject Property is located approximately 200 feet from a Pace bus station serving the 226, 230, and 250 Pace bus routes and approximately 1,500 feet from the Des Plaines Metra station. These alternative forms of transit have and will continue to mitigate any parking or traffic impacts associated with the School.

8. Standard: The proposed conditional use provides vehicular access to the property designed that does not create an interference with traffic on surrounding public thoroughfares.

Response: The Subject Property includes ample vehicular access for the School and will not cause interference with traffic on surrounding public thoroughfares. In connection with the original approval of the CUPs, the applicant constructed the parking, access, and related improvements depicted on the engineering plans previously approved by the City and re-submitted as part of this application. Those improvements are designed to provide vehicular access without causing traffic interference. The Subject Property includes approximately eight separate vehicular access points to the School's off-street parking facilities, including entrances from both Lee Street and the public alley. The School maintains its off-site Parking Agreement in order to continue to provide ample site access and parking.

9. Standard: The proposed conditional use does not result in the destruction, loss, or damage of a natural, scenic, or historic feature of major importance.

Response: The Proposed Amendment does not involve any redevelopment activities of any kind and, therefore, does not risk the destruction, loss, or damage of any natural, scenic, or historic feature. Rather, the Proposed Amendment supports the preservation of historic features by enabling the Applicant to continue acting as a steward of the historic School Building.

10. Standard: The proposed conditional use complies with all additional regulations in this title specific to the conditional use requested.

Response: The Proposed Amendment complies with all regulations in the Zoning Ordinance related to the conditional uses and the variation granted in connection with the City's original approval of the CUPs.

Traffic Impact Study

Proposed Little Bulgarian School

Des Plaines, Illinois



Prepared For:
Little Bulgarian School



April 18, 2019

1. Introduction

This report summarizes the methodologies, results, and findings of a traffic impact study conducted by Kenig, Lindgren, O'Hara, Aboona, Inc. (KLOA, Inc.) for the proposed Little Bulgarian School to be located in Des Plaines, Illinois. The site, which currently contains the vacant Immanuel Lutheran School, is located on the west side of Mannheim Road/Lee Street approximately 350 feet north of Thacker Street. As proposed, the existing building will be occupied by a Little Bulgarian School which will serve the following primary purposes:

- Weekday after school and evening programs for up to 60 students
- Weekday evening dance, language, and citizenship classes for up to 60 adults
- Saturday morning language and culture classes for up to 160 students
- Sunday morning language and culture classes for up to 80 students

As part of the development, the existing vacant space adjacent to the parcel will be developed with a 47-space parking lot that will supplement the existing parking north of the building, which will be modified to provide 16 parking spaces. Access to the development will be provided via a proposed access on Lee Street and via the north-south alley bordering the site.

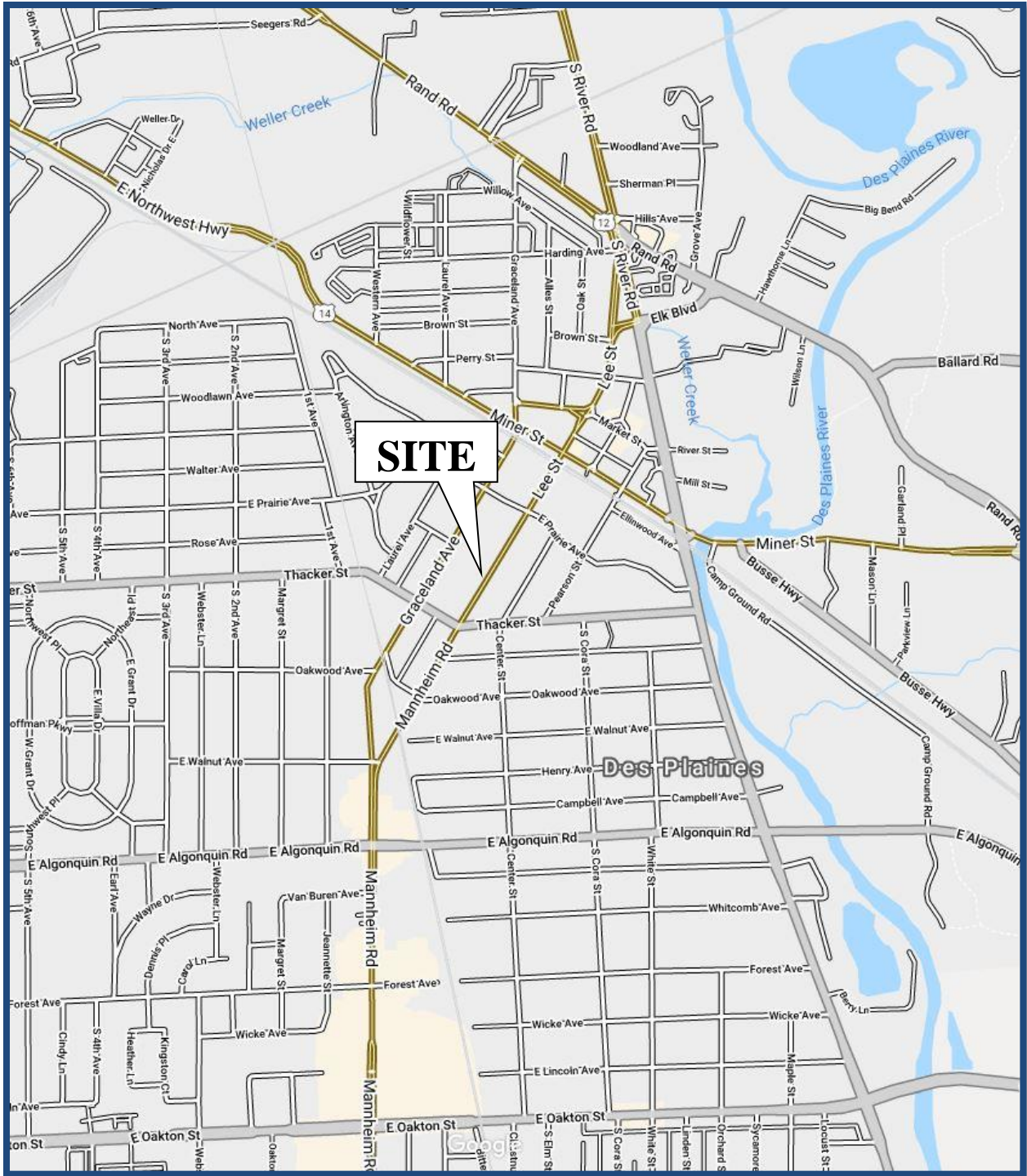
The purpose of this study was to examine background traffic conditions, assess the impact that the proposed school will have on traffic conditions in the area, determine if any roadway or access improvements are necessary to accommodate traffic generated by the proposed school, and evaluate the adequacy of the proposed parking supply. **Figure 1** shows the location of the site in relation to the area roadway system. **Figure 2** shows an aerial view of the site.

The sections of this report present the following:

- Existing roadway conditions
- A description of the proposed school
- Directional distribution of the school traffic
- Vehicle trip generation for the school
- Future traffic conditions including access to the school
- Traffic analyses for the weekday morning, weekday evening, and Saturday midday peak hours
- Recommendations with respect to adequacy of the site access and adjacent roadway system

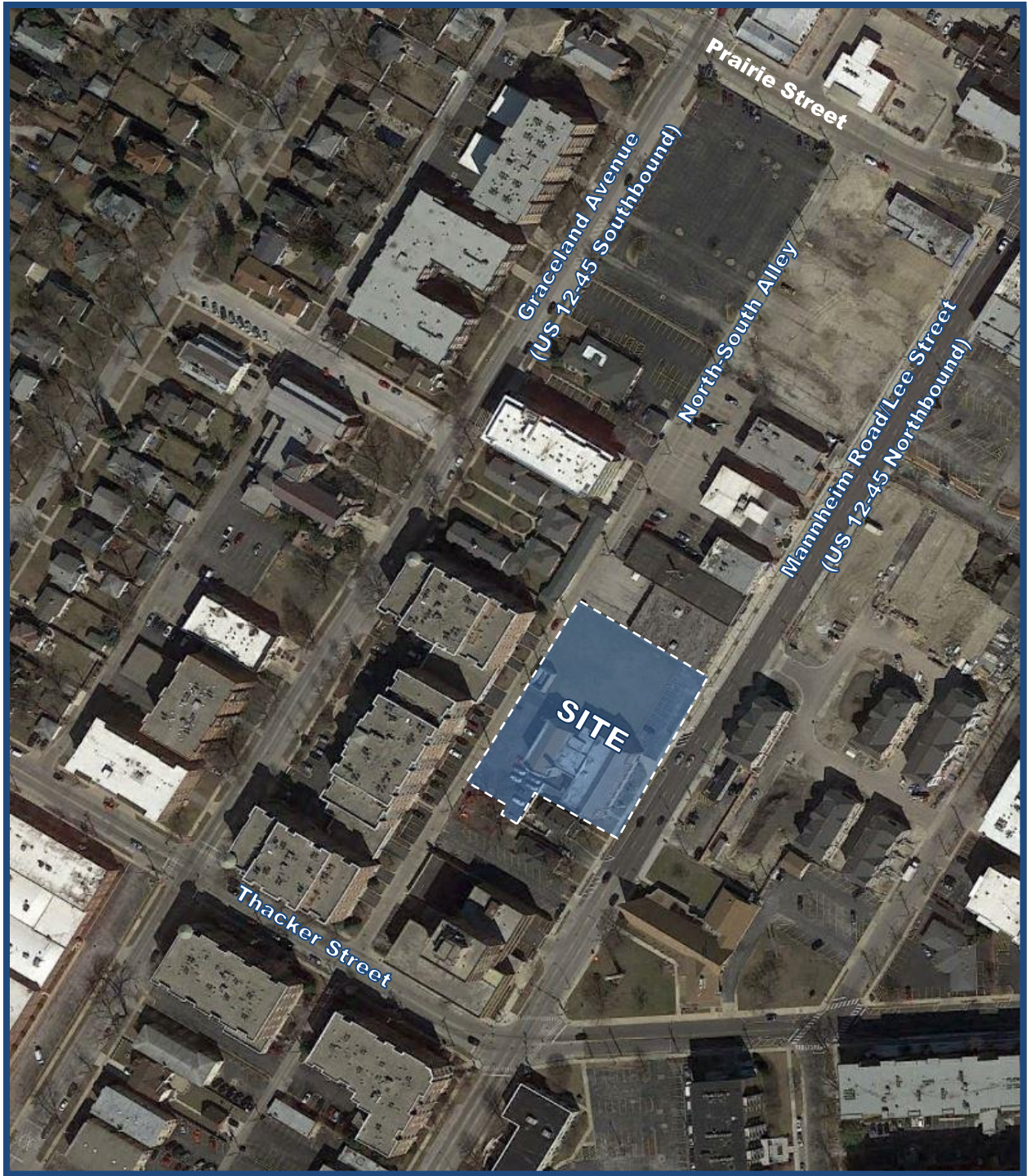
Traffic capacity analyses were conducted for the weekday morning, weekday evening, and Saturday midday peak hours for the following conditions:

1. Existing Conditions - Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes in the surrounding area.
2. Projected Conditions – Analyzes the capacity of the future roadway system using the projected traffic volumes that include the existing traffic volumes, ambient area growth not attributable to any particular development, and the traffic estimated to be generated by the proposed school.



Site Location

Figure 1



Aerial View of Site

Figure 2

2. Existing Conditions

Existing transportation conditions in the vicinity of the site were documented based on field visits conducted by KLOA, Inc. in order to obtain a database for projecting future conditions. The following provides a description of the geographical location of the site, physical characteristics of the area roadway system including lane usage and traffic control devices, and existing peak hour traffic volumes.

Site Location

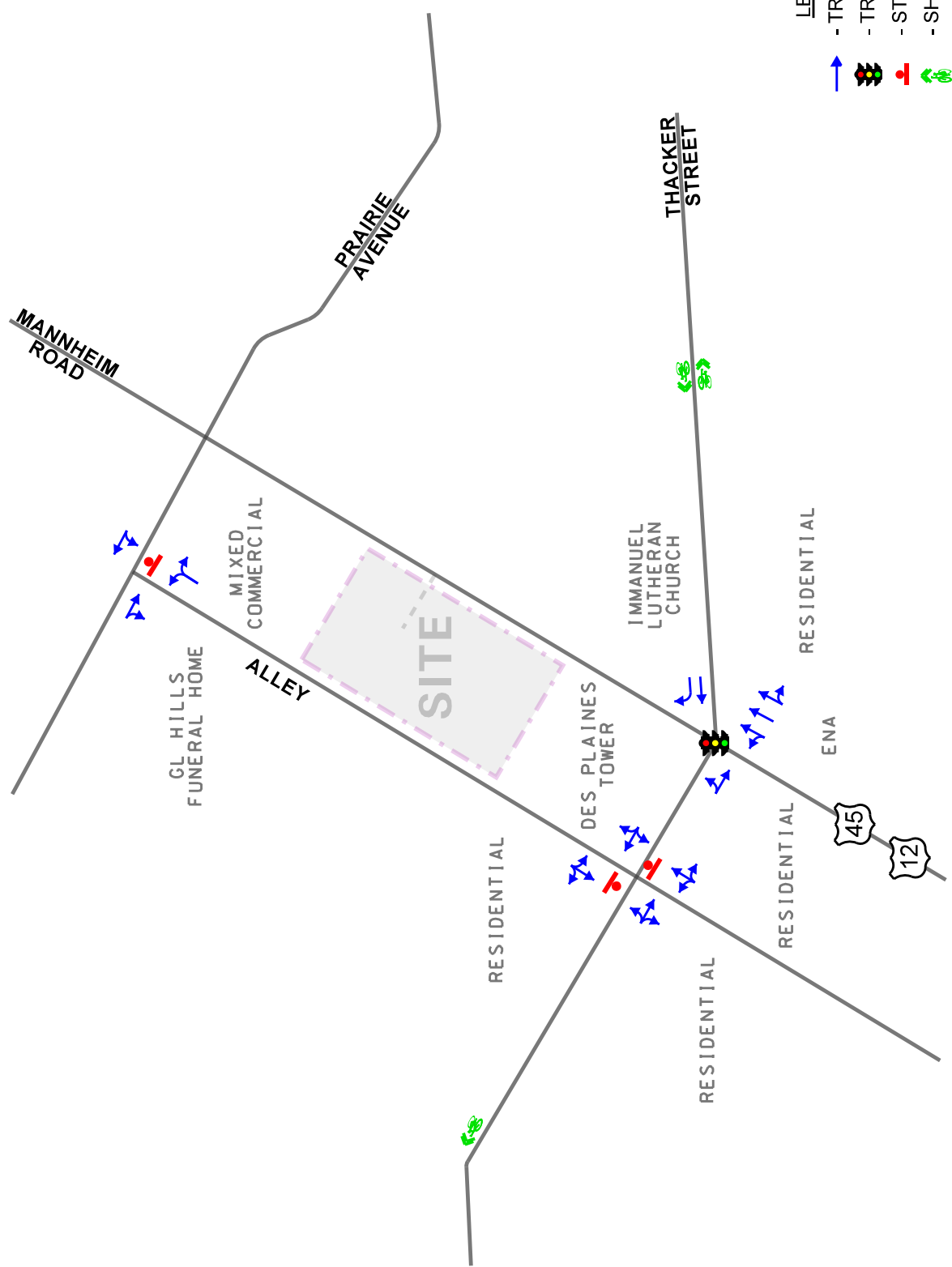
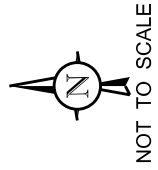
The site, which currently contains the vacant Immanuel Lutheran School building, is located on the west side of Mannheim Road/Lee Street approximately 350 feet north of Thacker Street. The site is bounded by a vacant retail building to the north, Mannheim Road/Lee Street to the east, an Immanuel Lutheran Church parking lot to the south, and a north-south alley to the west. Land uses within the vicinity of the site are primarily residential and commercial with multi-story apartment buildings and townhomes located east, west, and south of the site and commercial uses located north of the site.

Existing Roadway System Characteristics

The characteristics of the existing roadways near the school are described below and illustrated in **Figure 3**.

Mannheim Road/Lee Street (U.S. 12-45 Northbound) is a northbound-only, other principal arterial roadway that provides three lanes. At its signalized intersection with Thacker Street, Mannheim Road/Lee Street provides a combined through/left-turn lane, a through lane, and a combined through/right-turn lane. On-street parking is generally provided on the west side of the road. Mannheim Road/Lee Street is under the jurisdiction of the Illinois Department of Transportation (IDOT), has a posted speed limit of 30 mph, is not designated as a Strategic Regional Arterial (SRA), and carries an Annual Average Daily Traffic (AADT) volume of 6,900 (IDOT 2015) vehicles.

Thacker Street is an east-west, local roadway that provides one lane in each direction. At its signalized intersection with Mannheim Road/Lee Street, Thacker Street provides a combined through/left-turn lane on the eastbound approach and a through lane and a right-turn lane on the westbound approach. At its unsignalized intersection with the north-south alley, Thacker Street provides one lane in each direction. On-street parking is generally provided on both sides of the road. Thacker Street is under the jurisdiction of the City of Des Plaines, has a posted speed limit of 25 mph, and carries an Annual Average Daily Traffic (AADT) volume of 2,150 (IDOT 2015) vehicles.



- LEGEND**
- TRAVEL LANE
 - TRAFFIC SIGNAL
 - STOP SIGN
 - SHARROW

Existing Roadway Characteristics

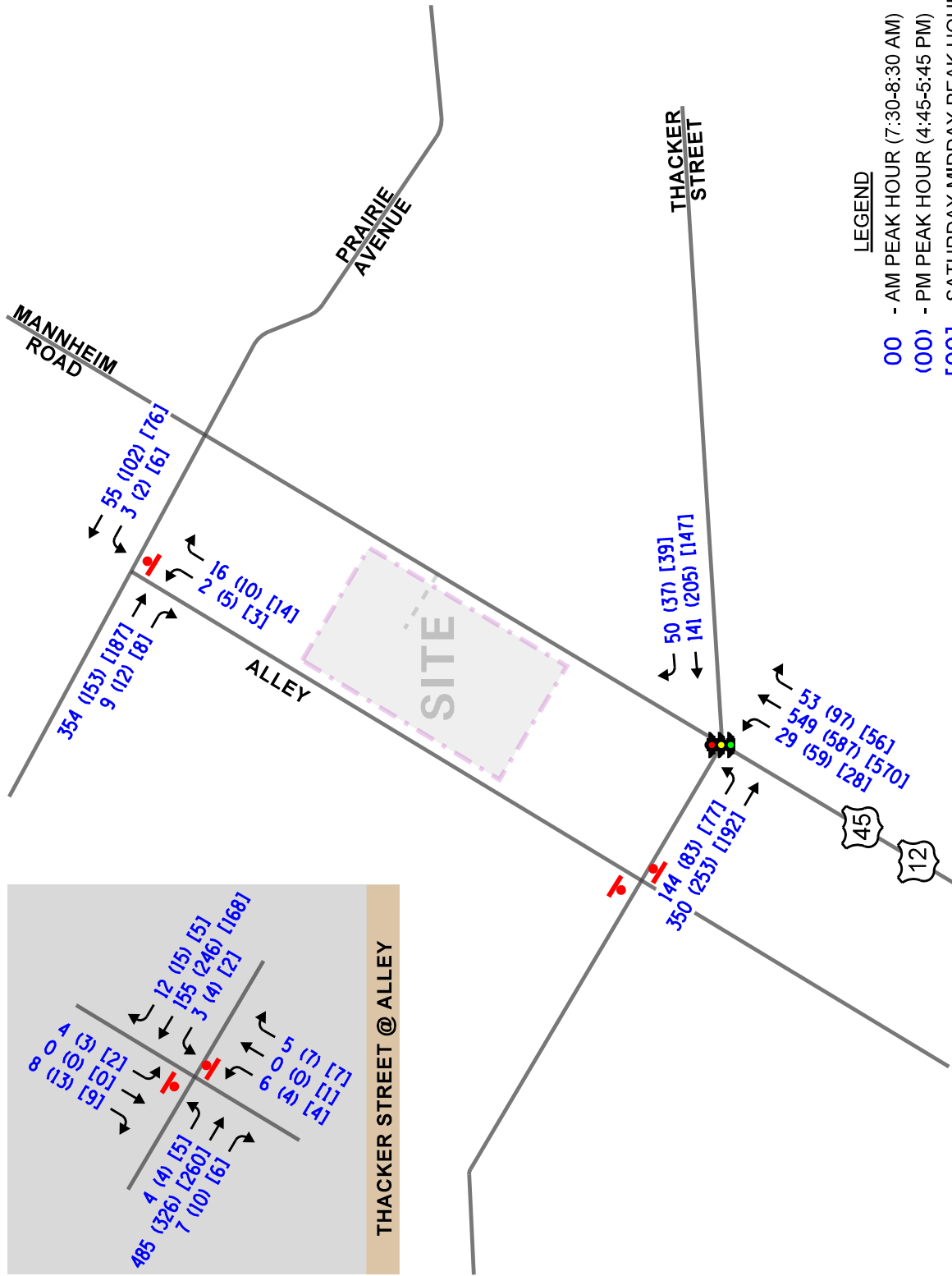
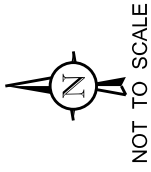
Little Bulgarian School
Des Plaines, Illinois

Prairie Avenue is an east-west, local roadway that provides one lane in each direction. At its unsignalized intersection with the north-south alley, *Prairie Avenue* provides one lane on both approaches. On-street parking is generally provided on both sides of the road. *Prairie Avenue* is under the jurisdiction of the City of Des Plaines.

Existing Traffic Volumes

In order to determine current traffic conditions in the vicinity of the site, KLOA, Inc. conducted peak period traffic counts using Miovision Scout Collection Units on Tuesday, November 13, 2018 during the weekday morning (7:00 A.M. to 9:00 A.M.) and weekday evening (4:00 P.M. to 6:00 P.M.) peak periods and on Saturday, November 10, 2018 during the Saturday midday (10:00 A.M. to 2:00 P.M.) peak period. The traffic counts were conducted at the intersections of Mannheim Road/Lee Street with Thacker Street, Thacker Street with the north-south alley, and *Prairie Street* with the north-south alley. The results of the traffic counts showed that the weekday morning peak hour of traffic occurs from 7:30 A.M. to 8:30 A.M., the evening peak hour of traffic occurs from 4:45 P.M. to 5:45 P.M., and the Saturday midday peak hour of traffic occurs from 12:45 P.M. to 1:45 P.M.

Figure 4 illustrates the existing peak hour traffic volumes for all three peak periods that the counts were performed. Copies of the traffic count summary sheets are included in the Appendix.



- LEGEND**
- 00 - AM PEAK HOUR (7:30-8:30 AM)
 - (00) - PM PEAK HOUR (4:45-5:45 PM)
 - [00] - SATURDAY MIDDAY PEAK HOUR (12:45-1:45 PM)

Existing Traffic Volumes

Little Bulgarian School
Des Plaines, Illinois

3. Traffic Characteristics of the Proposed School

In order to properly evaluate future traffic conditions in the surrounding area, it was necessary to determine the traffic characteristics of the proposed school, including the directional distribution and volumes of traffic that it will generate.

Proposed School Plan

As proposed, the existing Immanuel Lutheran School building will be occupied by a Little Bulgarian School. The building will serve the following primary purposes:

- After school and evening children's programs that include math and science tutoring, folklore dance classes, and other child-centered class activities. Children participating will either arrive when they are released from area schools or later in the evening. Based on information provided by the operator, the school will accommodate a combined total of 40 to 60 children participating in these programs.
- Dance, language, and citizenship classes for adults that will occur during the weekday evening. A majority of participants will arrive after work during the weekday evening peak hour and leave the site outside of typical peak hours. Based on information provided by the operator, the classes will have a typical enrollment of 40 to 60 adults.
- Bulgarian language, literature, geography, and culture classes for children that will occur on Saturday and Sunday mornings. Students will be divided into two staggered classes running from 9:00 A.M. to 1:00 P.M. and from 11:00 A.M. to 3:00 P.M. Based on information provided by the operator, the two classes combined will have maximum enrollment of 160 students on Saturday and 80 students on Sunday

In addition to the primary uses, the building will also be used for other minor or infrequent uses such as administrative offices or holiday performances, a majority of which will fall outside of typical peak hours. As part of the development, the existing parking lot will be modified to provide 16 spaces. Further, the vacant parcel north of the building will be improved with a 47-space parking lot for a combined total of 63 parking spaces. In addition, the school has signed a shared parking agreement with the Immanuel Lutheran Church to utilize their parking lot south of the site in case of overflow. A copy of the preliminary site plan is included in the Appendix.

Site Access

As proposed, access to the site will be provided via a proposed access drive on Lee Street located approximately 525 feet north of Thacker Avenue. This access drive will replace two existing access drives at this location. The access drive will provide one inbound lane and one outbound lane restricted to left-turn only movements and should be under stop sign control. Additional access to the site will be provided off the existing north-south alley along the site's west frontage and will be connected to the site via a southerly inbound-only access drive and via a northerly two-way access drive. The north-south alley provides one lane in each direction with connection to Thacker Street to the south and Prairie Avenue to the north.

Drop-Off/Pick-Up Operations

Students participating in the weekday afterschool programs, evening programs, Saturday classes, or Sunday classes will generally be dropped off by their parents at the beginning of the program and then picked up again once the program is completed. In order to reduce conflicts within the site and improve traffic flow parents should be instructed to follow the following pick-up/drop-off procedures:

- Parents who intend to park their vehicle and walk their child into the building should park in the proposed parking lot north of the building. These parents should utilize the Lee Street access drive to enter and exit the site.
- Parents who intend to pick-up/drop-off their children without exiting their vehicle should enter the site from the southerly north-south alley access and drop off/pick up their child from the north-south drive aisle along the site's west face. Parents should then exit the site at the northerly north-south alley access drive. Further, to reduce conflict on the alley, parents should be instructed to enter the alley from the south and exit to the north.

Separating the two pick-up/drop-off activity types will serve to improve traffic flow within the site and will limit the number of vehicles driving within the portion of the parking lot where parents will be walking between the building and their vehicle with their children.

A majority of adults attending the evening classes are expected to arrive and park on site for the duration of the class. Due to the low number of adults expected to participate in the classes, all parked vehicles can be accommodated within the proposed parking lot. In order to prevent conflict with the pick-up/drop-off activities of the after-school care programs, participants will be instructed to enter the site from the Lee Street entrance.

Directional Distribution

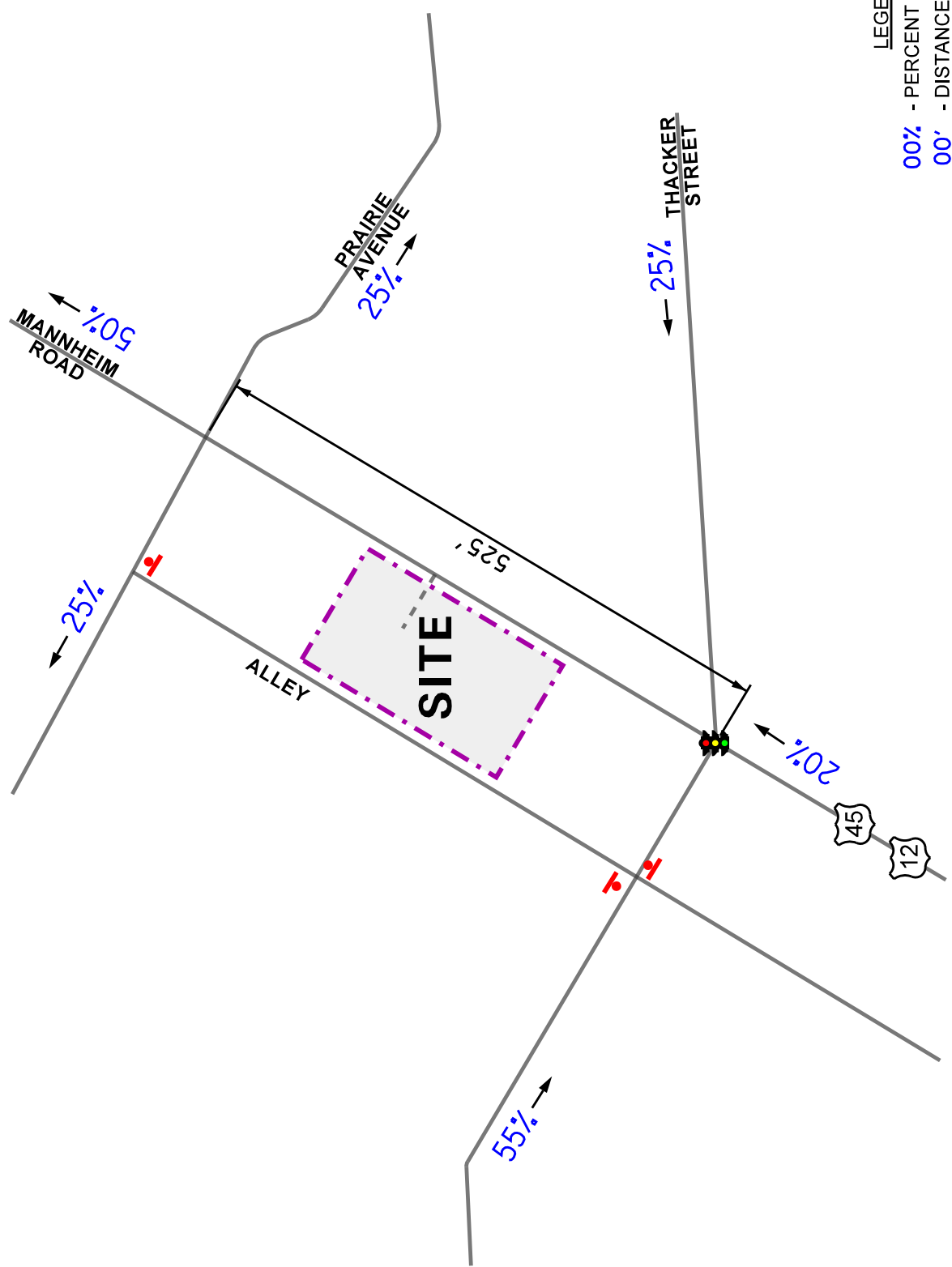
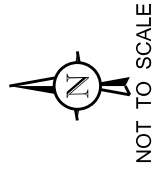
The directions from which employees, students, and parents will approach and depart the site were estimated based on existing travel patterns, as determined from the traffic counts. **Figure 5** illustrates the directional distribution of the school-generated traffic.

Peak Hour Traffic Volumes

The volume of traffic generated by a development is based on the type of land uses and the size of the development. The number of peak hour vehicle trips estimated to be generated by the proposed school were based on the following:

- The number of trips to be generated by the after-school and evening student programs and adult evening classes were based on information provided by the operator.
- The number of trips to be generated by the Saturday classes was based on traffic counts conducted by KLOA at an existing Little Bulgarian School located in Mount Prospect, Illinois. The counts were conducted on Saturday, December 8, 2018 between 8:30 A.M and 2:30 P.M. The existing Mount Prospect school serves one session of 95 students. In order to provide a conservative analysis, the same peak hour volumes were used as the Mount Prospect school despite the fact that the proposed school will split its students into 4 sessions with no more than 80 students.

For the purpose of this study, it was assumed that two-thirds of parents participating in pick-up/drop-off activity will not park their vehicle and will utilize the north-south alley. **Table 1** shows the traffic to be generated by weekday activities and **Table 2** shows traffic to be generated by Saturday activities.



LEGEND
00% - PERCENT DISTRIBUTION
00' - DISTANCE IN FEET

Little Bulgarian School
Des Plaines, Illinois

Estimated Directional Distribution

Table 1
 PEAK HOUR SITE-GENERATED TRAFFIC VOLUMES – OPERATOR INFORMATION

| Type/Size | Weekday Morning Peak Hour | | | Weekday Evening Peak Hour | | |
|---|---------------------------|-----|-------|---------------------------|-----|-------|
| | In | Out | Total | In | Out | Total |
| After School/Evening Student Programs (60 Children) | 0 | 0 | 0 | 30 | 30 | 60 |
| Adult Evening Classes (60 Participants) | 0 | 0 | 0 | 30 | 0 | 30 |
| Administrative Uses (5 Staff) | 5 | 0 | 5 | 0 | 5 | 5 |
| Total | 5 | 0 | 5 | 60 | 35 | 95 |

Table 2
 PEAK HOUR SITE-GENERATED TRAFFIC VOLUMES - KLOA SURVEYS

| Type/Size | Saturday Midday Peak Hour | | |
|--|---------------------------|-----|-------|
| | In | Out | Total |
| Saturday Morning Classes (80 Children/Session) | 55 | 48 | 103 |

4. Projected Traffic Conditions

The total projected traffic volumes include the existing traffic volumes, increase in background traffic due to growth, and the traffic estimated to be generated by the proposed subject school.

School Traffic Assignment

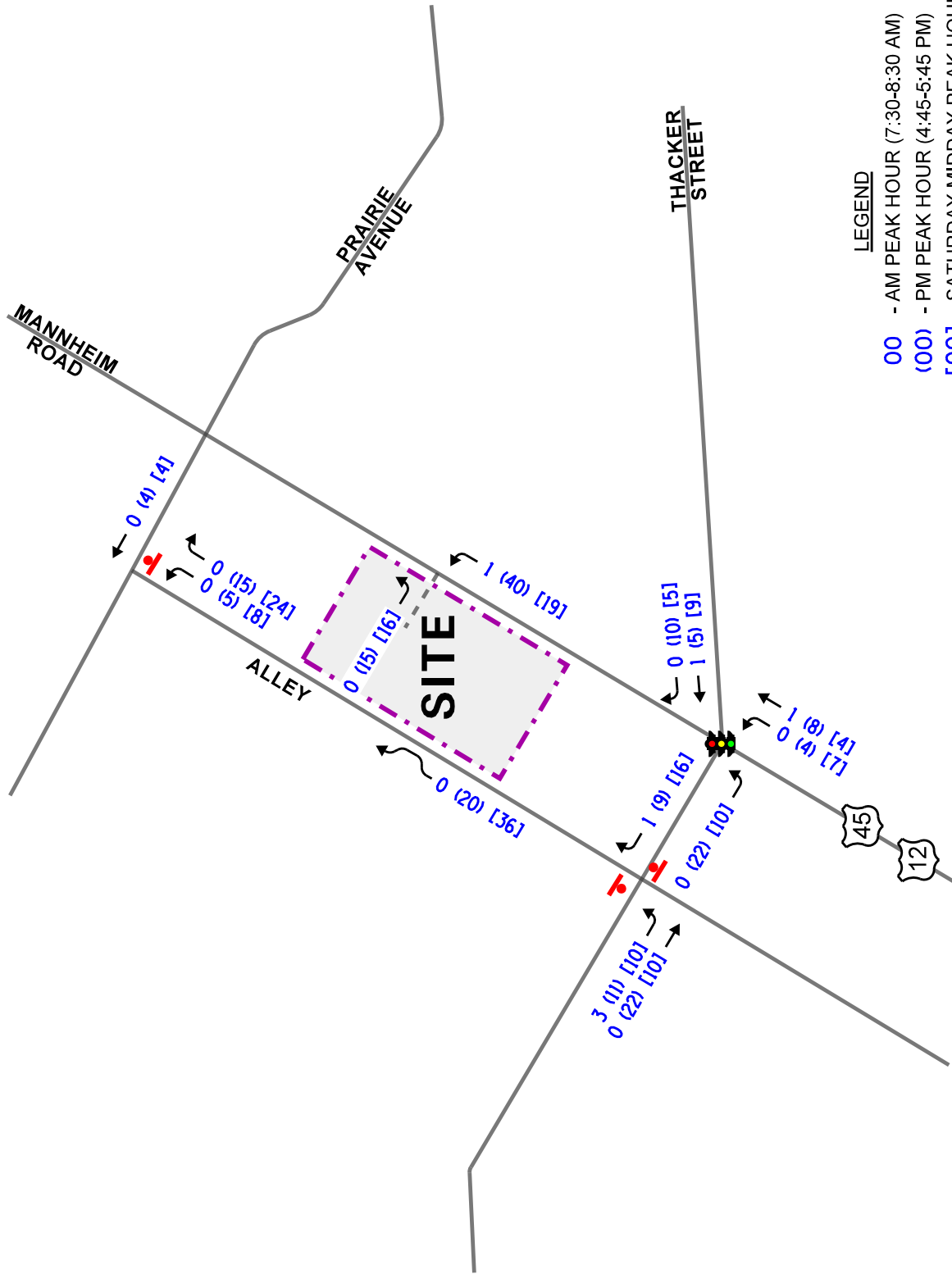
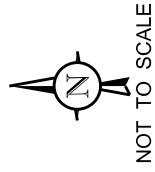
The estimated weekday morning, weekday evening, and Saturday midday peak hour traffic volumes that will be generated by the proposed school were assigned to the roadway system in accordance with the previously described directional distribution (Figure 5). The traffic assignment for the school is illustrated in **Figure 6**.

Background Traffic Conditions

The existing traffic volumes (Figure 4) were increased by a regional growth factor to account for the increase in existing traffic related to regional growth in the area (i.e., not attributable to any particular planned development). Based on AADT projections provided by the Chicago Metropolitan Agency for Planning (CMAP), the existing traffic volumes are projected to increase by a compound annual growth rate of 0.56 percent per year. As such, traffic volumes were increased by 3.4 percent (one-year buildout plus five years) to represent Year 2024 conditions. A copy of the CMAP projections letter is included in the Appendix.

Total Projected Traffic Volumes

The school-generated traffic was added to the existing traffic volumes accounting for background growth to determine the Year 2024 total projected traffic volumes, as shown in **Figure 7**. It should be noted that although the Saturday peak pick-up hour occurred mostly outside of the peak hour of adjacent roadway traffic, they were assumed to occur at the same times in order to provide a conservative analysis.



LEGEND

- 00 - AM PEAK HOUR (7:30-8:30 AM)
- (00) - PM PEAK HOUR (4:45-5:45 PM)
- [00] - SATURDAY MIDDAY PEAK HOUR (12:45-1:45 PM)

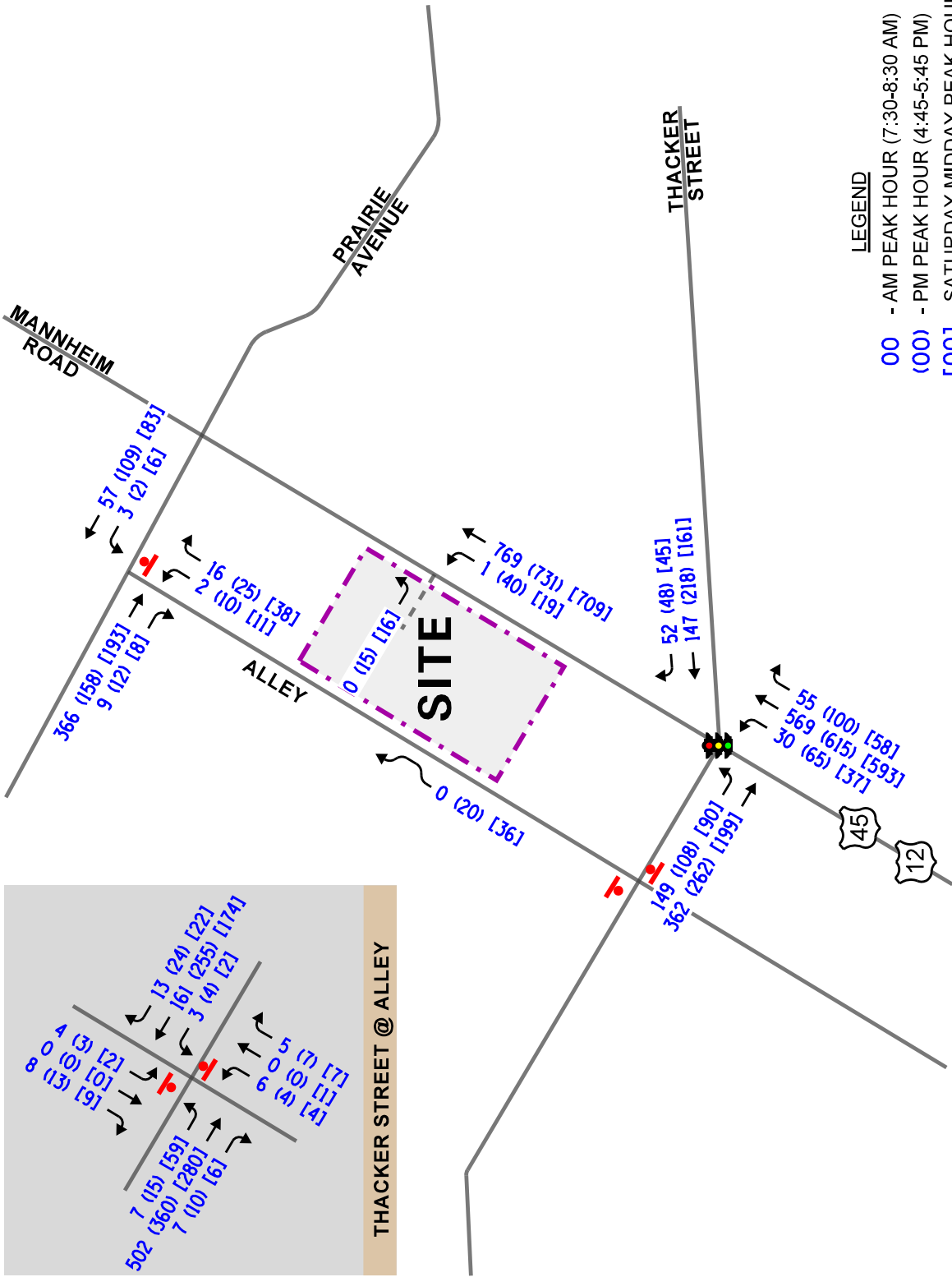
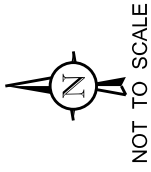
Little Bulgarian School
Des Plaines, Illinois

Estimated Site-Generated Traffic Volumes



Job No: 18-260

Figure: 6



- LEGEND**
- 00 - AM PEAK HOUR (7:30-8:30 AM)
 - (00) - PM PEAK HOUR (4:45-5:45 PM)
 - [00] - SATURDAY MIDDAY PEAK HOUR (12:45-1:45 PM)

Total Projected Traffic Volumes

Little Bulgarian School
Des Plaines, Illinois

5. Traffic Analysis and Recommendations

The following provides an evaluation conducted for the weekday morning, weekday evening, and Saturday midday peak hours. The analysis includes conducting capacity analyses to determine how well the roadway system and access drives are projected to operate and whether any roadway improvements or modifications are required.

Traffic Analyses

Roadway and adjacent or nearby intersection analyses were performed for the weekday morning, weekday evening, and Saturday midday peak hours for the existing (Year 2018) as well as future projected (Year 2024) traffic volumes.

The traffic analyses were performed using the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM)*, 6th Edition and analyzed using Synchro/SimTraffic 10 software.

The analyses for the unsignalized intersections determine the average control delay to vehicles at an intersection. Control delay is the elapsed time from a vehicle joining the queue at a stop sign (includes the time required to decelerate to a stop) until its departure from the stop sign and resumption of free flow speed. The methodology analyzes each intersection approach controlled by a stop sign and considers traffic volumes on all approaches and lane characteristics.

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter from A to F based on the average control delay experienced by vehicles passing through the intersection. The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for signalized intersections and unsignalized intersections are included in the Appendix of this report.

Summaries of the traffic analysis results showing the level of service and overall intersection delay (measured in seconds) for the existing and Year 2024 total projected conditions are presented in **Tables 3** through **5**. A discussion of the intersections follows. Summary sheets for the capacity analyses are included in the Appendix.

Table 3
 CAPACITY ANALYSIS RESULTS
 MANNHEIM ROAD/LEE STREET WITH THACKER STREET – SIGNALIZED

| | Peak Hour | Eastbound Left/Through | Westbound | | Northbound | | | Overall |
|-----------------------------------|---------------------------|---------------------------|-----------|-----------|------------|-----------|---|-----------|
| | | | Through | Right | L | T | R | |
| Year 2018 Existing Conditions | Weekday Morning Peak Hour | C 31.9 | B 17.3 | B 14.9 | C 25.2 | | | C 26.5 |
| | | | B – 16.7 | | | | | |
| | Weekday Evening Peak Hour | E 63.4 | C 33.3 | C 26.5 | B 13.1 | | | C 29.4 |
| C – 32.3 | | | | | | | | |
| Saturday Midday Peak Hour | E 55.7 | E 55.7 | C 34.9 | C 29.9 | A 7.9 | | | C 23.8 |
| | | | C – 33.8 | | | | | |
| Year 2024 Projected Conditions | Weekday Morning Peak Hour | C 29.4 | B 15.1 | B 13.4 | C 27.0 | | | C 26.2 |
| | | | B – 15.1 | | | | | |
| | Weekday Evening Peak Hour | D 49.1 | D 49.1 | C 27.3 | C 21.7 | B 17.5 | | |
| C – 26.3 | | | | | | | | |
| Saturday Midday Peak Hour | E 56.3 | E 56.3 | C 32.2 | C 28.3 | A 8.9 | | | C 24.6 |
| | | | C – 32.2 | | | | | |

Delay is measured in seconds.

Table 4

CAPACITY ANALYSIS RESULTS – EXISTING CONDITIONS - UNSIGNALIZED

| Intersection | Weekday Morning Peak Hour | | Weekday Evening Peak Hour | | Saturday Midday Peak Hour | |
|---|---------------------------|-------|---------------------------|-------|---------------------------|-------|
| | LOS | Delay | LOS | Delay | LOS | Delay |
| Thacker Street with North-South Alley | | | | | | |
| • Eastbound Left Turns | A | 7.6 | A | 7.8 | A | 7.6 |
| • Westbound Left Turns | A | 8.6 | A | 8.0 | A | 7.8 |
| • Northbound Approach | C | 15.3 | B | 11.9 | B | 11.2 |
| • Southbound Approach | B | 11.9 | B | 10.8 | B | 10.0 |
| Prairie Avenue with North-South Alley | | | | | | |
| • Westbound Left Turns | A | 8.1 | A | 7.7 | A | 7.7 |
| • Eastbound Approach | B | 11.0 | A | 9.9 | A | 9.7 |
| LOS = Level of Service Delay is measured in seconds. | | | | | | |

Table 5

CAPACITY ANALYSIS RESULTS – PROJECTED CONDITIONS - UNSIGNALIZED

| Intersection | Weekday Morning Peak Hour | | Weekday Evening Peak Hour | | Saturday Midday Peak Hour | |
|--|---------------------------|-------|---------------------------|-------|---------------------------|-------|
| | LOS | Delay | LOS | Delay | LOS | Delay |
| Thacker Street with North-South Alley | | | | | | |
| • Eastbound Left Turns | A | 7.6 | A | 7.9 | A | 7.8 |
| • Westbound Left Turns | A | 8.7 | A | 8.0 | A | 7.9 |
| • Northbound Approach | C | 15.9 | B | 12.4 | B | 12.5 |
| • Southbound Approach | B | 12.2 | B | 11.1 | B | 10.6 |
| Prairie Avenue with North-South Alley | | | | | | |
| • Westbound Left Turns | A | 8.1 | A | 7.7 | A | 7.7 |
| • Northbound Approach | B | 11.0 | B | 10.0 | B | 10.7 |
| Mannheim Road/Lee Street with the Proposed Access | | | | | | |
| • Eastbound Approach | -- | -- | B | 10.9 | B | 10.6 |
| LOS = Level of Service Delay is measured in seconds. | | | | | | |

Discussion and Recommendations

The following summarizes how the intersections are projected to operate and identifies any roadway and traffic control improvements necessary to accommodate the school-generated traffic.

Mannheim Road/Lee Street with Thacker Street

The results of the capacity analyses indicate that the intersection currently operates at Level of Service (LOS) C during the weekday morning, weekday evening, and Saturday midday peak hours. Under future conditions, the intersection is projected to continue to operate at the same LOS with an increase in delay of one second or less. It should be noted that the eastbound approach currently operates and is projected to continue to operate at LOS E during the weekday evening peak hour. However, the proposed school is not projected to add any traffic to this approach and it will continue to operate at the same LOS with an increase in delay of less than one second during the evening peak hour. As such, this intersection has sufficient reserve capacity to accommodate the school-generated traffic and no roadway improvements or signal modifications are required at this intersection.

Thacker Street with the North-South Alley

The results of the capacity analyses indicate that the northbound approach currently operates at LOS C or better during the weekday morning, weekday evening, and Saturday midday peak hours and the southbound approach operates at LOS B during all three peak hours. Under projected conditions, both approaches are projected to continue to operate at the same LOS with increases in delay of less than one second. Further, the eastbound and westbound left-turning movements operate and are projected to continue to operate at LOS A during the weekday morning, weekday evening, and Saturday midday peak hours. As such, the proposed school traffic will have a limited impact on the operations of this intersection and no roadway or traffic control improvements will be required.

Prairie Avenue with the North-South Alley

The results of the capacity analyses indicate that the northbound approach operates at LOS B or better during the weekday morning, weekday evening, and Saturday midday peak hours and is projected to continue to operate at LOS B with an increase in delay of less than one second. Further, the westbound left-turning movement is projected to operate at LOS A during all three peak hours. As such, the proposed school will have a limited impact on the operations of this intersection and no roadway or traffic control improvements will be required.

Lee Street with the Proposed Right-In/Right-Out Access Drive

The results of the capacity analysis indicate that outbound movements from the proposed access drive onto Lee Street are projected to operate at LOS B or better during the weekday morning, weekday evening, and Saturday midday peak hours with 95th percentile queues of one to two vehicles. As such, this access drive will be adequate in accommodating the traffic projected to be generated by the proposed development and will ensure efficient and flexible access is provided.

6. Conclusion

Based on the preceding analyses and recommendations, the following conclusions have been made:

- The proposed school, given its size and type, will generate a limited amount of traffic during the weekday peak hours.
- The staggered schedule of the Saturday morning classes will serve to distribute traffic over a longer time period and reduce traffic generated during any one peak hour.
- The modified existing parking lot and the proposed parking lot, in conjunction with the adjacent Immanuel Lutheran Church, will sufficiently accommodate the school's parking demands.
- The use of the alley for drop-off and pick-up activity with cars entering off Thacker Street and exiting onto Prairie Avenue will ensure efficient operation, reducing traffic conflicts and backups.

PARKING LOT LICENSE AGREEMENT

THIS PARKING LOT LICENSE AGREEMENT (this "License Agreement") is made as of the 31 day of May, 2019 (the "Effective Date"), by and between Immanuel Lutheran Church of Des Plaines Illinois, an Illinois not for profit corporation, ("Licensor"), and Little Bulgarian School in Chicago, an Illinois not for profit corporation ("Licensee").

RECITALS

A. Licensor is the owner of certain real property including a 28-space paved parking lot and improvements situated thereon (the "Parking Lot") and known as 854 Lec Street, Des Plaines, Illinois (together the "Property").

B. Licensee desires to use and Licensor desires to grant exclusive use of the 28-space Parking Lot together with reasonable pedestrian and vehicular access to and from the Property to Licensee as more fully set forth below.

NOW, THEREFORE, in consideration of \$1.00 and mutual covenants set forth herein, and other good and valuable consideration, Licensor and Licensee mutually agree as follows:

1. Recitals Incorporated. The recitals set forth above are incorporated herein by reference as if more fully set forth herein below.

2. Parking License. Licensor hereby licenses to Licensee, and Licensee hereby licenses from Licensor the Parking Lot, on an exclusive basis Monday through Friday from 6:39PM to 11:30PM, Saturday from 1:30PM to 11:59PM and on Sunday from 1:30PM to 11:30PM during the Term and upon the terms and conditions set forth herein and which shall be sufficient in size to park up to 28 cars and is depicted on **Exhibit A** attached hereto. The parties understand and agree that (i) Licensee parking shall be strictly limited to the Parking Lot, and (ii) Licensee shall use the Parking Lot in accordance with zoning and all present and future laws and ordinances, (iii) Licensor reserves the right to use up to two (2) parking spaces in the Parking Lot at any time during the month of May, 2019, and (iv) Licensor may use the entire Parking Lot for special events up to two (2) days per year with reasonable prior written notice to Licensee. Licensee and its employees, agents, invitees and contractors (the "Licensee Parties") shall comply with the reasonable regulations promulgated by Licensor from time to time related to parking of which Licensee has received written notice or which are prominently posted in the Parking Lot provided they are consistent with the terms and conditions of this License Agreement.

3. Term. This License Agreement is granted for a term (the "Term") beginning on the day Licensee acquires title to the property at 820-848 Lee Street, Des Plaines, IL from Licensor (which shall be the Effective Date shown above) (the "Commencement Date") and shall terminate on August 31, 2021 (the "Termination Date"). Notwithstanding the foregoing, Licensee may terminate this License Agreement early upon 60 days advance written notice to Licensor. Upon termination, Licensee shall immediately remove all vehicles from the Parking Lot and surrender the Parking Lot to Licensor free and clear of any litter and debris. Any vehicles not so removed within five (5) days following the Termination Date may be towed by Licensor at Licensee's sole cost and expense.

4. License Fee. Licensee shall pay Licensor a license fee of One Hundred and 00/100s Dollars \$100.00 per month, which fee shall be payable in advance, on the first day of each calendar month. With respect to any partial months at the beginning or end of the License term, this license fee shall be pro-rated based upon the total number of days in the applicable month. Licensee's initial payment shall be due the first of the month following any partial month used and shall include both partial and upcoming month's amount due.

5. Acceptance of Parking Lot. Licensee has inspected the Parking Lot and accepts the same as existing and in "AS IS" condition. Licensee shall make no alterations or modifications, structural or non-structural, to the Parking Lot. Licensor reserves the right to make alterations and modifications to any and all portions of the Parking Lot, provided that such modifications shall not alter the number of cars that may be parked by Licensee in the Parking Lot during the term of this License Agreement, and that in the event that such alterations or modifications interrupt Licensee's quiet enjoyment of the Parking Lot or any part thereof, Licensor shall provide reasonable alternative spaces of the same size, type and quality as close as possible to the Parking Lot at no additional cost to Licensee for the entire period of time that such use is interrupted.

6. Signage. Licensee shall have no right to install any signage in the Parking Lot or elsewhere on the Property.

7. Rules and Regulations. The use of the Parking Lot by Licensee is subject to reasonable rules and regulations prescribed by Licensor of which Licensee has received written notice or which are prominently posted in the Parking Lot, including, but not limited to, rules and regulations with respect to the direction and routing of vehicles provided such rules and regulations are consistent with this License Agreement.

8. Licensee's Obligations. Licensee, at its sole cost and expense, covenants and agrees (i) to keep the Parking Lot reasonably free from trash, litter, garbage, refuse, debris and obstructions, (ii) to repair any damage to the Parking Lot or the Property caused by Licensee or its invitees, employees, agents and/or contractors, and (iii) not to place, keep, permit or maintain within the Parking Lot any fence, barricade or other obstruction which unreasonably interferes with the intended uses thereof or prevents the free flow of pedestrian or vehicular traffic thereto or therein. If Licensee fails to perform any of the foregoing covenants, or any other covenants or agreements of Licensee under this License Agreement, Licensor shall provide written notice of the same to Licensee, and if Licensee fails to correct the same within fourteen (14) days, Licensor may perform the same and Licensee agrees to reimburse Licensor within ten (10) days of demand therefor for the full reasonable costs thereof. The foregoing shall survive the termination of this License Agreement for 1 year.

Licensor's Obligations. Licensor shall, at its sole cost and expense, (i) provide Licensee with the right of quiet enjoyment of the Parking Lot without interruption or disturbance from Licensor, any third party, or other party lawfully claiming by and through Licensor, (ii) except as specified herein, ensure that the Parking Lot and means of access and egress are free from trash, litter, garbage, refuse, debris or obstructions not placed there directly by Licensee or its authorized users, (iii) remove snow and ice from the Parking Lot and the means of egress and access thereto; and (iv) keep the Parking Lot in compliance with laws and ordinances and in good, paved condition, free of potholes or other hazards.

9. Insurance. Licensee shall, at Licensee's expense, obtain and maintain throughout the term of this License Agreement Commercial General Liability insurance in minimum amounts not less than

\$1,000,000.00 for one occurrence, \$2,000,000.00 annual aggregate, and Licensee shall forward to Licensor an endorsement to the foregoing liability policy naming Licensor as an additional insured.

10. Indemnification. Notwithstanding the insurance to be maintained by Licensee pursuant to Section 9, and to the extent permitted by law, Licensee shall indemnify and save Licensor, its parents, subsidiaries, affiliates, employees and agents, harmless from and against any claim, action, damage, liability and expense in connection with loss of life, personal injury and/or damage to personal property arising from or out of the use by Licensee of the Parking Lot or the Property, or occurring on or about any portion of the Parking Lot or the Property during the term of this License Agreement, which loss of life, personal injury and/or damage to personal property is occasioned by the negligent act of the Licensee, its agents, contractors, employees and invitees, except to the extent caused by the negligence or misconduct of Licensor, its agents or employees. Licensor hereby agrees to defend, indemnify and save Licensee harmless from any and all liabilities, damages, causes of action, suits, claims, judgments, costs and expenses of any kind (including reasonable attorney's fees) (i) relating to or arising from or in connection with Licensor's possession, use, occupation, management, repair, maintenance or control of the Parking Lot or any portion thereof, (ii) relating to or arising from or in connection with any negligent act or omission of Licensor or Licensor's agents, contractors, employees, invitees, licensees or others for whom Licensor is legally responsible, and/or (iii) relating to or arising from or in connection with the Licensor's material breach of any condition, covenant or obligation of this License imposed on Licensor; provided, however, that Licensor's indemnification shall not apply to the extent any of the foregoing in this paragraph arise from the negligence or willful misconduct of Licensee or its employees, agents, servants, licensees or contractors. The obligations of the parties under this section shall survive the termination of the License.

11. Limitation of Liability. Any goods, property or personal effects stored or placed by Licensee, its employees or agents, in or about the in the Parking Lot or the Property shall be at the sole risk of Licensee, and Licensor shall not in any manner be held responsible therefor. The parties agree that the foregoing limitation of liability shall not vitiate the indemnification by Licensor set forth in paragraph 10 above.

12. Breach. Upon (i) a breach of any monetary obligation which remains uncured for fourteen (14) days following written notice thereof from Licensor to Licensee, or (ii) a breach of any non-monetary obligation which remains uncured for thirty (30) days following written notice thereof from Licensor to Licensee, then Licensor shall have the right to bring an action in equity for specific performance of this License Agreement, or bring an action against Licensee for monetary damages at law. In the event Licensee defaults in the performance of any of the terms, covenants, agreements or conditions contained in this License Agreement, and Licensor places the enforcement of all or any part of this License, the collection of any monies due or to become due, in the hands of an attorney, Licensee agrees to pay Licensor's reasonable attorneys' fees and expenses where suit is actually filed. In addition, if any legal action, arbitration or other proceeding is commenced to enforce and/or interpret any and every provision of this License Agreement and/or to pursue any remedy for default of this License Agreement, the "Prevailing Party" shall be entitled to an award of its fees and expenses incurred in connection therewith, including without limitation, reasonable attorneys' fees. The term "Prevailing Party" shall include a party who receives substantially the relief desired whether by settlement, dismissal, summary judgment or otherwise.

13. Notices. Each notice given pursuant to this License Agreement shall be given in writing and shall be (i) delivered in person, (ii) sent by nationally recognized overnight courier service, or (iii) sent by certified mail, return receipt requested, first class postage prepaid, to Licensor or Licensee, as the case may be, at their respective notice addresses as set forth below, or at any such other address that may be given by one party to the other by notice pursuant to this section. Such notices, if given as prescribed in this section,

PARKING LICENSE LBSC V.1 0013369.001

shall be deemed to have been given (a) at the time of delivery if delivery is made in person, (b) on the next business day if deposited with a nationally recognized overnight courier service in time for next day delivery, (c) on the third business day following the date of mailing if mailed, or (d) at the time of delivery if delivery is refused or cannot be effected at the addressee's address (as evidenced in writing). During any interruption or threatened interruption of substantial delay in postal services, all notices shall be delivered personally or by nationally recognized overnight courier service. Electronic communication (i.e. "e-mail") shall not serve as "written notice" for the purposes described herein.

If to Licensor: Immanuel Lutheran Church of Des Plaines
855 Lee Street
Des Plaines, IL 60016

If to Licensee: Little Bulgarian School in Chicago
832 Lee Street
Des Plaines, IL 60016

14. Authority. If Licensee is a corporation or partnership, the person executing this License Agreement on behalf of Licensee represents and warrants that Licensee is duly organized and validly existing; that this License Agreement has been authorized by all necessary parties, is validly executed by an authorized officer or agent of Licensee and is binding upon and enforceable against Licensee in accordance with its terms.

15. No Conveyance. The license created hereunder is for the limited purpose described hereinabove. No title to or estate in any real or personal property is hereby conveyed, and the parties expressly agree that the rights established hereby do not create a relationship of landlord and tenant, partnership, joint venture or any other relationship with respect to the Parking Lot other than that of licensor and licensee.

16. Governing Law. This License Agreement shall be governed by and construed in accordance with the laws of the State of Illinois, without regard to the conflict of laws principles thereof.

17. Time of Essence. Time is of the essence with respect to all obligations under this License Agreement.


18. Final Agreement. This writing is intended by the parties hereto as a final expression of their agreement and is a complete and exclusive statement of its terms. This License Agreement can only be modified by a writing signed by each of the parties hereto.

19. Brokers. Licensor and Licensee each represent and warrant one to another that neither of them has employed any broker, agent or finder in carrying on the negotiations relating to this License Agreement. Licensor shall indemnify and hold Licensee harmless, and Licensee shall indemnify and hold Licensor harmless, from and against any claim or claims for broker or other commissions arising from or out of any breach of the foregoing representation and warranty by the respective indemnitors.

[Signatures appear on following page]

IN WITNESS WHEREOF, the parties hereto have executed this License Agreement as of the day and year first above written.

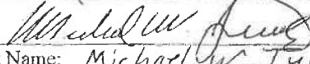
WITNESS/ATTEST:




Licensor:

Immanuel Lutheran Church

By:


Print Name: Michael W. Juritz
Title: Chairman

WITNESS/ATTEST:



Licensee:

By: G. P. A. R. O. V.
Print Name: GEORGE P. A. R. O. V.
Title: PRESIDENT

Exhibit A

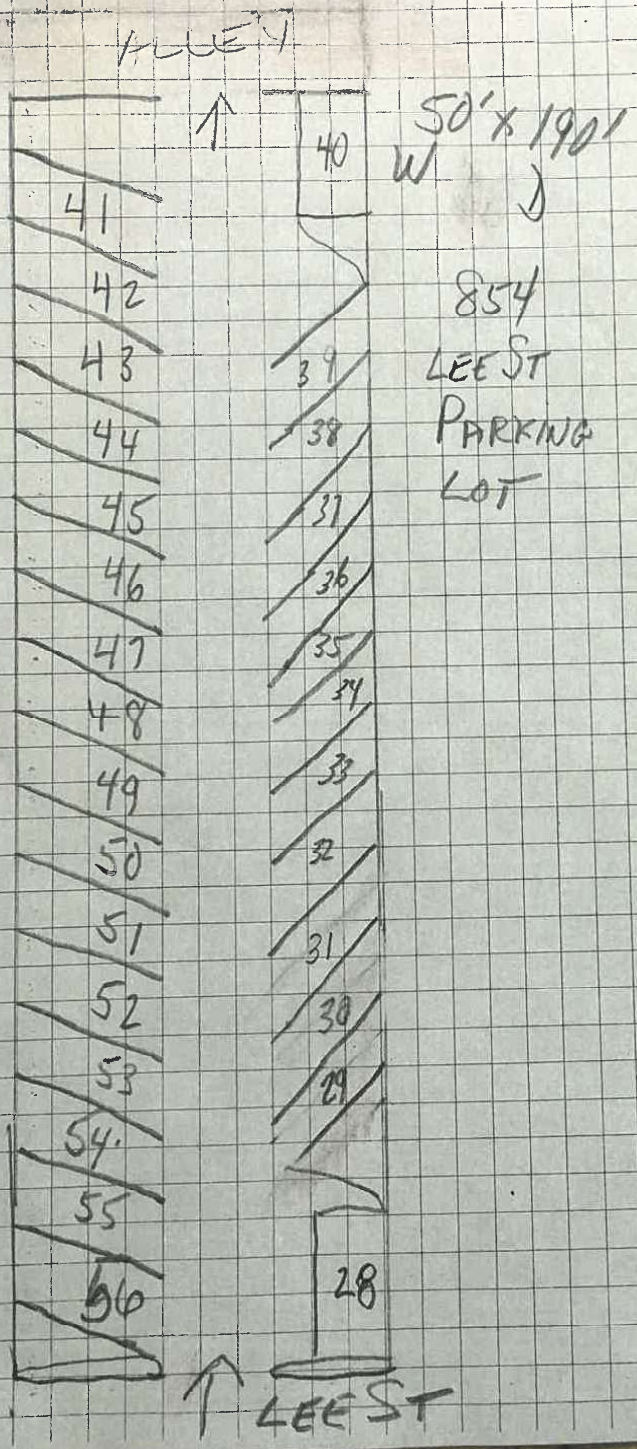
Parking Lot 1

IMMANUEL LUTHERAN CHURCH PARKING LOT - 28 PARKING SPACES LOCATED AT 854 LEE STREET, DES PLAINES, IL 60016

[SEE ATTACHED SITE PLAN OF PARKING LOT]

PARKING LICENSE LBSC V.1 0013369.001

6



**FIRST AMENDMENT AND RESTATEMENT OF
PARKING LOT LICENSE AGREEMENT**

THIS FIRST AMENDMENT AND RESTATEMENT OF PARKING LOT LICENSE AGREEMENT (this "Amendment") is entered into to be effective as of March 2, 2023 (the "Effective Date"), by and between **Immanuel Lutheran Church of Des Plaines Illinois**, an Illinois not for profit corporation ("Licensor"), and **Little Bulgarian School in Chicago**, an Illinois not for profit corporation ("Licensee").

RECITALS:

- A. Licensor and Licensee entered into that certain Parking Lot License Agreement dated May 31, 2019 (the "Agreement") with respect to the real property known as 854 Lee Street, Des Plaines, Illinois, as more particularly described in the Agreement (the "Property").
- B. The Property is improved with a paved parking lot containing 28 automobile parking spaces and related improvements (collectively, the "Parking Lot").
- C. Pursuant to Section 2 of the Agreement, Licensor granted Licensee a license to exclusive use of the Parking Lot during certain times and dates throughout the Term of the Agreement (as defined in the Agreement), subject to certain terms and conditions.
- D. Pursuant to Section 3 of the Agreement, the Term of the Agreement terminates on August 31, 2021 ("Termination Date").
- E. After the Termination Date, Licensor has continued to license the Parking Lot to Licensee, and Licensee has continued to license the Parking Lot from Licensor and has paid the license fee set forth in the Agreement to Licensor.
- F. The Licensor and Licensee have agreed to amend and restate the Agreement as set forth herein.

NOW, THEREFORE, in consideration of the Recitals, which are hereby incorporated into this Amendment as if fully set forth herein, and the mutual covenants hereinafter set forth and other good and valuable consideration, the receipt and sufficiency of which hereby are acknowledged, Licensor and Licensee agree as follows:

1. **Termination Date.** Section 3 of the Agreement is hereby amended by changing the Termination Date to August 31, 2025.

2. **Renewal Terms.** Section 3 of the Agreement is further amended by inserting the following text after the last sentence of Section 3: "The Term of the License Agreement shall automatically renew for a period of one (1) year commencing on the Termination Date, and for successive one (1) year periods commencing on each anniversary of the Termination Date thereafter (each, a "Renewal Term"), unless and until the Licensor provides written notice to

Licensee no fewer than 120 days before the Termination Date or the last day of the then-current Renewal Term of the Licensor's intent to terminate the License Agreement.

3. **Restatement and Continuing Effect.** Except as specifically amended by this Amendment, the provisions of the Agreement remain in full force and effect and are hereby restated, ratified, affirmed and approved, and shall be binding upon Licensor and Licensee and their respective successors and assigns. In the event of inconsistency conflict between the terms of this Amendment and the terms of the Agreement, the terms of this Amendment shall control.

4. **Defined Terms.** All capitalized terms used, but not specifically defined herein, shall have the meanings ascribed in the Agreement.

5. **Counterparts.** This Amendment may be executed in multiple counterparts, each of which when taken together shall constitute but one in the same instrument.

IN WITNESS WHEREOF, the parties hereto have executed this Amendment as of the Effective Date.

Licensor

IMMANUEL LUTHERAN CHURCH OF DES PLAINES ILLINOIS, an Illinois not-for-profit corporation

By: Immanuel Lutheran Church
Name: Robert A. Nesbitt
Title: Congregation Chairman

Licensee

LITTLE BULGARIAN SCHOOL IN CHICAGO, an Illinois not for profit corporation

By: G. Petrov
Name: George Petrov
Title: President of The Board

Subject Property Legal Descriptions

**Little Bulgarian School
820-852 Lee Street, Des Plaines, IL
Conditional Use Permit Amendment**

PARCEL 1: THE NORTH 1/2 OF LOT 8 AND THE SOUTH 25 FEET OF LOT 7, ALL IN BLOCK 4 IN PARSON AND LEE'S ADDITION TO DES PLAINES, A SUBDIVISION IN SECTION 17, TOWNSHIP 41 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PIN: 09-17-425-029-0000

STREET ADDRESS: 820 Lee Street, Des Plaines, Illinois 60016.

PARCEL 2: THE SOUTH 'A OF LOT 8 IN BLOCK 4 IN PARSON AND LEE'S ADDITION TO DES PLAINES, A SUBDIVISION IN SECTION 17 AND 20, TOWNSHIP 41 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PIN: 09-17-425-030-0000

STREET ADDRESS: 822 Lee Street, Des Plaines, Illinois 60016.

PARCEL 3: LOT 9 IN BLOCK 4 IN PARSON AND LEE'S ADDITION TO DES PLAINES, A SUBDIVISION IN SECTION 17 AND 20, TOWNSHIP 41 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PIN: 09-17-42-031-0000

STREET ADDRESS: 832 Lee Street, Des Plaines, Illinois 60016.

PARCEL 4: LOT 10 IN BLOCK 4 EXCEPT THE SOUTHWESTERLY 40 FEET THEREOF, IN PARSON AND LEE'S ADDITION TO DES PLAINES, A SUBDIVISION IN SECTION 17 AND 20, TOWNSHIP 41 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PIN: 09-17-425-032-0000

STREET ADDRESS: 842 Lee Street, Des Plaines, Illinois 60016.

PARCEL 5: THE SOUTHWESTERLY 40 FEET OF LOT 10 IN BLOCK 4 IN PARSON AND LEE'S ADDITION TO DES PLAINES, A SUBDIVISION IN SECTION 17 AND 20, TOWNSHIP 41 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PIN: 09-17-425-033-0000

STREET ADDRESS: 852 Lee Street, Des Plaines, Illinois 60016.

CITY OF DES PLAINES

ORDINANCE Z - 12 - 19

AN ORDINANCE GRANTING CONDITIONAL USE PERMITS AND A MAJOR VARIATION AT 820-848 LEE STREET, DES PLAINES, ILLINOIS (CASE #19-015-CU-V).

WHEREAS, George Petrov, on behalf of the Little Bulgarian School, ("**Petitioner**") is the contract purchaser of that certain property commonly known as 820-848 Lee Street, Des Plaines, Illinois ("**Subject Property**"); and

WHEREAS, the Subject Property is located within the C-5, Central Business District ("**C-5 District**") and is currently improved with a two-story masonry building ("**School Building**"), two single-family homes ("**Single-Family Homes**"), and an off-street parking area; and

WHEREAS, Immanuel Lutheran Church ("**Owner**") is the owner of the Subject Property; and

WHEREAS, Petitioners desires to operate (i) a commercially zoned assembly use and a private school for high school students within the School Building on the Subject Property; and (ii) lease; and

WHEREAS, pursuant to Section 12-7-3.H and 12-7-3.K of the Zoning Ordinance of 1998, as amended ("**Zoning Ordinance**") commercially zoned assembly uses and private schools are permitted in C-5 Districts only with conditional use permits; and

WHEREAS, Section 12-9-7 of the Zoning Ordinance requires: (i) commercially zoned assembly uses to provide one parking space for every 200 square feet of gross activity area; and (ii) private high schools to provide one parking space per classroom, plus one parking space per 200 square feet of area devoted to offices, plus one parking space for every six students based on maximum enrollment for a private school; and

WHEREAS, Petitioner and Owner have entered into a parking lease agreement ("**Parking Lease Agreement**"), pursuant to which Petitioner may use up to 28 parking spaces in the parking lot immediately south of 848 Lee Street, which is also owned by Owner; and

WHEREAS, Petitioner submitted an application to the City of Des Plaines Department of Community and Economic Development ("**Department**") for: (i) a conditional use permit to allow the operation of a commercially zoned assembly use on the Subject Property, in accordance with Sections 12-7-3.K and 12-3-4 of the Zoning Ordinance; (ii) a conditional use permit to allow the operation of a private school on the Subject Property, in accordance with Sections 12-7-3.K and 12-3-4 of the Zoning Ordinance (collectively, (i) and (ii) are the "**Conditional Use Permits**") and (iii) a major variation from Section 12-9-7 of the Zoning Ordinance to reduce the number of parking spaces to 63, where 73 parking spaces are required ("**Major Variation**") (collectively (i) through (iii) are the "**Requested Relief**"); and

WHEREAS, the Petitioner's application were referred by the Department to the Planning and Zoning Board of the City of Des Plaines ("**PZB**") within 15 days after the receipt thereof; and

WHEREAS, within 90 days from the date of the Petitioner's applications a public hearing was held by the PZB on April 23, 2019 pursuant to notice published in the *Journal* on April 3, 2019; and

WHEREAS, notice of the public hearing was mailed to all property owners within 300 feet of the Subject Property; and

WHEREAS, during the public hearing, the PZB heard competent testimony and received evidence with respect to how the Petitioner intended to satisfy and comply with the applicable provisions of the Zoning Ordinance. The PZB filed a written report with the City Council on April 24, 2019, summarizing the testimony and evidence received by the PZB and stating the PZB's recommendation, by a vote of 7-0, to approve the Petitioner's applications subject to certain terms and conditions; and

WHEREAS, the Petitioner made certain representations to the PZB with respect to the Requested Relief, which representations are hereby found by the City Council to be material and upon which the City Council relies in granting this request for the Conditional Use Permits and Major Variation; and

WHEREAS, the City Council has considered the written report of the PZB, the applicable standards for conditional use permits and major variations set forth in the Zoning Ordinance, and the Community and Economic Development Staff Memorandum dated April 30, 2019, including the attachments and exhibits thereto, and has determined that it is in the best interest of the City and the public to grant the Petitioner's application in accordance with the provisions of this Ordinance;

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Des Plaines, Cook County, Illinois, in the exercise of its home rule powers, as follows:

SECTION 1. RECITALS. The recitals set forth above are incorporated herein by reference and made a part hereof, the same constituting the factual basis for this Ordinance.

SECTION 2. LEGAL DESCRIPTION OF SUBJECT PROPERTY. The Subject Property is legally described as follows:

PARCEL 1: THE NORTH ½ OF LOT 8 AND THE SOUTH 25 FEET OF LOT 7, ALL IN BLOCK 4 IN PARSON AND LEE'S ADDITION TO DES PLAINES, A SUBDIVISION IN SECTION 17, TOWNSHIP 41 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PARCEL 2: THE SOUTH ½ OF LOT 8 IN BLOCK 4 IN PARSON AND LEE'S ADDITION TO DES PLAINES, A SUBDIVISION IN SECTION 17 AND 20, TOWNSHIP 41 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PARCEL 3: LOT 9 IN BLOCK 4 IN PARSON AND LEE'S ADDITION TO DES PLAINES, A SUBDIVISION IN SECTION 17 AND 20, TOWNSHIP 41 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PARCEL 4: LOT 10 IN BLOCK 4 EXCEPT THE SOUTHWESTERLY 40 FEET THEREOF, IN PARSON AND LEE'S ADDITION TO DES PLAINES, A SUBDIVISION IN SECTION 17 AND 20, TOWNSHIP 41 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PARCEL 5: THE SOUTHWESTERLY 40 FEET OF LOT 10 IN BLOCK 4 IN PARSON AND LEE'S ADDITION TO DES PLAINES, A SUBDIVISION IN SECTION 17 AND 20, TOWNSHIP 41 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PINs: 09-17-425-029-0000, 09-17-425-030-0000, 09-17-42-031-0000,
09-17-425-032-0000 & 09-17-425-033-0000

Commonly known as 820-848 Lee Street, Des Plaines, Illinois.

SECTION 3. CONDITIONAL USE PERMITS. Subject to and contingent upon the conditions, restrictions, limitations and provisions set forth in Section 5 of this Ordinance, the City Council grants the Petitioner the Conditional Use Permits to allow the following on the Subject Property: (i) the operation of a commercially zoning assembly; and (ii) the operation of a private school. The Conditional Use Permits granted by this Ordinance are consistent with and equivalent to a "special use" as referenced in Section 11-13-25 of the Illinois Municipal Code, 65 ILCS 5/11-13-25.

SECTION 4. MAJOR VARIATION. The City Council finds that the Major Variation satisfies the standards set forth in Section 12-3-6.H of the Zoning Ordinance and, pursuant to the City's home rule powers, that the Major Variation is otherwise necessary and appropriate. Subject

to and contingent upon the conditions, restrictions, limitations and provisions set forth in Section 5 of this Ordinance, the City Council hereby grants the Major Variation from Section 12-9-7 of the Zoning Ordinance to reduce the required number of parking spaces on the Subject Property from 73 to 63.

SECTION 5. CONDITIONS. The Conditional Use Permits granted in Section 3 and the Major Variation granted in Section 4 of this Ordinance shall be, and are hereby, expressly subject to and contingent upon the following conditions, restrictions, limitations, and provisions:

A. Compliance with Law and Regulations. The development, use, operation, and maintenance of the Subject Property by the Petitioner must comply with all applicable City codes and ordinances, as the same have been or may be amended from time to time, except to the extent specifically provided otherwise in this Ordinance.

B. Compliance with Plans. Except for minor changes and site work approved by the City Director of Community and Economic Development or Director of Public Works and Engineering (for matters within their respective permitting authorities) in accordance with all applicable City standards, the development, use, operation, and maintenance of the Subject Property by the Petitioner must comply with the following plans provided by Petitioner:

1. Geometry Plan, prepared by Engineering Resource Associates, Inc., consisting of one sheet, and dated March 2019, a copy of which is attached to, and by this reference, made a part of, this Ordinance as **Exhibit A**; and
2. Planting Plan, prepared by Engineering Resource Associates, Inc., consisting of one sheet, and dated April 2019, a copy of which is attached to, and by this reference, made a part of, this Ordinance as **Exhibit B**; and
3. Project Narrative, prepared by the Board of Directors of the Little Bulgarian School, consisting of five sheets, dated April 2019, a copy of which is attached to, and by this reference, made a part of, this Ordinance as **Exhibit C** (collectively, Exhibits A through C are the "**Plans**").

B. Additional Conditions. The development, use, and maintenance of the Subject Property shall be subject to and contingent upon the following additional conditions:

1. The operation of the commercially zoned assembly and private school uses shall be located only within the School Building at 832 Lee Street. The Single-Family Homes shall not be used for commercially zoned assembly or private school uses.
2. Any expansion for any use shall require the Petitioner to obtain an amendment to the Conditional Use Permits.
3. The Subject Property shall only be used as a commercially zoned assembly use and for a private school for the following activities:
 - a. Community services;
 - b. Recreational and social activities that comply with all applicable codes;
 - c. Private school and adult education lessons; and
 - d. Office uses directly related to Little Bulgarian School Organization.
4. The south radius of the proposed Lee Street driveway shall be a five-foot radius to visually reinforce the one-way northbound flow of Lee Street. A "No Right Turn" sign shall be installed on the private side of the property line adjacent to the Lee Street curb cut to prevent wrong way traffic. These items shall be shown on future engineering drawings.
5. The curb cuts being closed on Lee Street shall be restored to the City of Des Plaines and IDOT specifications, including the abutting streetscape.
6. That the maximum number of people gathering in any space shall not exceed the maximum occupancy load prescribed by the Fire Protection Department.
7. Any food service preparation for any member shall come from a commercial grade kitchen.
8. The Petitioner shall maintain the Parking Lease Agreement as long as the Subject Property is used for a commercially zoned assembly use and a private school. Any amendment to the Parking Lease Agreement shall be approved by the Director of Community and Economic Development. If the Parking Lease Agreement is terminated, the Petitioner shall enter into a new parking lease agreement for at least 10 parking spaces within 30 days of termination of the Parking Lease Agreement.
9. Drawings submitted for permit shall be in substantial compliance with the Plans except where amendments are needed to comply with all applicable codes.
10. Stop signs shall be added on the Subject Property for the two drive aisles that are adjacent to the proposed Lee Street entrance/exit.

SECTION 6. NONCOMPLIANCE.

A. Any person, firm or corporation who violates, disobeys, omits, neglects or refuses to comply with, or resists the enforcement of, any of the provisions of this Ordinance shall be fined not less than seventy five dollars (\$75.00) or more than seven hundred and fifty dollars (\$750.00) for each offense. Each and every day that a violation of this Ordinance is allowed to remain in effect shall constitute a complete and separate offense. In addition, the appropriate authorities of the City may take such other action as they deem proper to enforce the terms and conditions of this Ordinance, including, without limitation, an action in equity to compel compliance with its terms. Any person, firm or corporation violating the terms of this Ordinance shall be subject, in addition to the foregoing penalties, to the payment of court costs and reasonable attorneys' fees.

B. In the event that the Petitioner fails to use, develop, and maintain the Subject Property in accordance with the provisions, conditions, and restrictions of this Ordinance and of the applicable provisions of the Zoning Ordinance, the Conditional Use Permits granted in Section 3 of this Ordinance and the Major Variation granted in Section 4 of this Ordinance may be revoked after notice and hearing before the Zoning Administrator of the City, all in accordance with the procedures set forth in Section 12-4-7 of the Zoning Ordinance. In the event of revocation, the development and use of the Subject Property will be governed solely by the regulations of the C-5 District. Further, in the event of such revocation of the Conditional Use Permit and the Variation, the City Manager and City's General Counsel are hereby authorized and directed to bring such zoning enforcement action as may be appropriate under the circumstances. The Petitioner acknowledges that public notices and hearings have been held with respect to the adoption of this Ordinance, has considered the possibility of the revocation provided for in this Section, and agrees not to challenge any such revocation on the grounds of any procedural infirmity or any denial of

any procedural right, provided that the notice and hearing required by Section 12-4-7 of the Zoning Ordinance is provided to the Petitioner.

SECTION 7. RECORDATION; BINDING EFFECT. A copy of this Ordinance must be recorded in the Office of the Cook County Recorder of Deeds. This Ordinance and the privileges, obligations, and provisions contained herein run with the Subject Property and inure to the benefit of, and are binding upon, the Petitioner and its respective personal representatives, successors, and assigns, including, without limitation, subsequent purchasers of the Subject Property.

SECTION 8. EFFECTIVE DATE.

A. This Ordinance shall be in full force and effect only after the occurrence of the following events:

1. its passage and approval by the City Council in the manner provided by law;
2. its publication in pamphlet form in the manner provided by law;
3. the filing with the City Clerk by the Petitioner of an unconditional agreement and consent to accept and abide by each and all of the terms, conditions, and limitations set forth in this Ordinance, and demonstrating the Petitioner's consent to its recordation. Said unconditional agreement and consent shall be in substantially the form attached to, and by this reference made a part of, this Ordinance as **Exhibit D**; and
4. at the Petitioner's sole cost and expense, the recordation of this Ordinance together with such exhibits as the City Clerk deems appropriate, with the Office of the Cook County Recorder.

B. In the event that the Petitioner does not file with the City Clerk a fully executed copy of the unconditional agreement and consent referenced in Section 8.A.3 of this Ordinance, within 60 days after the date of passage of this Ordinance by the City Council, the City Council shall have the right, in its sole discretion, to declare this Ordinance null and void and of no force or effect.

SECTION 9. SEVERABILITY. If any paragraph, section, clause or provision of this Ordinance is held invalid, the remainder shall continue in full force and effect without affecting the validity of the remaining portions of the Ordinance.

[SIGNATURE PAGE FOLLOWS]

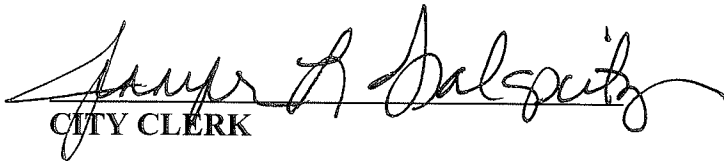
PASSED this 3rd day of June, 2019.

APPROVED this 3rd day of June, 2019.

VOTE: AYES 7 NAYS 0 ABSENT 1

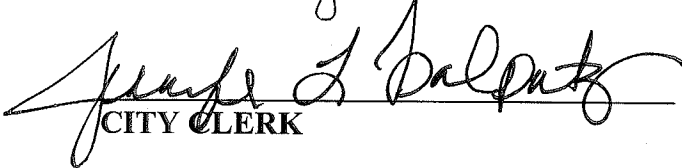

MAYOR

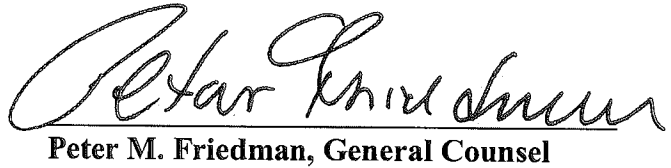
ATTEST:


CITY CLERK

Published in pamphlet form this
4th day of June, 2019.


Approved as to form:


CITY CLERK


Peter M. Friedman, General Counsel

I, GEORGE PETROV, being the owner or other party in interest of the property legally described within this Ordinance, having read a copy of the Ordinance, do hereby accept, concur and agree to develop and use the Subject Property in accordance with the terms of this Ordinance.

Dated: 6/6/19


(Signature)

DP-Ordinance Approving a CUP for a Private School and a Major Variation for Parking at 832 Lee Street

CITY OF DES PLAINES

ORDINANCE NO. Z-12-19

AN ORDINANCE GRANTING CONDITIONAL USE PERMITS AND A
MAJOR VARIATION AT 820-848 LEE STREET, DES PLAINES, ILLINOIS
(CASE #19-015-CU-V)

ADOPTED ON JUNE 3, 2019
BY THE CITY COUNCIL
OF THE
CITY OF DES PLAINES

Published in pamphlet form by authority of the City Council of the City of Des Plaines,
Cook County, Illinois, on this 4th day of June, 2019.

| DATE | BY | DESCRIPTION |
|---------|-----|---------------------------|
| 4/21/19 | JLM | ISSUED FOR PERMITS |
| 4/21/19 | JLM | REVISED PER CITY COMMENTS |

| DATE | BY | DESCRIPTION |
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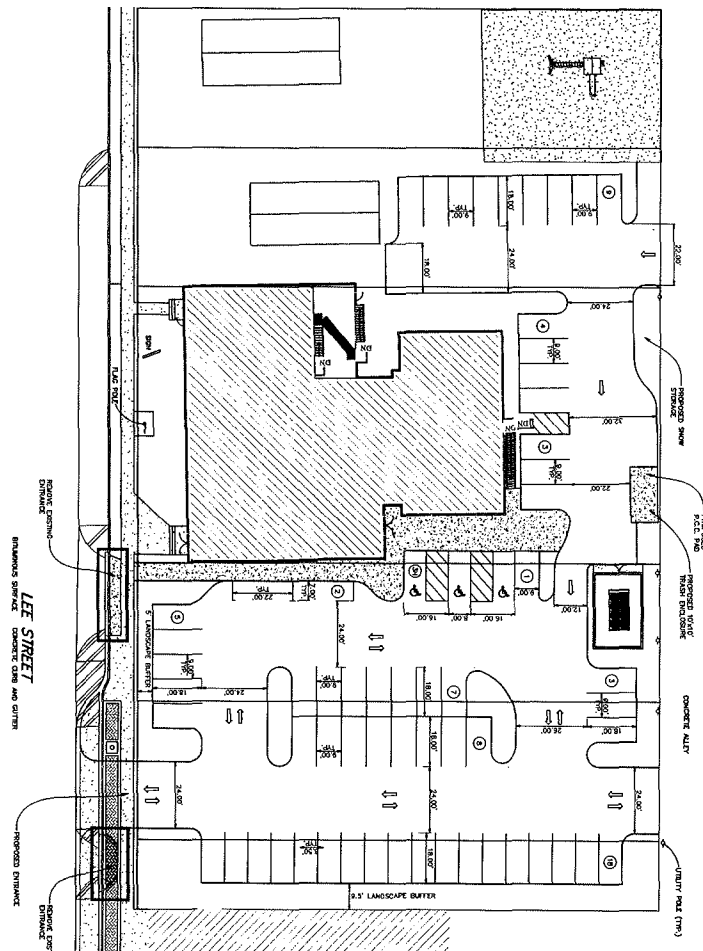
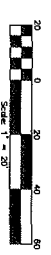


ENGINEERING RESOURCE ASSOCIATES
 3300 W. WASHINGTON AVENUE, SUITE 200
 CHICAGO, IL 60659
 PHONE (773) 232-2200
 FAX (773) 232-2201

LITTLE BUCKHORN SCHOOL
 625 LEE STREET
 DES PLAINES, ILLINOIS

GEOMETRY PLAN

SCALE - 1" = 20'
 DATE - MARCH 2019
 DRAWN BY - JLM
 CHECKED BY - JLM



PARKING SPACES:
 REGULAR STALLS: 40
 ACCESSIBLE STALLS: 3
 TOTAL: 43





Little Bulgarian School

Project Narrative

Prepared by:

Board of Directors - Little Bulgarian School

April 2019

Executive Summary

Little Bulgarian School (LBS) began operation in the school year 2005/2006 with total of 6 students. The organization was created mainly for Bulgarian children who wish to learn the Bulgarian language, history and culture. In 2010, LBS was recognized as a 501 (c) (3) non-profit organization by the US authorities allowing it grow even faster. As of 2018, LBS provides education services to more than 600 children in 2 facilities in Elk Grove Village and 1 in Mount Prospect. LBS is officially registered with the Bulgarian Ministry of Education and is entitled to issue certificates of graduation from grade 1st to 12th.

For the new building, LBS plans to transfer 100 children from our Saturday school in Mount Prospect plus additional 20 kids from the Elk Grove location. By the second year we plan to expand this location to 200 kids and to begin to offer Sunday classes. On the weekend the classes will have 2 shifts – 9 am to 1 pm and 11 am to 3 pm, depending on the parent's preference. For the weekend classes we plan to have up to 15 teachers / staff members on site. Students will be dropped off and picked up by their parents at the back of the building as per the recommendations of the traffic study.

In addition to the Saturday school, in order to financially support the building, we will start to offer after school programs that include math and science tutoring, folklore dance classes, and other child centered class activities. These activities will be composed of 12-20 children per class session, with one instructor and the possibility of one helper. The classes will range from 5:30PM to 8:00PM on a weekday basis, there might be 2 to 3 classes at a time. Students will be dropped off by their parents in the back of the building and picked up in the same manner. As part of the purchase contract LBS will be renting the 2 houses that are on the premises back to the seller. Other than collecting rent, LBS has no other plans to use the 2 residences as a part of the organization's activities relating to the school building.

For adults we plan to start offering weekday evening classes for citizenship and ESL, as well as folklore dances. These classes will have up to 2 instructors and 15-20 adults attending at a time.

We plan to organize small gatherings / concerts to commemorate the holidays with performances by the children from the school. At these events we estimate to have about 100 families attending. The events will be happening in the gym on or around the following dates:

1. First day of school (1st week of Sept)
2. Halloween
3. November 1st
4. Christmas
5. March 3rd.
6. Easter

7. May 24

Currently the gym has been used by a local basketball team for many years. LBS will seek to extend that relationship after we purchase the building. Other than this we have no immediate plans to rent any parts of our facility to other parties.

The building will not be used as a place of worship.

Currently, LBS has no central location/office and this is preventing parents, teachers, foreign dignitaries, community leaders, and art performers to gather easily and exchange information and knowledge. LBS is relying on the Elk Grove Library for meetings, Elk Grove High School, Christus Victor Lutheran Church and Christian Life College classes, as well as the private residential homes of many of our parents and board members. LBS is committed of finding a permanent home to provide even better service to its students, faculty and the local community:

The main development objectives of this new venue are:

- Establish a main hub for all current activities offered by LBS in the Mount Prospect facility including Bulgarian weekend classes and all other after school activities.
- Positively influence the Des Plaines and nearby suburbs Slavic community, consisting of Macedonian, Serbian, Polish, Russian, Ukrainian and many other nationalities, by offering adults classes and activities.
- Community outreach center for all Slavic and other nationalities in the Des Plaines and nearby suburbs.
- Open 2 new FTE positions to manage the building in Des Plaines, attract new residents.
- Maintains high level of integrity and full transparency to the community.

Future possible uses of the new venue may include the following services and programs:

- Children's Library and Book Share center.
- Food Pantry center.
- General Clothing provision program.
- Citizenship classes.
- Adult weekday evening language classes.
- Folklore and dance classes for kids and adults.
- Math and science tutoring.
- Pre- and afterschool programs.

Mission:

LBS is an educational and cultural center, the preferred choice in learning Bulgarian language, tradition and history, while also building children's tolerance and compassion for other cultures and languages. LBS also provides quality afterschool services for working families of all socio-economic levels in a nurturing environment to the local community.

Vision:

LBS will strive to remain the best educational center to preserve the Bulgarian national and spiritual identity for future generations by inspiring passion for learning the Bulgarian language and keeping Bulgarian tradition alive.

Little Bulgarian School history of unique children teaching technics, via games, songs, drama and priority given to conversational speech, has proven to be the right growth strategy. The training relies exclusively on the most modern methods of language teaching, where students experience the joy of learning. The organization also performs social functions as it is often used as meeting place and community resource for the children as well as parents.

Management Team:

Little Bulgarian School is registered as a non-profit organization under state and federal authorities and it is managed by 5 board members with combined professional experience in the finance, legal and most importantly educational area of more than 135 combined years.

Market Position and Future Growth:

According to the 2000 Census there were 63K Bulgarians living in the State of Illinois and in the 2010 Census this number increase to 105K, or 70% higher in just 10 years. It is currently estimated that more than 150K Bulgarians permanently live in State of Illinois, with very high concentration in the Chicago Northwest suburbs like Des Plaines, many of which now started a family.

Based on the Bulgarian Ministry of Education for the 2018/2019 school year, there are total of 12 registered Bulgarian schools in Chicagoland with total of 1142 children enrolled. Little Bulgarian school represented total of 597 children, or a market share of 52%.

Given the current trend, it is expected that more and more young Bulgarians will continue to relocate to the Northwest suburbs from other states or Bulgaria, as Chicago metro is now the biggest Bulgarian community establishment in North America. Bulgarian families with young children will prefer to move in specifically to Des Plaines as there are many established Bulgarian daycares, restaurants, for example Balkanika, Mehanata, and Nick's Grill, two Bulgarian churches (St. Sophia Bulgarian Orthodox Church, New Life Evangelical Bulgarian Church), as well as many ethnic grocery stores for example Malincho and Serdika. In addition, there are many organized year-round Bulgarian picnics, concerts and social events. All this

shows that the need for Bulgarian schools in the area will continue to grow significantly, as well as the need for social-sport events and adult classes.

Little Bulgarian School is well positioned for strong growth in the future, and a permanent home and cultural hub will further fuel this growth to provide outstanding services to the local community, while also help families and children transition into the American society. Little Bulgarian School has proven thru out the years that it has positive cultural and development impact to the community and it is expected to do so even more in the future.

EXHIBIT D

UNCONDITIONAL AGREEMENT AND CONSENT

TO: The City of Des Plaines, Illinois ("**City**");

WHEREAS, George Petrov, on behalf of the Little Bulgarian School, ("**Petitioner**") is the contract purchaser of that certain property commonly known as 820-848 Lee Street, Des Plaines, Illinois ("**Subject Property**"); and

WHEREAS, Petitioner applied to the City of Des Plaines for: (i) a conditional use permit to allow the operation of a commercially zoned assembly use on the Subject Property, in accordance with Sections 12-7-3.K and 12-3-4 of the Zoning Ordinance; (ii) a conditional use permit to allow the operation of a private school on the Subject Property, in accordance with Sections 12-7-3.K and 12-3-4 of the Zoning Ordinance (collectively, (i) and (ii) are the "**Conditional Use Permits**") and (iii) a major variation from Section 12-9-7 of the Zoning Ordinance to reduce the number of parking spaces to 63, where 73 parking spaces are required ("**Major Variation**") (collectively (i) through (iii) are the "**Requested Relief**"); and

WHEREAS, Ordinance No. Z-12-19 adopted by the City Council of the City of Des Plaines on June 3, 2019 ("**Ordinance**"), grants approval of the Conditional Use Permits and Major Variation subject to certain conditions; and

WHEREAS, Petitioner desires to evidence to the City its unconditional agreement and consent to accept and abide by each of the terms, conditions, and limitations set forth in said Ordinance, and their consent to recording the Ordinance and this Unconditional Agreement and Consent against the Subject Property;

NOW, THEREFORE, Petitioner does hereby agree and covenant as follows:

1. Petitioner hereby unconditionally agrees to accept, consent to and abide by all of the terms, conditions, restrictions, and provisions of that certain Ordinance No. Z-12-19, adopted by the City Council on June 3, 2019.
2. Petitioner acknowledges and agrees that the City is not and shall not be, in any way, liable for any damages or injuries that may be sustained as a result of the City's review and approval of any plans for the Subject Property, or the issuance of any permits for the use and development of the Subject Property, and that the City's review and approval of any such plans and issuance of any such permits does not, and shall not, in any way, be deemed to insure Petitioner against damage or injury of any kind and at any time.
3. Petitioner acknowledges that the public notices and hearings have been properly given and held with respect to the adoption of the Ordinance, has considered the possibility of the revocation provided for in the Ordinance, and agrees not to challenge any such revocation on the grounds of any procedural infirmity or any

denial of any procedural right, provided that the procedures required by Section 12-4-7 of the City's Zoning Ordinance are followed.

4. Petitioner agrees to and does hereby hold harmless and indemnify the City, the City's corporate authorities, and all City elected and appointed officials, officers, employees, agents, representatives, and attorneys, from any and all claims that may, at any time, be asserted against any of such parties in connection with (a) the City's review and approval of any plans and issuance of any permits, (b) the procedures followed in connection with the adoption of the Ordinance, (c) the development, construction, maintenance, and use of the Subject Property, and (d) the performance by Petitioner of its obligations under this Unconditional Agreement and Consent.
5. Petitioner hereby agrees to pay all expenses incurred by the City in defending itself with regard to any and all of the claims mentioned in this Unconditional Agreement and Consent. These expenses shall include all out-of-pocket expenses, such as attorneys' and experts' fees, and shall also include the reasonable value of any services rendered by any employees of the City.

ATTEST:

GEORGE PETROV

By: _____

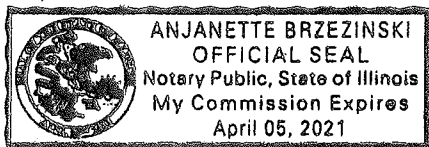
Mary G. [Signature]

GPetrov

SUBSCRIBED and **SWORN** to
before me this 6th day of
June, 2019.

Anjanette Brzezinski

Notary Public





MEMORANDUM

Date: April 20, 2023
To: Planning and Zoning Board (PZB)
From: Jonathan Stytz, AICP, Senior Planner JS
Cc: John T. Carlisle, AICP, Director of Community and Economic Development
Subject: Consideration of Major Variations for Trellis Height and Width in the R-1 District at 1773 Webster Lane, Case #23-019-V

Issue: The petitioner is requesting Major Variations to allow an 11-foot-tall and 50-foot-wide trellis in the interior side yard at 1773 Webster Lane where a maximum height of six feet and a maximum width of eight feet are permitted.

Petitioner: Demetrios and Isabelle Giokaris, 1773 Webster Lane, Des Plaines, IL 60018
Owner: Demetrios and Isabelle Giokaris, 1773 Webster Lane, Des Plaines, IL 60018
Case Number: 23-019-V
PIN: 09-29-101-022-0000
Ward: #5, Alderman Carla Brookman
Existing Zoning: R-1 Single Family Residential district
Existing Land Use: Single Family Residence
Surrounding Zoning: North: R-1 Single Family Residential district
South: R-1 Single Family Residential district
East: R-1 Single Family Residential district
West: R-1 Single Family Residential district
Surrounding Land Use: North: Single Family Residence (Residential)
South: Single Family Residence (Residential)
East: Single Family Residence (Residential)
West: Single Family Residence (Residential)
Street Classification: Webster Lane is classified as a local road.

Comprehensive Plan: The Comprehensive Plan illustrates the site as residential.

Zoning/Property History: Based on City records, the subject property was annexed into the City in 1953 and has been used as a single-family residence.

Background: *Structure Installation and Enforcement*
The existing structure is located along the north property line and spans 50 feet from the residence to the frame shed as shown on the attached Plat of Survey/Site Plan. This structure was installed without a permit in November 2022. On November 17, 2022, a complaint was filed regarding the structure in question, and Code Enforcement visited the property on November 18, 2022 to investigate. Based on the findings, code enforcement sent the property owner a letter informing that the structure did not meet either the fence or trellis limitations of the Zoning Ordinance. Even though the rules have since been amended (Ordinance Z-6-23, see further discussion later in this report), the structure would not have complied with height limitations under the old rules. On November 23, 2022, staff informed the petitioner of the determination and identified that the existing structure shall be removed and replaced with a structure that conforms to trellis or fence rules, or apply for a variation.

Variation Request
On December 8, 2022, the petitioner informed staff of the intent to pursue a minor variation for height of the structure. By March 6, 2023, a complete submittal was provided to staff for the variation request, and the petitioner seeks to have a structure that would be defined as a trellis (see attached drawings); provided, however, that the trellis would be taller and wider than is allowed. Therefore, approval of relief is necessary.

Recent Text Amendments for Fences, Trellises, and Arbors
On April 3, 2023, text amendments to clarify regulations and add terms for fences, trellises, and arbors were approved through Ordinance Z-6-23. The following definitions have been established for fence and trellis structures:

- **FENCE:** A structure used as a barrier or boundary to enclose, divide, or screen a piece of land. The term “fence” includes fences, walls, and other structural or artificial barriers that function as a wall or a fence. For the purposes of this Title, the term "fence" does not include arbors, trellises, or naturally growing shrubs, bushes, and other foliage. Fences must be made of wood, vinyl, metal, masonry, or combination thereof. The height of a fence is measured from the immediately adjacent finished grade to the highest point of the fence.
- **TRELLIS:** A freestanding structure with latticework intended primarily to support vines or climbing plants. The height of a trellis is measured from the immediately adjacent finished grade to the highest point of the trellis.

In addition to the new definitions, a summary of the new trellis regulations—as they relate to the petitioner’s request—are below.

- **Location:** Trellises are permitted in all required front, side, corner-side, or rear yards or the buildable area with some limitations;
- **Height:** Allows trellises up to six feet in height in the interior side yards;

- **Width:** Trellises cannot exceed 8 feet in width;
- **Material:** Trellises must be constructed of wood, wrought iron, vinyl, or similar decorative material; and
- **Separation:** Trellises may not be attached to or located less than six feet from other trellises.

Even though the structure existing on the subject property was constructed prior to the approval of these amendments, the structure in question was not "...otherwise lawful..." (Section 12-5-6) and therefore could not be legally nonconforming. The structure, both existing and as proposed with this application (altered), is subject to the new standards.

Project Description:

Overview

The petitioners, Demetrios and Isabelle Giokaris, have requested major variations to allow an 11-foot-tall and 50-foot-wide structure with a 4-foot-long overhang (into the subject lot, not over the lot line). Sketch plans for the proposed structure are attached. The petitioner intends to alter the existing structure so that it would match the proposed plans and be classified as a trellis. The subject property consists of a 18,014-square-foot (0.41-acre) lot with a 2,365-square-foot, two-story brick house, one frame shed, concrete stoop areas, a wood deck, and concrete driveway connecting to Webster as shown in the attached Plat of Survey/Site Plan. The existing structure is currently solid and positioned one foot off the property line in the interior side yard directly next to an existing fence as shown on the Photos of Existing Conditions.

Intended Adjustments to Existing Structure

In the Responses to Standards, the petitioners claim that the addition of the solid structure was necessitated by nuisances caused by their next-door neighbor to appropriately screen them from the neighbor's property. They also indicate that the proposed structure would be utilized as a support for climbing plants. However, both the existing solid composition of the structure and horizontal boards starting approximately 6 feet above the ground at the top of the existing 6-foot-tall fence prevent any plantings from growing on or attaching to the structure for support. Therefore, as shown on the plans, the petitioner intends to remove every other (i.e. alternating) horizontal board on the structure and extend this pattern down to grade in order to create openings for climbing plants and meet the classification of a trellis. There are no plans to reduce the height or width of the structure, requiring variations from Sections 12-7-1.C and 12-8-14.B.1 of the Ordinance.

PZB Considerations

Based on the substantial size, positioning, and design of the structure in question, the PZB may wish to analyze if the true intent/utilization of the structure, as proposed to be designed, is more to serve as a barrier to screen the petitioner's property from view from the neighbor instead of a structure for the primary purpose of providing support for climbing plants. Further, the PZB may inquire as to what plantings the petitioner intends to install on the property that necessitate an 11-foot-tall, 50-foot-wide trellis structure, or namely why alternative plantings that do not need support from other structures were not installed in its place. Nonetheless, see staff's analysis of the variation standards.

Variation Findings: Variation requests are subject to the standards set forth in Section 12-3-6(H) of the Zoning Ordinance. Rationale for how the proposed amendments would or would not satisfy the standards is provided below and in the attached petitioner responses to standards. The Board may use the provided responses as written as its rationale, modify, or adopt its own.

- 1. Hardship: No variation shall be granted pursuant to this subsection H unless the applicant shall establish that carrying out the strict letter of the provisions of this title would create a particular hardship or a practical difficulty.**

Comment: Considering the other opportunities available, the zoning challenges encountered do not rise to the level of hardship or practical difficulty. The petitioner argues that the nuisances caused by their neighbor require the installation of the structure in question to serve as buffer screening between the two properties. This is further enforced by the size and design of the existing structure extending 50 feet between the existing shed and residence. Approval would allow a structure that is almost double the allowable height and six times the allowable width. Through either testimony in the public hearing or via the submitted responses, the Board should review, question, and evaluate whether a hardship or practical difficulty exists.

PZB Additions or Modifications (if necessary): _____

- 2. Unique Physical Condition: The subject lot is exceptional as compared to other lots subject to the same provision by reason of a unique physical condition, including presence of an existing use, structure, or sign, whether conforming or nonconforming; irregular or substandard shape or size; exceptional topographical features; or other extraordinary physical conditions peculiar to and inherent in the subject lot that amount to more than a mere inconvenience to the owner and that relate to or arise out of the lot rather than the personal situation of the current owner of the lot.**

Comment: The subject property is a typical rectangular, interior lot that is neither exceptional to the surrounding lots nor contains unique physical features that prevent the petitioner from complying with the appropriate regulations. It has been noted that there have been reoccurring nuisances generated by the neighboring property. However, *these are conditions of the neighboring property abutting the petitioner's property—not unique physical conditions of the subject property itself*, which is the basis of this variation standard. As there are ample opportunities for the petitioner to locate a code-compliant trellis or trellises on the subject property, the request for an 11-foot-tall and 50-foot-wide trellis appears to be more of a personal preference of the property owner instead of a definable physical condition.

PZB Additions or Modifications (if necessary): _____

- 3. Not Self-Created: The aforesaid unique physical condition is not the result of any action or inaction of the owner or its predecessors in title and existed at the time of the enactment of the provisions from which a variance is sought or was created by natural forces or was the result of governmental action, other than the adoption of this title.**

Comment: As there is no definable unique physical condition of the subject property itself, it is noted that the property attributes as they relate to size and location were not caused by the petitioner. However, the development of the deck and 11-foot-wide and 50-foot-wide barrier-type structure was directly constructed by the petitioner, and any perceived unique physical conditions or hardships created from these items are a direct result of the actions of the property owner.

PZB Additions or Modifications (if necessary): _____

_____.

4. Denied Substantial Rights: The carrying out of the strict letter of the provision from which a variance is sought would deprive the owner of the subject lot of substantial rights commonly enjoyed by owners of other lots subject to the same provision.

Comment: Carrying out the strict letter of this code for height and width restrictions of a trellis structure does not deprive the property owners of substantial rights. First, while homeowners are able to construct trellises, as permitted by the trellis regulations, having the ability to construct a trellis in and of itself is not a right granted to property owners. Enforcing the trellis height and width requirements does not deny the property owners from constructing a trellis on their property but requires said trellis structure to conform with the applicable requirements that apply to any trellis structure installed throughout the City.

Given the initially installed, existing 11-foot-tall and 50-foot-wide structure on the subject property, as illustrated in the attached Photos of Existing Conditions, and the petitioner's rationale that the structure in question is necessary to address perceived nuisances from a neighboring property, the PZB may discuss whether the true intention of this structure is more to provide a substantial barrier between the two properties rather than providing support for climbing plants. Even if solely for the intention of supporting climbing plants, the PZB may ask itself if the ability to install a structure at this scale is a right to which Des Plaines property owners are entitled given there are available alternatives to achieve the functional needs of a trellis.

PZB Additions or Modifications (if necessary): _____

_____.

5. Not Merely Special Privilege: The alleged hardship or difficulty is neither merely the inability of the owner or occupant to enjoy some special privilege or additional right not available to owners or occupants of other lots subject to the same provision, nor merely the inability of the owner to make more money from the use of the subject lot.

Comment: Granting this variation may, in fact, provide a special privilege for the property owner not available to other single-family residential properties. Variation decisions are made on a case-by-case, project-by-project basis upon applying the variation standards. In those evaluations, the determining body (e.g. PZB and/or City Council) usually determines the applicant has exhausted design options that do not require a variation. In this case, there are different design options, sizes, and positions for a trellis structure on the subject property, none of which warrant the substantial size of the structure in question. Granting a variation for this design at this location, when other viable options are available throughout the property, could be too lenient and tread into the territory of allowing a special privilege.

PZB Additions or Modifications (if necessary): _____

_____.

- 6. Title And Plan Purposes: The variation would not result in a use or development of the subject lot that would be not in harmony with the general and specific purposes for which this title and the provision from which a variation is sought were enacted or the general purpose and intent of the comprehensive plan.**

Comment: On one hand, the project would allow re-investment into a single-family home, which the Zoning Ordinance and Comprehensive Plan want to encourage. However, the existing structure is solely for the benefit of the property owner and is not consistent with any general and specific purposes of the Comprehensive Plan. For one, the structure is 50-feet-wide, extending from the residence to an existing shed, resembling a fence/barrier more than any standard trellis structure. Further, the petitioner references Section 12-10-1 of Chapter 10, “Landscaping and Screening”, of the Zoning Ordinance related to the purpose of the landscaping requirements in their responses to standards. A trellis structure is not, by definition, natural foliage utilized to meet the landscaping requirements detailed in this section nor is a trellis mentioned in the section as a means to satisfy the landscaping requirements. Instead, a trellis is a built structure which purpose is decoration and support for climbing plants. Conversely, a fence structure is referenced several times in the aforementioned section as a means to appropriately screen non-residential uses from residences. This code section reference by the petitioner clarifies the intention and use of the structure in question as a screening mechanism—similar to a fence—under the guise of a trellis. In addition, the existing structure is solid with horizontal boards starting approximately 6 feet above grade, which does not provide support for climbing plants. Even the proposed alterations to the structure appear to be more focused on maintaining the barrier-like presence of the structure rather than serving the purpose of a trellis. Nonetheless, the existing barrier-type structure is not harmonious with other residences in the R-1 district and does not meet the regulations for either trellis or fence structures. There are reasonable options for designing a trellis structure to create an adequate space for the growing of various plant material without the height and width of the existing structure in question.

PZB Additions or Modifications (if necessary): _____

_____.

- 7. No Other Remedy: There is no means other than the requested variation by which the alleged hardship or difficulty can be avoided or remedied to a degree sufficient to permit a reasonable use of the subject lot.**

Comment: There are several alternatives to the height and width variations being requested. The code allows for the installation of multiple trellis structures on the property, with a minimum 6-foot-separation between structures. It also allows for various trellis heights based on the trellis location on the property, restricting trellis height to 6 feet or less in required yards but allowing a maximum trellis height of 8 feet in the buildable area. In addition to the above improvements, natural plantings can also be added to provide a natural barrier between the properties as sought by the petitioner. In short, there are ample alternatives available based on the regulations, not just for trellis structures in which multiple designs and locations are available based on the regulations, but also other improvements such as landscaping. The PZB may wish to ask why certain alternative designs are not feasible.

PZB Additions or Modifications (if necessary): _____

_____.

8. Minimum Required: The requested variation is the minimum measure of relief necessary to alleviate the alleged hardship or difficulty presented by the strict application of this title.

Comment: The approval of the height and width variations may provide relief for the petitioner given their current proposal for the existing structure installed on site. However, staff argues that the alleged hardship related to nuisances from a neighboring property could be satisfied with alternative proposals that better utilize the physical characteristics of the property, incorporate trellis structures in a cohesive and harmonious way with the neighboring built environment, and meet the appropriate requirements. And while the minor adjustments to the existing barrier-type structure may be more convenient and less intensive than the alternative plans, these are not factors in staff's analysis that demonstrate a true hardship or practical difficulty. The PZB may determine if the measure of relief is appropriate or necessary in its recommendation.

PZB Additions or Modifications (if necessary): _____

_____.

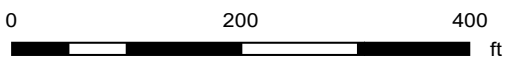
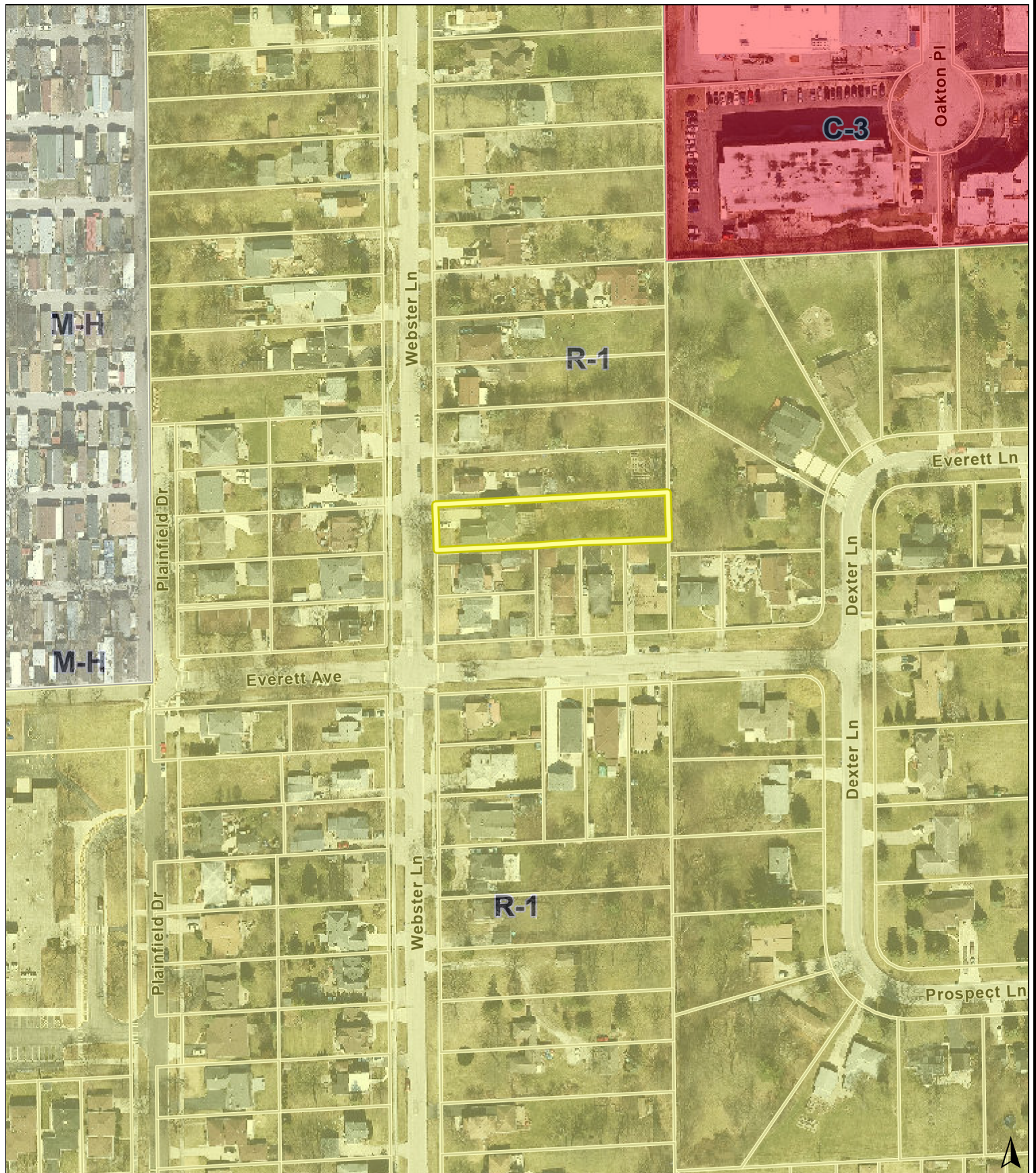
PZB Procedure and Recommended Conditions: Under Section 12-3-6(F) of the Zoning Ordinance (Major Variations), the PZB has the authority to recommend approval, approval subject to conditions, or denial of the request to City Council. The decision should be based on review of the information presented by the applicant and the standards and conditions met by Section 12-3-6(H) (Findings of Fact for Variations) as outlined in the Zoning Ordinance. If the PZB recommends approval of the request, staff recommends the following conditions.

Conditions of Approval:

1. No easements are affected or drainage concerns are created.
2. The structure must be freestanding without any attachment to existing structures on the property.
3. All appropriate building permit documents and details, including dimensions and labels necessary to denote the addition, must be submitted and approved for the proposed project. All permit documents shall be sealed and signed by a design professional licensed in the State of Illinois and must comply with all City of Des Plaines building and life safety codes.

Attachments:

- Attachment 1: Location and Zoning Map Petitioner's
- Attachment 2: Site & Context Photos
- Attachment 3: Existing Condition Photos
- Attachment 4: Responses to Standards for Variation
- Attachment 5: Plat of Survey and Site Plan
- Attachment 6: Trellis Sketches



Print Date: 4/20/2023

Notes

Disclaimer: The GIS Consortium and MGP Inc. are not liable for any use, misuse, modification or disclosure of any map provided under applicable law. This map is for general information purposes only. Although the information is believed to be generally accurate, errors may exist and the user should independently confirm for accuracy. The map does not constitute a regulatory determination and is not a base for engineering design. A Registered Land Surveyor should be consulted to determine precise location boundaries on the ground.



1773 Webster Ln – Public Notice and Front of Property



1773 Webster Ln – Close-up of Existing “Trellis” Structure



1773 Webster Ln – Across Street view of Existing “Trellis” Structure



1773 Webster Ln – View of “Trellis” Structure at Front Property Line





RESPONSES TO
STANDARDS FOR VARIATIONS

1. Hardship: No variation shall be granted pursuant to this subsection H unless the applicant shall establish that carrying out the strict letter of the provisions of this title would create a particular hardship or a practical difficulty.

ANSWER: Because Mr. Venetsanakos directs light, noise and video cameras at heights in excess of 8 feet, the existing regulation does not protect our family. The proposed trellis seeks to balance the existing regulation with the exigent need for buffer screening.

2. Unique Physical Condition: The subject lot is exceptional as compared to other lots subject to the same provision by reason of a unique physical condition, including presence of an existing use, structure, or sign, whether conforming or nonconforming; irregular or substandard shape or size; exceptional topographical features; or other extraordinary physical conditions peculiar to and inherent in the subject lot that amount to more than a mere inconvenience to the owner and that relate to or arise out of the lot rather than the personal situation of the current owner of the lot.

ANSWER: We constructed a nice outdoor deck in the rear of our home. Since 2017, Mr. Venetsanakos has engaged in a campaign to prevent my family and friends from using the deck and backyard by subjecting us to a barrage of light, noise and other disturbances targeting the deck area and backyard. By way of one example, Mr. Venetsanakos has installed audio speakers at both ends of the south wall of his garage (only 6 ft. away) and one on a post that reaches approximately 10 feet, which is over my fence and aimed directly at our deck and home. Further, we note that our lot is over 300 feet deep, so the trellis will be small in the context of our yard, and not seen as it runs along the wall of his garage.

3. Not Self-Created: The aforesaid unique physical condition is not the result of any action or inaction of the owner or its predecessors in title and existed at the time of the enactment of the provisions from which a variance is sought or was created by natural forces or was the result of governmental action, other than the adoption of this title.

ANSWER: My family has lived in our home since 2005, without any disturbance. Since 2017, Mr. Venetsanakos has created the untenable situation which has been extensively documented with the City of Des Plaines, the Des Plaines Police Department, and our Alderperson.

4. Denied Substantial Rights: The carrying out of the strict letter of the provision from which a variance is sought would deprive the owner of the subject lot of substantial rights commonly enjoyed by owners of other lots subject to the same provision.

ANSWER: Mr. Venetsanakos has installed audio speakers at both ends of his south side garage and one on a post that reaches approximately 10 feet high and is aimed directly at our deck and home. Strict adherence to the trellis height restriction of 8 feet would deprive our family of the substantial right to inhabit our backyard, which is a right commonly, if not universally, enjoyed by other residents of Des Plaines.

5. Not Merely Special Privilege: The alleged hardship or difficulty is neither merely the inability of the owner or occupant to enjoy some special privilege or additional right not available to owners or occupants of other lots subject to the same provision, nor merely the inability of the owner to make more money from the use of the subject lot.

ANSWER: We are not seeking a special privilege or additional rights. Other Des Plaines homeowners have the right to enjoy their backyards. We are being deprived of this very same right through no fault or action of our own.

6. Title And Plan Purposes: The variation would not result in a use or development of the subject lot that would be not in harmony with the general and specific purposes for which this title and the provision from which a variation is sought were enacted or the general purpose and intent of the comprehensive plan.

ANSWER: The proposed trellis will be harmonious and appropriate in appearance with the intended character of our neighborhood. The City of Des Plaines has enacted regulations to ensure that residential properties perpetuate the existing high quality residential character of the City. The landscaping requirements of the City are intended to “preserve and enhance the appearance, character, health, safety, and general welfare of the community by fostering esthetically pleasing development.” The regulations are intended to “*increase the compatibility of adjacent uses, and minimize the adverse impact of noise, dust, motor vehicle headlight glare or other artificial light intrusions, and other objectionable activities or impacts conducted on or creating by adjoining or neighboring uses.*” (ORD. Z-8-20, 2-18-2020) (Chapter 10, Section 12-10-1).

The proposed trellis is consistent with the goals, objectives, and policies of the City's zoning regulations and specifically the purpose of the landscaping and screening requirements.

7. No Other Remedy: There is no means other than the requested variation by which the alleged hardship or difficulty can be avoided or remedied to a degree sufficient to permit a reasonable use of the subject lot.

ANSWER: Even after 6 years of police intervention, and legal cease and desist letters, Mr. Venetsanakos continues his abuse and harassment. We have run out of options, and there is no other reasonable or economically feasible solution to buffer the noise, lights, cameras, and harassment.

8. Minimum Required: The requested variation is the minimum measure of relief necessary to alleviate the alleged hardship or difficulty presented by the strict application of this title.

ANSWER: The requested variation of an additional 3 feet to the height of the proposed trellis is necessary because Mr. Venetsanakos directs, light, noise and video cameras at heights reaching 10 feet. I know this trellis buffering will provide relief because the current trellis that the City wants me to dismantle has proven to be an effective buffer and helps me live a more peaceful life to which I am entitled.

PLAT OF SURVEY

MARCHESE SURVEYING, INC.

RESIDENTIAL - COMMERCIAL SURVEYS

714 Fairview Lane
Bartlett, Illinois 60103

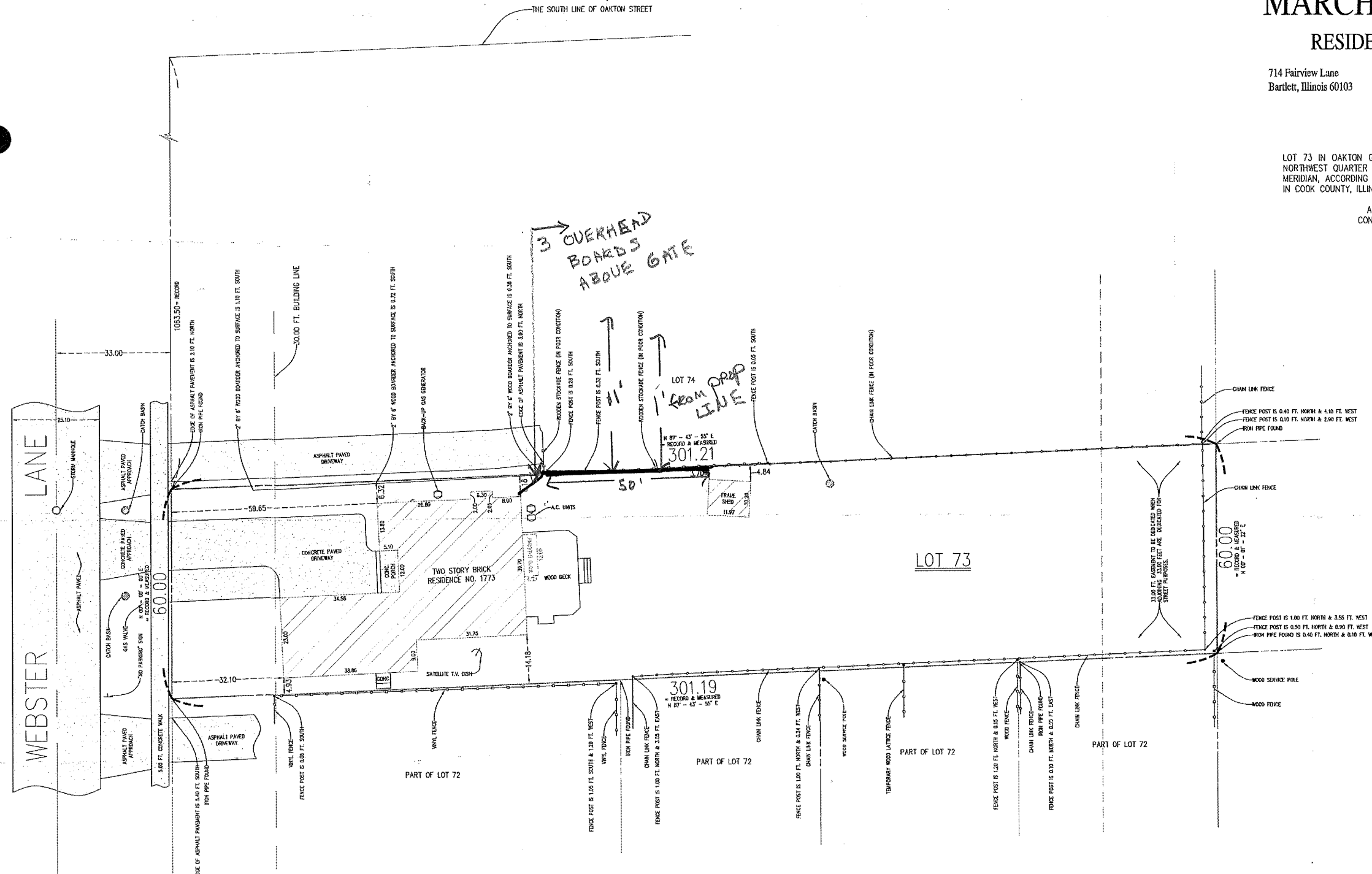
Phone: (630) 830-1570
Fax: (630) 830-1844
E-Mail: marcheseurveying@tbcglobal.net

PROPERTY DESCRIPTION

LOT 73 IN OAKTON GARDENS, BEING A SUBDIVISION OF THE EAST HALF OF THE WEST HALF OF THE NORTHWEST QUARTER OF SECTION 29, TOWNSHIP 41 NORTH, RANGE 12, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED MAY 13, 1937 AS DOCUMENT NUMBER 11996751, IN COOK COUNTY, ILLINOIS.

ALSO KNOWN AS: 1773 WEBSTER LANE IN DESPLAINES, ILLINOIS,
CONTAINING 18,057.85 SQUARE FEET OR 0.42 ACRES, MORE OR LESS.

NORTH



SCALE: ONE INCH = FIFTEEN FEET

ORDER NO.: 17-18502

ORDERED BY: MR. DRAKE JAMES LEORIS, JR.
ATTORNEY AT LAW

COMPARE ALL POINTS BEFORE BUILDING BY SAME AND AT ONCE
REPORT ANY DIFFERENCE FOR BUILDING LINE AND OTHER
RESTRICTIONS NOT SHOWN HEREON. REFER TO YOUR CONTRACT,
DEED, TITLE INSURANCE POLICY AND ZONING ORDINANCE.

STATE OF ILLINOIS
COUNTY OF DUPAGE

I, ROCCO J. MARCHESE, HEREBY CERTIFY THAT I HAVE SURVEYED THE
ABOVE DESCRIBED PROPERTY AND THAT THE PLAT DRAWN HEREON IS
A CORRECT REPRESENTATION OF SAID SURVEY.
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS
MINIMUM STANDARDS FOR A BOUNDARY LINE SURVEY.

DATED AT BARTLETT, APRIL 14, 2017

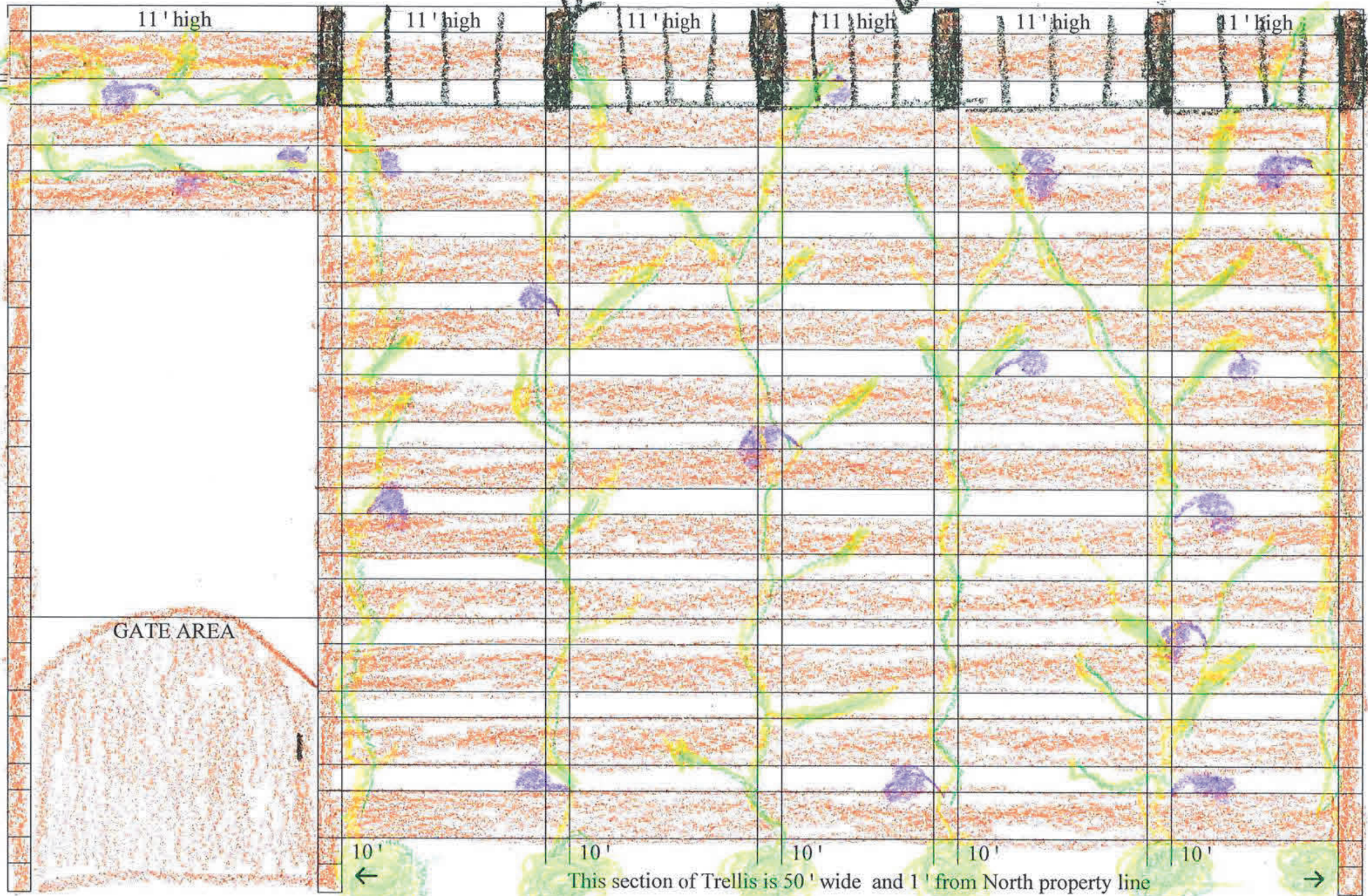
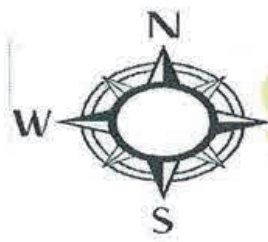


Rocco J. Marchese
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 3039
MY LICENSE EXPIRES ON NOVEMBER 30, 2018

ANY REPRODUCTION OF THIS PLAT IS STRICTLY PROHIBITED WITHOUT
WRITTEN CONSENT FROM MARCHESE SURVEYING, INC.

NOT VALID UNLESS SEAL IS IN RED INK.

TRELLIS with Pergola Tops (extend 4' South)

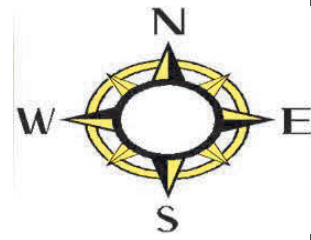


10' ← This section of Trellis is 50' wide and 1' from North property line → 10'

PLANTS →

8889553.1

TRELLIS with Pergola Tops (extend 4' South)



HOUSE
WALL

GATE AREA

SHED
WALL

11' high

11' high

11' high

11' high

11' high

11' high

10'

10'

10'

10'

10'



This section of Trellis is 50' wide and 1' from North property line

